

EXECUTIVE

Date: Tuesday 7th June, 2022
Time: 10.00 am
Venue: Council Chamber

AGENDA

1. Apologies for Absence
2. Declarations of Interest
3. Minutes - Executive - 29 April 2022 3 - 6
4. Minutes - Executive - 10 May 2022 7 - 14

EXECUTIVE MEMBER FOR REGENERATION

5. Adoption of Stainsby Country Park and Masterplan 15 - 410
6. Any other urgent items which in the opinion of the Chair, may be considered.

Charlotte Benjamin
Director of Legal and Governance Services

Town Hall
Middlesbrough
Monday 30 May 2022

MEMBERSHIP

Mayor A Preston (Chair) and Councillors B Cooper, D Coupe, TA Grainge, S Hill, L Mason, E Polano, M Smiles and S Walker

Assistance in accessing information

Should you have any queries on accessing the Agenda and associated information please contact Chris Lunn / Georgina Moore, 01642 729742 / 01642 729711, chris_lunn@middlesbrough.gov.uk / georgina_moore@middlesbrough.gov.uk

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EXECUTIVE

A meeting of the Executive was held on Friday 29 April 2022.

PRESENT: Mayor A Preston (Chair) and Councillors B Cooper, D Coupe, TA Grainge, L Mason and E Polano

OFFICERS: C Benjamin, R Brown, S Reynolds, M Walker and I Wright

APOLOGIES FOR ABSENCE: Councillors S Hill and M Smiles

21/124 **DECLARATIONS OF INTEREST**

There were no declarations of interest received at this point in the meeting.

21/125 **PROCEDURAL GUIDANCE - VERBAL UPDATE**

The Monitoring Officer made a verbal statement regarding the Council's Access to Information Procedure Rules and the process that had been followed prior to the meeting of the Executive, which was held on 19 April 2022.

Due to a procedural oversight in fulfilling the requirements of a general exception in relation to a key decision, as per the Council's Constitution, it was advised that there was a requirement for the Executive to re-consider the reports entitled 'Homes for Ukraine Impact' and 'Star Academies - Option for Land Disposal'.

21/126 **HOMES FOR UKRAINE IMPACT**

The Executive Member for Culture and Communities and the Director of Environment and Community Services submitted a report for the Executive's consideration. The purpose of the report was to provide an update on the impact of the Homes for Ukraine programme in Middlesbrough and seek agreement on how it was to be managed locally.

The Department for Levelling Up, Housing and Communities (DLUHC) had released guidance around the expectations on local authorities, as a result of the Homes for Ukraine scheme. The guidance covered issues such as data sharing, safeguarding, payments for hosts and guests, education, health, community integration and more.

Middlesbrough Council proposed the following management approach:

- liaising with North East Regional Migration Partnership (NEMP), British Red Cross, and Local Resilience Forum (LRF) regarding reception arrangements at port of entry;
- completing property checks when data on sponsor was shared by DHLUC;
- completing safeguarding checks through internal systems and Disclosure and Baring Service (DBS) as required;
- managing the interim payment for guests;
- providing education;
- making appropriate service referrals;
- supporting arrivals in accessing benefits and working in partnership with statutory and voluntary services;
- providing homelessness assistance if matching failed in line with statutory duties;
- administering payments to sponsors;
- utilising a proportion of the tariff to launch trauma informed hosting courses through VCS; and
- ensuring health protection was in place (i.e. GP registration, Covid vaccination, childhood immunisations).

Appendix 1 of the submitted report set out the expectations on Middlesbrough Council for Ukraine Resettlement and Appendix 2 illustrated the local process for guests arriving in Middlesbrough.

OPTIONS

The option to do nothing had been considered and it was not recommended due to the fact that central government had placed responsibility on local authorities to support guests and make check on sponsors. It was therefore necessary to have a management plan in place.

ORDERED

That the impact of the Homes for Ukraine programme in Middlesbrough be noted and that the management approach, to introduce the scheme locally, be agreed.

REASON

Introducing the management approach planned to allow the Council and key partners to provide services to sponsors and guests and ensure that any support needs were met.

21/127

STAR ACADEMIES - OPTION FOR LAND DISPOSAL

The Executive Member for Environment, Finance and Governance and the Director of Finance submitted a report for the Executive's consideration. The purpose of the report was to outline the case to provide Star Academies, in partnership with Eton College, with a first option to purchase the designated land at Middlehaven under agreed commercial terms, subject to outlined conditions.

Star Academies was recognised for its extensive experience of establishing, developing and continually improving outstanding schools that delivered outstanding results, including in terms of the progress made by pupils from disadvantaged backgrounds and for progress made by their most able learners.

The site, identified as site No 19, highlighted in Appendix A of the submitted report was 1.45 acres, and had been declared surplus to the operational requirements of the Council and was currently being held as available for sale.

The Council's vision for the regeneration of Middlehaven included the development of educational provision in the area, including development at Middlesbrough College and Outwood Academy Riverside. The development of educational provision in the area brought animation to Middlehaven and increased footfall in both the immediate area and the town centre.

It was planned that the commercial principles would involve the following:

- Star Academies being given the ringfenced option to acquire the land - subject to achieving DFE support and funding for the sixth form college development;
- land being disposed of at market value; and
- the land being subject to revaluation.

OPTIONS

The Council could have chosen not to support the disposal of a site. That, however, would have led to the potential of the project being delivered elsewhere. Middlesbrough Council recognised both the educational benefits the project would deliver, along with the wider economic and regeneration impacts that such an opportunity presented. Therefore, the Council was committed to ensuring that it facilitated a site that met the key objectives of the project.

The Council could have facilitated the site on non-commercial terms. The application of the asset disposal policy had been applied to the last three educational developments in which all three had included land disposals. For the three transactions, commercial terms had been agreed. As such, it was imperative that the Council maintained its position that the asset disposal policy was followed and equity in its application towards the principles of commerciality in the transaction was maintained.

ORDERED

That Star Academies be provided with a first option to purchase the designated land at Middlehaven under agreed commercial terms, subject to:

- **DFE funding for the project being approved;**
- **the land being disposed under commercial terms at market value; and**
- **a further report being brought to Executive on the detail of the disposal, in accordance with the Council's Asset Disposal Policy.**

REASON

The decision was recommended for the following reasons:

- **the previous three educational developments had all been progressed on commercial terms, namely Discovery School at Natures World, Northern School of Art and more recently Outwood Academy Riverside at Middlehaven;**
- **the proposal would fit with the Council's vision for the regeneration of Middlehaven;**
- **it would generate a capital receipt;**
- **ensuring the parcel of land was ringfenced for the development signalled a clear commitment to all parties that the Local Authority was supportive of the proposal; and**
- **the future potential disposal may be at a value in excess of the £150k threshold.**

The decision(s) will come into force after five working days following the day the decision(s) was published unless the decision becomes subject to the call in procedures.

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EXECUTIVE

A meeting of the Executive was held on Tuesday 10 May 2022.

PRESENT: Mayor A Preston (Chair) and Councillors B Cooper, D Coupe, TA Grainge, S Hill, L Mason, E Polano and M Smiles

ALSO IN ATTENDANCE: E Craigie (Teesside Live)

OFFICERS: S Butcher, G Field, R Horniman, A Hoy, S Reynolds, E Scollay, A Wilson and I Wright

21/128 **DECLARATIONS OF INTEREST**

There were no declarations of interest received at this point in the meeting.

21/129 **MINUTES - EXECUTIVE - 19 APRIL 2022**

The minutes of the Executive meeting held on 19 April 2022 were submitted and approved as a correct record.

21/130 **CULTURAL CAPITAL INVESTMENT PROSPECTUS**

The Executive Member for Culture and Communities and the Director of Regeneration and Culture submitted a report for the Executive's consideration. The purpose of the report was to:

- a) brief the Executive Members on the Cultural Capital Investment Prospectus and Cultural Partnership; and
- b) seek support for the Cultural Development Fund.

The Cultural Capital Investment Prospectus, developed by Middlesbrough Cultural Partnership, was a live document that brought together the current opportunities for investment within the town's creative sector.

The partnership had held an online event on Monday 25 April 2022 to officially launch the prospectus to both internal and external stakeholders, who would be instrumental to delivering the projects detailed in the prospectus.

The partnership worked collectively to bid for the Cultural Development Fund (CDF) for an investment of £4.25 million in the town. Organisations involved in the bid included Middlesbrough Council, MIMA, The Auxiliary, Middlesbrough Railway Station and Platform Arts. The partnership had successfully been awarded £4.25 million from the Department for Culture, Media and Sport.

Feasibility studies had been completed by consultants and a breakdown of the elements that the funding planned to cover, for each of the respective organisations, was tabulated underneath paragraph 7 of the submitted report.

The Executive commended the work undertaken by the Head of Culture and the Cultural Services Team in securing funding of £4.25 million from the Department for Culture, Media and Sport.

OPTIONS

A further option would have been to not accept the funding, which was not recommended as the Council had applied for it.

ORDERED

1. That the development of Middlesbrough Cultural Partnership's plans for

investment across the town be supported.

2. That, provided the holistic budget limit was not exceeded, the Director of Regeneration and Culture and Director of Finance be delegated authority on all decisions pertaining to the delivery of the Cultural Development Fund, including:
 - entering into a funding agreement with Arts Council England;
 - procurement and contractual arrangements;
 - appointment of specialist advisors;
 - marketing and communications; and
 - asset management and operations.

REASON

The funding had been secured to deliver significant improvements across the cultural provision of the town.

21/131

DEMOLITION OF THE FORMER SLAM NIGHTCLUB

The Executive Member for Environment, Finance and Governance and the Director of Environment and Community Services submitted a report for the Executive's consideration. The purpose of the report was to seek approval for the demolition of the former Slam nightclub.

The building was located in Middlesbrough's Historic Quarter, under the A66. It fronted onto Exchange Square, with the rear facing Wilson Street. The building was on a main thoroughfare from the retail core to the Railway Station, Boho Zone and Middlehaven.

The property was last used as the Slam bar and nightclub, which closed its doors to customers in 2016. The building had been vacant since that date. Until recently, whilst Middlesbrough Council owned the freehold, the property had been subject to a 125 year lease. That lease had now been surrendered by the leaseholder.

As a result of water ingress at the bridge joint, one of the A66 columns needed significant, imminent repair works to make it safe, which would require part-demolition of the building.

For the reasons summarised in the submitted report, it was an opportune time to consider the future of the building; to assess its commercial value versus maintenance and risk factors; and, to understand if the space had a role in opening up links from central Middlesbrough to the Boho Zone, Middlehaven area and Historic Quarter.

OPTIONS

Following partial demolition to carry out essential bridge repair works some reinstatement works would have been necessary to make the building water-tight and safe.

Minimal works would have been undertaken to achieve that, which would have left the building in an unlettable condition. That would have inevitably led to further deterioration over time and, if the building was subsequently let, would have created access issues and would have cost implications for further inspections of the bridge and any future maintenance requirements. Therefore, that option was not recommended.

Sale of the building - That was not recommended, given the ongoing need for access to the A66 and its column supports.

Operational use - the property could have been an operational/storage facility for the Council. That was not recommended for the following reasons:

- operational or storage space may have been better suited to an out of town centre location;
- it would not have generated any income and required operational costs, utilities and business rates.
- the Council would have carried the maintenance liability; and

- **there were potential access issues for bridge inspections.**

ORDERED

That the demolition of the former Slam nightclub be approved.

REASON

As a minimum, the Council was required to part-demolish the building in order to carry out essential A66 repair work. The costs to partly demolish and then reinstate the building to a letting standard would have cost almost as much as full demolition. Leaving the building in-situ would have, in the future, created access issues for general and principal inspections of the overbridge structure and supporting columns and additional cost if further remedial or maintenance works were ever required to the bridge columns or spans.

21/132

GREEN STRATEGY - YEAR TWO ACTION PLAN

The Executive Member for Environment, Finance and Governance and the Director of Environment and Community Services submitted a report for the Executive's consideration. The purpose of the report was to seek approval of Middlesbrough Council's Green Strategy Year Two Action Plan and to summarise the progress made in Year One.

The Green Strategy had three main aims:

- To make Middlesbrough Council net carbon neutral by 2029.
- To ensure Middlesbrough as a town was net carbon neutral by 2039.
- To make Middlesbrough a lead authority on environmental issues

The Green Strategy was an overarching climate change and sustainability strategy that contained a number of themed action plans based upon the One Planet Living (OPL) framework and principles, which were:

- Zero Carbon;
- Zero Waste;
- Land Use and Wildlife and Sustainable Water;
- Sustainable Transport;
- Culture and Community;
- Health and Happiness;
- Equity and Local Economy;
- Local and Sustainable Food; and
- Sustainable Materials.

The Strategy had been adopted and approved by Executive in 2021 and had delivered a range of achievements in Year One (2021/22), which were summarised at paragraph 5 of the submitted report.

The Mayor raised a query regarding Middlesbrough's recycling rates. The Director of Environment and Community Services explained that Middlesbrough's current recycling rate was 33% and the Council was undertaking work to educate residents on recycling. It was also added that a new Government Bill would propose plans to boost recycling and detail new targets.

A Member commented that it would be beneficial for electric car charge points to be promoted and a map of locations to be developed.

OPTIONS

The other option was to do nothing and to not approve the proposed Year Two Action Plan - That would have resulted in the Council not meeting its set climate change obligations and ambitions to be a lead authority on environmental initiatives and therefore was not recommended for that reason.

ORDERED

1. That the progress achieved in Year One (2021/22) of the Green Strategy, as outlined, be noted and the following Year Two (2022/23) actions be approved:
 - Commence with data capture for carbon emissions for the town of Middlesbrough.
 - Enhance digital connectivity and develop a digital poverty strategy.
 - Develop a culture programme for parks with green credentials.
 - Deliver an Eco-festival.
 - Introduce active travel measures and hubs.
 - Establish a green social prescribing model.
 - Develop clean air zones.
 - Work with suppliers to help them reduce their carbon footprint.
 - Develop a social value framework and charter for Council procurement contracts.
 - Establish a food re-distribution hub in Middlesbrough.
 - Develop five new community growing sites.
 - Achieve the Sustainable Food Places Gold Award with a '22 Carrot Gold' campaign.
 - Develop two new nature reserves.
 - Purchase new machinery which will help manage meadows and verges more sustainably.
 - Plant 13,000 trees and sow 80,000m² of urban meadows.
 - Work with business and organisations to help them further reduce their carbon impact and explore energy saving measures.
 - Further expand the Council's electric vehicle fleet.
 - Work with the Tees Valley Combined Authority to install new electric vehicle charging points in locations across the town.
 - Retain World Tree City status.
 - Hold a Climate Showcase event for business and communities.

REASON

The recommendation to progress with the outlined actions for Year Two of the Green Strategy was the only possible decision, other than to do nothing.

The Green Strategy was a long-term environmental programme and approach to respond to the climate emergency. Middlesbrough had to act now to rise to that challenge to ensure it met targets of becoming net carbon neutral by 2029 for the Council and 2039 as a town. The actions for Year Two planned to enable the Council to move closer to achieving its three overall environmental aims:

- To make Middlesbrough Council net carbon neutral by 2029.
- To ensure Middlesbrough as a town was net carbon neutral by 2039.
- To make Middlesbrough a lead authority on environmental issues.

The Council therefore had an obligation to respond to climate change and lead by example by delivering against its promised actions within the Green Strategy on which it undertook public consultation in 2021.

The proposed Year Two actions of the Green Strategy had not been examined by Scrutiny.

21/133

IMPROVING OUR HIGHWAYS

The Executive Member for Environment, Finance and Governance and the Director of Environment and Community Services submitted a report for the Executive's consideration. The purpose of the report was to request the Executive to approve prioritisation of immediate works required for 2022/23 (detailed in Appendix 1 of the submitted report), on the premise that during which time a re-evaluation exercise would be undertaken to address the approach to Red / Amber carriageway defects.

The deterioration of national carriageways was an issue across the country. In Middlesbrough, 15% of all carriageways were currently rated Red / Amber. It was therefore appropriate to re-evaluate the approach to maintaining and improving highway assets, with a view to improving the condition of the road network.

The estimated cost of dealing with all Red / Amber roads using existing contracts was £18.9m. The submitted report described how savings of 18% could be derived, reducing the bill to £15.5m. It was thought that the tendering process would not be complete until December 2022 and therefore the report recommended an approach that would result in £2m of works commencing within Quarter Two of 2022/23.

Whilst the work took place, a re-evaluation exercise would be undertaken to address Red / Amber carriageway defects as per Department for Transport classifications; developing a longer-term strategy for maintenance and improvements to carriageways, in-line with the medium-term financial plan.

The Executive was in agreement that the significant investment of £15 million would enable the Council to develop a planned approach for identifying, maintaining and improving carriageways in Middlesbrough.

OPTIONS

Due to the significant levels of deterioration across the road network and the due diligence work undertaken as part of the submitted report, no other options had been considered.

ORDERED

- 1. That identification and progression of focused activity in 2022/23 be approved to address the worst-affected 12.9% of Red / Amber roads at a cost of £2m (detailed at Appendix 1), noting that in doing so higher direct award prices (estimated at approximately £360k) would be incurred.**
- 2. That the undertaking of re-evaluation exercise be approved, in developing a longer-term strategy for identifying, maintaining and improving carriageways to address 100% of Red / Amber roads, tendering for works via the NEPO framework.**

REASON

To provide the necessary detail to allow the Executive to make an informed decision based on need and impact, whilst demonstrating that the service remained fit for purpose to best address the needs of the town.

21/134

PROPOSED SERVICE DELIVERY MODEL AND ASSOCIATED SUBSIDISED CHARGES FOR RESIDENTIAL PEST CONTROL

The Executive Member for Environment, Finance and Governance and the Director of Environment and Community Services submitted a report for the Executive's consideration. The purpose of the report was to seek approval of the proposed service delivery model and associated subsidised charges for residential pest control.

At Full Council on 23 February 2022, £90,000 was allocated to providing a subsidised residential pest control service in Middlesbrough. That planned to fund additional resources to ensure that current service levels did not drop and that the Council could offer a subsidised chargeable residential service, which was reliable. The income from the service, along with the £90,000 budget allocation, planned to make it self-sustainable based on the number of visits in 2017/18, which was when the Council last provided a residential service.

The proposed charge for residential pest control in Middlesbrough was £15 per visit, regardless of what type of pest was being treated. Whilst some local authorities had a free service for rats and mice inside the home, they still charged up to £40+ per visit for those pests in gardens and other outside areas.

A cost analysis of the Tees Valley local authorities and private businesses had been

undertaken, the findings of which were detailed below paragraph 6 of the submitted report.

The £15 per visit proposed in the submitted report would be for any residential pest control visit either inside the home or in the outside boundary of the residential property. Therefore, the overall cost for members of the public would be lower than neighbouring authorities and significantly lower than that of private contractors.

OPTIONS

The option to do nothing was not recommended as the issue had been agreed as a priority at the Full Council budget setting meeting on 23 February 2022, and funding had been allocated to provide the service.

ORDERED

1. That the proposed service delivery model and associated subsidised charges be approved for residential pest control:
 - a) A chargeable residential pest control service be reintroduced as per the staffing proposal.
 - b) A subsidised charge for pest control be set at £15 per visit, being comparable with neighbouring local authorities and approximately 50% lower than some external providers. That would not apply to people who were in properties of social registered landlords.
 - c) That payment must be made prior to each visit.
 - d) That charges be reviewed in 12 months to assess demand and sustainability of the service.

REASON

A chargeable residential pest control service needed to be reintroduced due to increased demand for the service and the fact that the budget had been allocated via the budget setting process at Full Council on 23 February 2022.

The allocation of the £90,000 budget, which was approved at Full Council on 23 February 2022, allowed the Council to provide a subsidised service with charges set at £15 per visit in order to ensure that the service was deliverable and sustainable.

Payments were required prior to each visit, to reduce the potential of bad debt.

21/135

TEES ADVANCED MANUFACTURING PARK - NEXT PHASE - PART A

The Executive Member for Regeneration, the Executive Member for Environment, Finance and Governance, the Director of Regeneration and Culture and the Director of Finance submitted a report for the Executive's consideration. The purpose of the report was to propose the development of a second phase at Tees Advanced Manufacturing Park (TeesAMP), funded by Middlesbrough Council (£8.82m), subject to complementary funding from Tees Valley Combined Authority (TVCA); and, an additional £335k to balance the first phase development budget.

In March 2022, Cushman & Wakefield Debenham Tie Leung (C+W) completed a two part review on the first phase development value and demand within the light-industrial sector moving forward.

Summary development disposal valuations were set out in Part B of the submitted report.

Overall, the report concluded that the Council's original £12.523m investment in the first phase had realised a significant uplift in asset values, and was simultaneously providing considerable revenue income. That had stimulated investment in Middlesbrough, uplifted land values and had incentivised investment in nearby land and property assets.

Further development therefore provided an opportunity to secure additional capital receipts, or, substantially increased asset valuations coupled with additional annual rental income to support the Council's financial position more generally.

OPTIONS

A summary of the options were tabulated below:

Options	Strategic Fit	Affordability & Value for Money	Conclusion
Do Nothing	No - Lost opportunity and negligent use of a strategic asset.	No - A dormant site will require revenue for maintenance.	Discard
Asset Disposal	No - Risk of lower quality of development and denuded corollary benefit realisation.	No - A sale would generate c. £2m capital receipt, with no future rental income benefit.	Discard
Council funded second phase development	Yes - Ensuring a high quality of development, with corollary benefit realisation	No - The Council would assume all risk mitigation and increase the likely schematic break-even point.	In abeyance
Council and TVCA funded second phase development	Yes - Ensuring a high quality of development, with corollary benefit realisation.	Yes - Reduces risk exposure whilst improving asset value by £3.8m and generating income.	Proceed

ORDERED

1. That the information contained in Part A of the report be noted.
2. That the decision be taken once all the financial or exempt information contained in Part B of the report has been considered.

REASON

The detailed business case coupled with C+W Update Report concluded that a second phase of development at TeesAMP:

- a) represented a proportionate cost for the development;
- b) provided a reasonable risk and return profile for the Council; and
- c) provided the opportunity to realise substantial corollary benefits, as highlighted in paragraphs 25, 26 and 30 of the submitted report.

21/136 **EXCLUSION OF THE PRESS AND PUBLIC**

The resolution to exclude the press and the public was agreed.

21/137 **EXEMPT - TEES ADVANCED MANUFACTURING PARK - NEXT PHASE - PART B**

The Executive Member for Regeneration, the Executive Member for Environment and Finance & Governance, the Director of Regeneration and Culture and the Director of Finance submitted a report for the Executive's consideration.

ORDERED

1. That the recommendations of the report be approved.

REASONS

The decision was supported by the following reason:

For reasons outlined in the report.

The decision(s) will come into force after five working days following the day the decision(s) was published unless the decision becomes subject to the call in procedures..

MIDDLESBROUGH COUNCIL	
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Report of:	Executive Member for Regeneration - Councillor Eric Polano Director of Regeneration and Culture - Richard Horniman
Submitted to:	Executive
Date:	7 June 2022
Title:	Adoption of Stainsby Country Park and Masterplan
Report for:	Decision
Status:	Public
Strategic priority:	Physical environment
Key decision:	Yes
Why:	Decision(s) will have a significant impact in two or more wards
Urgent:	No
Why:	

Executive summary	
<p>Adopting the Stainsby Country Park and Masterplan will protect the best interests of Middlesbrough, this report outlines.</p> <p>It is recommended that Executive adopts the Masterplan which has been amended following public consultation.</p> <p>Adopting the Masterplan will give the Council the best possible control over the development of the land between Acklam and the A19.</p> <p>This is crucial as the first of a number of planning applications relating to the site has already been submitted and is currently under consideration.</p> <p>The Masterplan sets standards for a country park alongside new community facilities, sports facilities and play areas, as well as hundreds of new homes.</p>	

The new housing is necessary to deliver the required number of homes by 2029.

The aim of adopting the Masterplan is to give the Council more sway when considering planning applications. The vision for a country park would support nature, wildlife, trees and healthy living, and be key in creating a high quality place to live.

Not adopting the Masterplan would leave the Council with limited power to influence how the area is developed, including the design of any roads.

Without a Masterplan, private developers would have more freedom to develop housing and infrastructure – including a road – as they see fit. This is because while the land is allocated for housing in the Council's Local Plan, adopted in 2014, the Local Plan does not contain sufficient information to control the level of detail the Council wants to achieve.

The Local Plan is the main document that will be considered in any application. The masterplan supplements the Local Plan and sets out how the Council wants to see any development delivered.

Executive is not being asked to approve housing or a road, simply the design standards and principles that developers would be expected to meet.

The Masterplan has been amended to make clear that as much as possible of Mandale Meadow will be retained in its current condition and incorporated into the wider country park. If the Masterplan is implemented it will lead to a significant increase in high quality and accessible open space for the community thanks to the country park.

The report also recommends that the Executive delegates authority to the Director for Regeneration, in consultation with the Executive Member for Regeneration, to make any subsequent minor amendments to the Stainsby Country Park and Masterplan.

The implications of the recommendations have been considered by the appropriate officers of the Council and are set out in the main body of the report.

Purpose

1. In preparing the Masterplan, the Council is not seeking to develop the Stainsby site. Nor is it seeking to introduce a link road. The aim of the Council is to ensure the best possible development when the private sector bring their scheme(s) forward.
2. The report seeks to adopt the Stainsby Country Park and Masterplan, which has been finalised following public consultation, and delegate authority to the Director for Regeneration, in consultation with the Executive Member for Regeneration, to make any subsequent minor amendments to the Stainsby Country Park and Masterplan.

Background and relevant information

3. The Executive approved the Stainsby Country Park and Masterplan for the purpose of public consultation in December 2020. As detailed in that report, the Masterplan is a culmination of a significant amount of background work and numerous public consultation events following the adoption of the site as a housing allocation in the Local Plan 2014.
4. Following its approval, the Stainsby Country Park and Masterplan was initially subject to public consultation between 1st February and 1st March 2021. However, due to a minor, short-term technical issue with the externally-hosted consultation portal, the closing date for the consultation was extended to 5th March 2021 in order to ensure that nobody was disadvantaged as a consequence of this.
5. Due to the Covid-19 lockdown restrictions, the entire consultation was undertaken remotely, using the approaches set out below:
 - Emails and letters were sent to statutory consultees, and other consultees included within the Council's planning consultation database (1,501 in total);
 - Letters were sent to addresses living within the site or in close proximity to the site, as would be the case for planning applications (731 in total);
 - Site notices were placed in prominent locations within and adjacent to the site;
 - A press release raised awareness of the consultation;
 - The Council's social media channels were utilised to raise awareness of the consultation. A total of 57,131 people engaged with campaign organically via the Council's Facebook, LinkedIn and Twitter pages. In addition, a paid sponsored campaign was also conducted to reach people living in Middlesbrough specifically, and reached a total of 62,283 accounts via Facebook, Twitter and Instagram;
 - The Masterplan, and various background and informative documentation, was placed on the Council's website and consultation portal, with hard copies being made available upon request. A total of 3,506 website visits were recorded; and
 - Planning officers have been available to discuss the briefs via telephone and email throughout the consultation period.

What is a Masterplan?

6. Masterplans are valuable tools in the planning and development process. The National Planning Policy Framework (NPPF) emphasises that good design is a key aspect of sustainable development, and that design guides (such as masterplans) should be prepared to provide clarity over design expectations and a framework for creating distinctive places with a consistent and high quality of design.
7. The Council uses masterplans to aim for the best possible design for development sites that are already identified in the Local Plan by expanding on established policy requirements. They set out detailed design guidance that the Council expects to achieve and are used as a basis for negotiating with developers to achieve high quality schemes. Masterplans also tie in with a national planning push for developments to meet local standards of beauty, quality and design.
8. The Stainsby Country Park and Masterplan is ambitious, requiring future developments on the site to incorporate a top quality country park, a large proportion of green space and community facilities, alongside new housing. It aims to try and safeguard against a poor quality development that is not reflective of local need. Poor quality and ill-thought-out developments are a real risk if there is no masterplan in place. A recent example of a masterplan working to positive effect is in Nunthorpe, where a planning application did not meet its expectations and planning permission to build was refused on design grounds. A subsequent appeal to the planning inspector was also dismissed due to design considerations.
9. It is important to recognise the limitations of what masterplans can achieve. Critically, they cannot rewrite or delete Local Plan policies. Local Plans are prepared using an extensive evidence base and subject to independent examination before being adopted by the Council. Masterplans can only expand on how the Council wishes to see existing policies delivered on Stainsby if, and when, private landowners or developers decide to bring the site forward for development.
10. Masterplans are also unable to dictate what will be submitted in planning applications, and do not give certainty to a specific development going ahead. Whilst masterplans will be taken into consideration, the Local Plan policy remains the primary consideration and applications that are aligned to the policy will normally be approved.
11. There are certain aspects of the development process that masterplans cannot control, such as:
 - The decision of landowners to sell private land;
 - Private developers purchasing land;
 - Private developers making a planning application;
 - Recommendations and requirements made by statutory consultees, such as Highways England; and
 - Planning permission being granted by the planning committee.
12. Ultimately, the decision to bring Stainsby forward for development, and the delivery of the link road, will be one taken by private landowners. The Masterplan identifies the preferred location of a road should it be required. Any planning applications for the site

would need to consider the design of the development, the impact of traffic and the necessity of a road.

13. The Masterplan provides the Council with a detailed design document that gives the best opportunity to ensure the quality of development and provides a strong basis against which negotiations with developers can take place.
14. Should the Masterplan not be adopted, the authority will have a significantly reduced level of control with regards to the design of the development, and the alignment and design of the road.

Summary of Consultation

15. Responses were received from 268 individuals and organisations. The key issues raised during the consultation, and the Council's response to these, are set out below. Whilst a significant number of the comments received were objecting to all or part of the Masterplan, and principally against the inclusion of a link road (by far the biggest issue), a large number also had positive comments to make on how to improve the Masterplan. These have been taken on board as appropriate and summarised below. It should also be noted that there was also a sizeable portion of people commenting who were in support of the road and the proposals in the Masterplan.
16. A significant number of responses did not specifically relate to the Masterplan, but other Council documents such as the Local Plan and its background evidence. It is not appropriate to address these issues through this report. Instead these matters will be picked up through the preparation of the new Local Plan.
17. Similarly, a number of responses highlighted issues that will need to be taken into account through separate processes at a later date, such as through future planning applications and the Country Park Management Plan. As such, this report will not specifically address these issues, but they will be considered as part of the preparation of the Management Plan.
18. Full details of the consultation responses are set out in the Report of Consultation (Appendix 1).

E-Petition

17. Between 14th Jan 2021 and 14th April 2021 an e-petition calling for 'No Road through Mandale Meadow' was submitted to the Council with 1,658 signatories. This petition was opened before the consultation on the Masterplan commenced and closed after the consultation ended. As such it cannot be considered as part of the consultation responses to the Masterplan. A number of the issues raised by the petition were, however, raised by comments received to the consultation exercise on the Masterplan, and as such are addressed in this report. These were namely:
 - A road and country park are not compatible;
 - The character of Mandale will be forever changed and blighted;
 - The road would be contrary to achieving a zero/reduced carbon footprint; and
 - There should be no road exit to the east side of the Stainsby development through Mandale Meadow.

Link Road

19. The issue that generated the highest number of responses and dominated the consultation exercise was the inclusion of the link road within the Masterplan. Whilst some people were supportive of both the principle of having a link road and the preferred route, others submitted objections. Many of the objectors simply did not agree that a road linking through to the Mandale junction was necessary or desirable, with the impact on Mandale Meadow being cited as the main concern. Many of these objectors suggested that a loop road, with the road coming back out on to Low lane, was preferable.
20. Those who indicated their support for the link road recognised that the alternative options had already been considered and that it would be necessary to support the housing development. It was also recognised that the link road was part of the wider package of infrastructure that would benefit the wider area.

Key Issues Raised

- No Road/No Road through Mandale Meadow;
- Alternative Road - A19 Overpass;
- Alternative Road - A19 Slip Road;
- Alternative Road - Loop Road;
- Loop road would put too much pressure on Low Lane/Acklam Road;
- Full business case should be prepared for each option;
- Issues with Speed Limit and Road Safety;
- Scheme will Cause more Traffic & Congestion;
- Supportive of the link road;
- Improvements need to Mandale/A1130 roundabout;
- Road is too wide;
- Issues with existing Jack Simon Way; and
- Impact of road on Local Wildlife Site.

Response to the issues raised

21. The link road is part of the adopted Local Plan policy for the site. Various studies have concluded that the provision of a link road connecting the A1130 to the B1380 is essential not only to serve the new housing development at Stainsby, but also to provide additional capacity to the local road network to support the wider housing growth programme set out in the Local Plan. A number of those that support the road do so on the basis of the wider benefits that it will deliver for the surrounding areas.
22. Highways England were consulted on the Masterplan. In their response they have made no commitment to the provision of the link road, only recognising that further information is required to fully assess its impacts upon their network. Highways England have, however, recognised that a key stress point on their network is the A19/A174 junction and, as such, they will resist proposals which add to that stress, and are seeking mitigation measures that will help alleviate these issues.

23. Differing alignment options, as have been suggested, have been assessed independently by external Transport Consultants. This assessment utilised the Department for Transport's Early Assessment and Sifting Tool (EAST) and using a scoring methodology established that a through route from Low Lane to Mandale Road was the most appropriate solution. The Masterplan follows this recommendation and sets out the Council's preferred alignment for the link road, running along the western part of Mandale Meadow.
24. The most 'popular' alternative route option was a loop road that provides a connection to the B1380 only. This option has already been assessed and discounted as it will not provide sufficient capacity for the planned housing development and it would exacerbate the congestion problems at the A19/A174 junction. Some objections were received highlighting the pressure this option would place on Low Lane and Acklam Road.
25. Furthermore without a through road the development would become a cul-de-sac which is not generally conducive to running viable bus services. The lack of high quality public transport would fundamentally undermine the sustainable aspirations of the development which will detrimentally impact the authority's highways strategies in managing demand on the network and will increase car dependence. This increased dependence on private car travel will affect wider areas of Middlesbrough, increasing queuing, delay, reducing air quality and the health of the town.
26. The alternative route across the A19 was also put forward through the consultation. Again, this has already been assessed and discounted due to the significant additional costs associated with its construction, and the increased visual intrusion for existing housing. In addition, some of the route would lie outside of the Middlesbrough Council area, meaning the Council would not be in a position to ensure its delivery.
27. The other additional suggestion was to restrict traffic on the link road to electric/hydrogen/public transport only. This option would not address the primary issue of congestion, particularly at the A19/A174 junction, but also on the wider local road network including at Marton Road and Acklam Road. It would also be difficult to enforce such a route without expensive infrastructure, which would become redundant within a relatively short space of time as car manufacturers phase out petrol and diesel engine production over the next 10 years or so. Potentially, such a solution could also increase pollution levels to unacceptable levels on other key routes in Middlesbrough.
28. Legal advice provided to the Council was clear that the issue of the road is a key policy consideration and one that the Masterplan cannot ignore. It is, therefore, necessary to include the road within the Masterplan, and appropriate to show a preferred route that provides the best opportunity to control development once planning applications are submitted.
29. In summary, no additional viable options for the link road have been put forward during the consultation, and it is considered that the existing option of creating the link road along the western part of Mandale Meadow should remain within the Masterplan in accordance with the adopted Local Plan. It would be for any future planning application to ultimately consider the impact of traffic and the need for a road. The proposed route has been aligned as far to the west of the site as possible to maximise the distance from adjacent residential areas.

Country park/green space/play areas/nature/habitats

30. The impact of the Masterplan on the provision of green space was another key consideration for those responding to the consultation. Many people focused on the proposals affecting Mandale Meadow, both in terms of the loss of land and the impact on its current character, including its ecological value, and the potential conflict between the road and wildlife. Some simply did not want a country park, and others highlighted concerns about specific elements of the approach set out in the Masterplan.
31. Others recognised the potential benefits of the proposals, including a higher overall amount of public open space provision, better play facilities for children and new sports facilities. Others recognised the opportunities that could be brought about through the Masterplan, such as opportunities to enhance meadows and incorporate new habitats into sustainable drainage schemes, and other ways in which the biodiversity of the area could be enriched for the benefit of the local and wider communities.

Key Issues Raised

- Loss of greenspace;
- Nature and habitat destruction/impact of road on local wildlife site;
- New planting should be with native species;
- Paths are too wide / unnecessary;
- Lighting of Streets will be bad for nature;
- Covenant Removal;
- No play or sport provision;
- No benefit of having orchards/allotments;
- Support play/sport/youth facilities;
- Too many formal play areas identified;
- Support landscaping proposals;
- Enhancement of existing meadows;
- Retention of green space is encouraging;
- No country park needed/wanted;
- Concerns over the management of the country park;
- Concerns over Green Space Management;
- Better play facilities for Children;
- Positive towards the country park/nature reserve;
- Loss of farmland;
- Sustainable Drainage should be designed to incorporate habitats and look natural; and
- Conflict between road and wildlife.

Response to the issues raised

32. The Country Park is part of the adopted Local Plan policy for the site. Many of the respondents highlighted the value of the existing Mandale Meadow and expressed objection to the potential loss of this open space. There were also a number of respondent who were supportive of the proposals to provide a Country Park, new sport and play facilities, and habitat enhancements.

33. It is recognised that a portion of Mandale Meadow will be lost to the development of the road, and that this will have an impact on that particular open space. However, the Masterplan, in line with adopted policy, sets out the Council's commitment to mitigating this loss by substantially increasing the overall amount of public open space. The Masterplan has been amended to make clear that the remaining part of Mandale Meadow will be retained, as much as is possible, in its current condition and integrated as such into the Country Park. It is considered that through the implementation of the Masterplan that there will be a significant net gain to the wider community through the provision of a country park.
34. There were some concerns raised about specific design aspects of the country park, in particular the provision of footpaths and their proposed widths being too wide, thus leading to unnecessary loss of green space and impact upon wildlife and ecological habitats. Footpaths could also not be in keeping with some of the existing areas, bringing an artificial, man-made element to otherwise natural areas. The indicated footpaths have been removed from Mandale Meadow and some of these other areas and the Masterplan revised to be less prescriptive regarding their nature.
35. With regards to play areas, there was support for new facilities, although some people raised concerns over their potential to become a focus for anti-social behaviour. The large number of areas identified was also questioned given that their catchments significantly overlap and, as such, could exacerbate some of the anti-social behaviour issues highlighted. The plan showing such spaces has been removed and the text of the Masterplan enhanced to require developers to provide such open space in accordance with identified requirements/standards.
36. One of the issues raised is that the sustainable drainage schemes (SUDS) should not be engineered but designed to form part of the wildlife network and contribute to the increase biodiversity of the site. The wording of the Masterplan has been strengthened to reflect this commitment.
37. Concerns were also raised over the future management of the Country Park. The Masterplan highlights the Council's commitment to take responsibility for the ongoing management once the country park has been established. The details of this have yet to be established, and will not be known until a later date. If there is insufficient existing capacity and capability for the Council to do this then additional resources will need to be procured. However, it will be necessary to bring a management plan and associated costings before Executive for further approval at the appropriate time.
38. A number of respondents raised the issue of nature and habitat destruction, and the impact of street lighting on nature. There is an existing Local Wildlife Site that will be affected by the development of the link road. This site was identified before the Local Plan was adopted, and the established approach is to ensure that there is an overall net gain to biodiversity as a result of development. Whilst it is accepted that the proposals will result in the loss of some existing habitats, the Masterplan will ensure that these will be minimal and that there will be significant overall gains to habitats and nature. It was suggested that new planting should focus on native species, and this will be considered in detail at a future date through the Country Park Management Plan. The Masterplan will also be amended to ensure that appropriate wildlife corridors that avoid any conflict with the link road be sought. Future planning applications must be subject to an ecology assessment, which will assess the existing situation and set out

details of how the development will mitigate for the loss of any habitats. Street lighting is an essential part of any development that helps to ensure human safety. The Council will commit to minimising the impact of street lighting on nature by seeking habitat creation away from areas where street lighting is necessary.

39. Whilst some people thought the creation of orchards and allotments were good ideas, a number questioned the demand for these and raised questions about how they would be managed. In particular, concerns were raised that fruit trees would be susceptible to vandalism. Others thought that the local population's needs were already sufficiently catered for through orchards at Nature's World and Larchfield.

40. In summary, the Masterplan has been amended to clarify that the remaining part of Mandale Meadow and other established wildlife habitats will be retained, as much as is possible, in their current condition. In these areas it has been necessary to remove footpaths and be less prescriptive over their widths. Additionally, the Masterplan has been amended to:

- be clearer on the provision of formal play areas;
- strengthen wording to reflect the commitment to incorporate habitats into SUDS;
- seek to establish appropriate wildlife corridors to avoid any conflict with the link road; and
- minimise the impact of street lighting on nature by seeking habitat creation away from areas where street lighting is necessary.

Design/layout/facilities/general details

41. The purpose of the Masterplan is to set out a vision, and detailed guidance, for how the Council would like to see the Stainsby site developed out. The overall aim is to ensure development of high quality, with well-designed homes and layouts that are conducive to a healthy and happy life. The Country Park will be a substantial area of green space, which will have multiple functions and serve the wider population of Middlesbrough. The Council considers that the approach to setting out these requirements in a masterplan will provide a greater level of certainty over what will be delivered.

42. Many of the responses recognised the value in the Masterplan-led approach to securing good quality development with the necessary infrastructure and facilities to support it. Others raised concerns over some of the detailed designs illustrated in the document. A key point raised was the level of detail set out in the Masterplan which could have an impact on bringing good quality development forward should circumstances change.

Key Issues Raised

- Masterplan is too detailed/inflexible which could prevent alternative, good quality design coming forward;
- Insufficient consideration given to the historic environment, including impact on Stainsby medieval village and the wider archaeological interest in the site;
- Garages are too small;
- Gardens are too small;

- Boundary treatment concerns;
- Improved housing design is needed;
- Suggested Green Energy Housing;
- Security/safety issues;
- Privacy concerns;
- Covid Pandemic means rethink needed;
- Risks of antisocial behaviour;
- Inadequate consultation undertaken/plans are confusing;
- Provides a benchmark for future development;
- Positive towards the overall development;
- Positive towards the provision of new school and amenities, proposed in right location;
- Job creation;
- Boundary should be extended to include application site for new retail at Low Lane;
- Timing of delivery of new facilities;
- Agree overall but would prefer an alternative route for the link road;
- Provision of an independent access for farm vehicles from Stainsby Hill Farmstead to A19 bridge to enable access to farmland to west of A19; and
- Stainsby Hill Farmstead should be linked for access purposes to adjacent new housing to enable segregation from farm traffic.

Response to the issues raised

43. It is recognised that, in parts, the Masterplan sets out very detailed requirements, particularly with regards to footpaths, play areas, boundary treatments, feature walls, and the widths of roads, pavements, gardens etc. Whilst the Masterplan aims to strike a balance between being prescriptive and being flexible, both developers and members of the local community highlighted the drawbacks of including very detailed requirements. We have, therefore, re-considered our approach and have concluded that some changes can be made to ensure more flexibility and less restrictive design through development, yet providing sufficient detail to guide required development and design outcomes. The changes are detailed in the 'Changes to the Stainsby Country Park and Masterplan' section below.
44. It was suggested that the boundary be extended to include proposed retail at Low Lane, currently subject to a planning application. The Masterplan includes a proposed local centre that is central to the overall Stainsby scheme, which is considered to be a more sustainable option. The proposed location is also contrary to adopted Local Plan policy, and as such cannot be considered as part of the Masterplan. It is, therefore, not recommended that the boundary be amended in this location.
45. With regards to Stainsby Hill Farmstead, it is accepted that the Masterplan doesn't properly reflect the intention to retain the existing farm buildings and access to them via the A19 bridge, and maintain access between the farmstead and land in their ownership which forms part of the holding to the west of the A19. The Masterplan will be amended to maintain access to these areas.

Health/Noise/Pollution

46. The Masterplan seeks to achieve a quality development alongside a Country Park that will provide access to high quality public green space for future and existing residents, to help improve quality of life. Some of the responses agreed that the additional green space being provided by the Masterplan would have a positive impact on people's physical and mental health.
47. A small number of people raised concerns that the development at Stainsby would result in additional pollution, including noise, whilst others highlighted the potential health impacts arising from losing green space.

Key Issues Raised

- Environmental issues – noise and pollution; and
- Mental and physical health.

Response to the issues raised

48. It is recognised that development can result in noise and environmental pollution, both in terms of the construction phase and from the use once complete. Similarly, the occupiers and users of the development can be affected by pollution. These issues can only be properly considered at the planning applications stage, where these matters will be subject to detailed assessment and consideration, taking into account existing levels of noise and other pollution. Environmental Health legislation and controls can be used to control these so they do not breach acceptable levels. It is also recognised that the health benefits associated with the provision of such a significant amount of open space significantly outweigh any negative impacts, and will not only provide benefit to the local community but also the wider area and help deliver the Council's Green Strategy and achievement of the One Planet Living objectives.
49. A number of respondents did highlight the positive benefits that green space has on people's mental and physical health. Concerns were raised that the loss of green space, and development in general, would have a detrimental impact on health. However, a key component of the Masterplan is to provide a significant increase in the amount of green space available through the creation of a country park. Whilst some losses to existing space will be necessary to accommodate the road, it is considered that the benefits of implementing the Masterplan would outweigh these drawbacks. The need for new housing development is well established and there are significant benefits to people's wellbeing through the provision of good quality new housing.

Speed limit/parking/public transport/pedestrian & cycling

50. The Masterplan sets out some key principles regarding layouts, roads, pedestrian and cycle links, and public transport. The overarching aim is to create a sustainable development that is not dominated by the private car, provides facilities and infrastructure in a central location that minimises the need to travel, and provides the opportunity to use non-car modes of travel that provide links to the wider area.

Key Issues Raised

- Reduce Speed Limit;
- Meadow Car Park;
- Existing parking issues on Jack Simon Way;
- Off Street Parking to Stop Traffic Congestion;
- Cycle route should be extended to include JSW;
- Support public transport;
- Bus Route Dependant on Bus Companies;
- No Buses through Estate;
- Road is too narrow to accommodate buses;
- Lack of Public Transport;
- Support pedestrian and cycling infrastructure;
- Discourage Vehicle; and
- Electric vehicle Charging Points.

Response to the issues raised

51. Some people questioned whether the 30mph speed limit for the primary road was appropriate for a residential area, whilst others sought confirmation that 20mph would be applied through the scheme. A hierarchy approach to the highway network has been taken from primary through to tertiary routes. At each stage the function of the highway changes and becomes more informal from a traffic movement priority to a sense of place. The primary road will not have direct frontage access but will still include design measures to restrain vehicle speeds. Reducing the primary route to 20mph would diminish the impact of implementing lower speed limits (20mph) on adjacent residential roads served from the primary route. This approach is consistent with what can be seen elsewhere on the network.
52. It was suggested that the car park shown at Mandale Meadow wouldn't be suitable for all users, particularly as it would be necessary to cross the link road to access the main part of the Country Park to the south of the road. However, locating the car park to the south of the road would take up more meadow space. The exact location and details of proposed car parking will form part of detailed planning applications. However, the Masterplan proposes numerous high quality crossing points which seek to restrain vehicle speeds, enable pedestrians/cyclists to safely cross in two movements and will link car parking to the Meadow and the rest of the Country Park.
53. Some responses suggested that the proposed cycle route should be extended to include the existing section of Jack Simon Way. The existing development served from this road was brought forward without a Masterplan so limited space is available making it difficult to retro fit cycle facilities. However, this is a priority for the authority and options will be considered to ensure that continuous cycle facilities are provided between Mandale Road and Low Lane.
54. Similarly, the issue of on street parking along Jack Simon Way was raised. As the existing development was brought forward without masterplan this is not something that can easily be changed. New development will not have the same impact on Jack Simon Way if implemented in accordance with the masterplan. Traffic Regulation

Orders or other measures could be implemented on the existing section of Jack Simon Way, should they be deemed necessary.

55. Public transport was also raised, with some people supporting bus routes and others disagreeing. Some people questioned whether bus companies would want to route buses through the new development, which we think they will given the road layouts in the Masterplan. It was suggested that the roads would be too narrow to accommodate buses, but the width of both the existing sections of Jack Simon Way, and the proposed link road, are in accordance with national guidance to allow two-way bus movements.

56. It was suggested that the Masterplan should include measures to discourage vehicle use. It is considered that the Masterplan and highway layout have sought to enable all future residents to be within nationally recognised walking distances of public transport and, together with other sustainable initiatives (including the design of the layout), will seek to promote sustainable travel. Connectivity has been planned into the Masterplan to enable adjacent residential areas to easily access services and facilities within the scheme. Such an approach could increase patronage on bus services and integrate the development into surrounding areas, increasing the attractiveness of and commercial viability of services to operators. The Masterplan seeks to facilitate the delivery of a highly sustainable community and as such public transport forms one of many options in reducing dependence on the private car.

57. Requiring electric vehicle charging points was also suggested. The Council will look to secure the installation of charging points within car parks. Responsibility for electric vehicle charging within residential properties will be the responsibility of the housebuilders, and the Council will proactively work with them and support the provision of such infrastructure.

Need for housing/greenfield vs brownfield development/house value reduction

58. The Stainsby development is a key component of the Housing Local Plan, adopted by the Council in 2014. It is a strategic development site that, along with a package of other housing sites across the borough, aims to deliver the identified housing requirement to 2029. The overall level of housing being delivered in Middlesbrough is important not only to meet identified needs, but also to support the wider economic growth objectives of the area.

Key Issues Raised

- Housing numbers in the Local Plan should be revisited;
- No more house building required;
- Resistance to greenfield development/brownfield development should be pursued instead;
- House Value Reduction; and
- Developers Control / Local Plan Number Increase.

Response to the issues raised

59. The overall need for additional housing is established in the Local Plan, and the site is allocated for the uses set out in the Masterplan. Therefore, this issue is beyond the

scope of the Masterplan consultation. Furthermore, recent research undertaken on behalf of the Council for the Local Plan review has concluded that 400 dwellings per annum will be required across the borough to meet our needs. This figure will be examined at the Local Plan examination, along with all the evidence and representations made on the issue. The site at Stainsby is an established strategic site that will make an important contribution to meeting existing and future housing needs.

60. The Council is committed to delivering new housing on brownfield sites wherever possible. However, there is insufficient brownfield land that is available and deliverable for housing development. Of the land that is available, not all of it is appropriate for all types of housing. As such, greenfield land is vital for meeting the quantity and range of housing that is required.

61. The value of existing houses, and the impact that new development has on their value, is not a material planning consideration. In any case, the value of property is dependent on a range of factors, and it is not possible to quantify the specific impact that new development has on values.

Summary of Changes to the Stainsby Country Park and Masterplan

62. A number of key changes have been made to the Masterplan as a result of the consultation, primarily to enhance the wildlife credentials of the site and strengthen the Council's ability to achieve a high quality of design. These are contained in the revised Masterplan attached at Appendix 2, and are summarised below:

- Remove some of the very detailed requirements, providing more flexibility;
- Remove the plan showing the provision of play areas but in doing so strengthening the text to ensure that required standards and provision are met throughout the development leading to a more balanced approach to providing green space to support the new development;
- Remove footpaths as shown within the Mandale Meadow, Blue Bell beck, Saffwood Beck, and Acklam Meadow areas, and be less prescriptive over their requirements and nature;
- Remove references to specific sizes shown in the cross-sectional diagrams regarding the size of dwellings, parking, roads, pavements, gardens etc.;
- Require that Sustainable Drainage seek to maximise the use of natural features incorporating ecology and habitats, rather than seeking hard engineered solutions;
- Seek solutions to establish appropriate wildlife corridors to avoid any conflict with the link road;
- Minimise the impact of street lighting on nature by seeking habitat creation away from areas where street lighting is necessary;
- Providing reassurance that existing cherished areas such as Mandale Meadow and Bluebell Beck will be retained in their current state as much as is possible and incorporated into the Country park whilst achieving key aspects of the overall development;
- Removing the specific design options for feature walls;
- Incorporating and protecting the access routes between Stainsby Hill farm and the remaining agricultural land holdings to the west of the A19;

- Encouraging electric vehicle charging in homes and the provision of charging facilities within public car parks;
- Providing car parking to both the north and south of the link road at Mandale Meadow; and
- Including details of what the submission requirements will be for any planning applications.

63. In addition it is proposed to tidy up the Masterplan by addressing typographical errors, and remove inconsistencies.

Status of the Masterplan

64. The draft Masterplan was approved as 'Design Guidance' that would build upon and provide more detailed guidance about policies in the Local Plan. Consultation was undertaken in accordance with the standards for Supplementary Planning Documents (SPD) as set out in the Council's Statement of Community Involvement, except where it was necessary to comply with the Coronavirus Regulations that were in force at that time.

What decision(s) are being recommended?

That the Executive:

1. adopts the Stainsby Country Park and Masterplan Supplementary Planning Document, to enable the Council to control development proposals brought forward by private sector landowners and developers in the area; and
2. delegates authority to the Director for Regeneration, in consultation with the Executive Member for Regeneration, to make any subsequent minor amendments to the Stainsby Country Park and Masterplan.

Rationale for the recommended decision(s)

65. The Stainsby Country Park and Masterplan is a key Council document that reflects our ambitions for high quality housing alongside the provision of a country park. The Masterplan will help to clarify the Council's expectations for the delivery of a high quality development scheme whilst, at the same time, reducing risk and uncertainty for future developers.

Other potential decision(s) and why these have not been recommended

66. **Not to adopt the Stainsby Country Park and Masterplan.** This will mean that there will be an absence of guidance on the expectations for the country park and the key layout principles, type of housing and quality of development that the Council will be seeking at Stainsby. There will be less certainty for future developers and a potentially higher risk that the country park would not be delivered.

Impact(s) of the recommended decision(s)

Legal

67. The Stainsby Country Park and Masterplan sets out the Council's expectations for the development at Stainsby. There are no statutory requirements relating to the preparation of such guidance.
68. Consultation has been undertaken in accordance with the legislation and latest Government guidance on undertaking public consultations in view of Covid-19.
69. Once adopted, the Stainsby Country Park and Masterplan will become a material consideration in the determination of planning applications for the site.

Strategic priorities and risks

70. The Stainsby Country Park and Masterplan will have a positive impact on the Council's risks. It will support the delivery of the Council's housing growth programme, which is critical for the successful delivery of the MTFP (O1-051 and O1-045).
71. Should the scheme not be approved, it could create a pressure on the 5 year land supply, and lead to unallocated sites outside of the Local Plan being developed.
72. If poor economic growth occurs, then this will reduce public and private sector investment in the town and the development of Stainsby will be at risk (Risk: 01-005).
73. It is proposed in the Local Plan that Stainsby will deliver a mix of dwelling types including 3-4 bed detached and semi-detached dwellings, aiming to deliver housing to help retain its economically active population. If insufficient Council Tax Band D+ properties are built this may result in further population decline which will impact upon the Medium Term Financial Plan (Risk: 01-029).
74. The high level risks mentioned above, which are identified in the Strategic and Directorate Risk Registers, will be reduced as a consequence.

Human Rights, Equality and Data Protection

75. The Stainsby Country Park and Masterplan has been subject to an initial Impact Assessment (IA), which accompanies this report (see Appendix 3). This identifies that a full IA is not necessary.

Financial

76. The costs associated with the preparation of the Stainsby Country Park and Masterplan, and the associated consultation, have been met from established departmental budgets.
77. It is anticipated that the Masterplan will help deliver development that will broaden the Council Tax base and increase revenue. This is critical for the financial sustainability of the Council and will help achieve the objectives of the Medium Term Financial Plan.

78. The housing development aspect of the Masterplan will be brought forward by private developers. The wider infrastructure costs, including the country park and the link road, will be funded through financial contributions from the housing developers of the Stainsby site, developers of other housing sites in the south of the borough that will benefit from the provision of this infrastructure, and already secured S106 contributions. Where possible this will be enhanced through taking advantage of grant opportunities as and when they arise.
79. The Country Park Management Plan, including associated costings, will be the subject of future consideration and approval by Executive.
80. With the introduction of net biodiversity gain, the Country Park provides an opportunity to secure funding from other developments to increase and enhance biodiversity value, further increasing the value of the Country Park to local communities.

Actions to be taken to implement the recommended decision(s)

Action	Responsible Officer	Deadline
Once adopted, the Stainsby Country Park and Masterplan will be a material planning consideration in the determination of any future planning applications and will carry significant weight in the decision making process. The document will be placed on the Council's website.	Paul Clarke, Head of Planning	June 2022

Appendices

1	Report of Consultation on the Stainsby Country Park and Masterplan
2	Stainsby Country Park and Masterplan
3	Initial Impact Assessment of the Stainsby Country Park and Masterplan

Background papers

Body	Report title	Date
Middlesbrough Council	Adoption of Stainsby Country Park and Masterplan (no decision made)	June 2021
Middlesbrough Council	Stainsby Country Park (formally known as Stainsby/Stainsby Detailed Masterplan)	December 2020
Middlesbrough Council	Stainsby Draft Masterplan and Next Steps	January 2019
Middlesbrough Council	Stainsby Masterplan (including Stainsby North)	September 2018

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Report of Consultation on the Stainsby Country Park and Masterplan

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Introduction

1. The Executive approved the Stainsby Country Park and Masterplan for the purpose of public consultation in December 2020. As detailed in that report, the Masterplan is a culmination of a significant amount of background work and numerous public consultation events following the adoption of the site as a housing allocation in the Local Plan 2014.
2. Following its approval, the Stainsby Country Park and Masterplan was initially subject to public consultation between 1st February and 1st March 2021. However, due to a minor, short-term technical issue with the externally-hosted consultation portal, the closing date for the consultation was extended to 5th March 2021 in order to ensure that nobody was disadvantaged as a consequence of this.
3. Due to the Covid-19 lockdown restrictions, the entire consultation was undertaken remotely, using the approaches set out below:
 - Emails and letters were sent to statutory consultees, and other consultees included within the Council's planning consultation database (1,501 in total);
 - Letters were sent to addresses living within the site or in close proximity to the site, as would be the case for planning applications (731 in total) (see Appendix 1);
 - Site notices were placed in prominent locations within and adjacent to the site (see Appendices 2 and 3);
 - A press release raised awareness of the consultation;
 - The Council's social media channels were utilised to raise awareness of the consultation. A total of 57,131 people engaged with campaign organically via the Council's Facebook, LinkedIn and Twitter pages. In addition, a paid sponsored campaign was also conducted to reach people living in Middlesbrough specifically, and reached a total of 62,283 accounts via Facebook, Twitter and Instagram;
 - The Masterplan, and various background and informative documentation, was placed on the Council's website and consultation portal, with hard copies being made available upon request. A total of 3,506 website visits were recorded; and
 - Planning officers have been available to discuss the briefs via telephone and email throughout the consultation period.
4. Responses were received from 268 individuals and organisations. The key issues raised during the consultation, and the Council's response to these, are set out below. Whilst a significant number of the comments received were objecting to all or part of the Masterplan, and principally against the inclusion of a link road (by far the biggest issue), a large number also had positive comments to make on how to improve the Masterplan. These have been taken on board as appropriate and summarised below. It should also be noted that there was also a sizeable portion of people commenting who were in support of the road and the proposals in the Masterplan.
5. It should be noted that a significant number of responses did not specifically relate to the Masterplan, but other Council documents such as the Local Plan and its background evidence. It is not appropriate to address these issues through this report. Instead these matters will be picked up through the preparation of the new Local Plan.

6. Similarly, a number of responses highlighted issues that will need to be taken into account through separate processes at a later date, such as through future planning applications and the Country Park Management Plan. As such, this report will not specifically address these issues, but they will be considered as part of the preparation of the Management Plan.
7. Full details of the responses received are set out in the rest of this report.

Question 1 – In this section of the form, should you wish to suggest an alternative road alignment that has not already been considered by the Council, please provide details in the comment box below

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Alexander Henegan				Clearly, under the plan a road is required but its route linking to the Mandale Roundabout will result in a huge traffic problem for the Acklam area. Access to the A19 and journeys to Thornaby will be severely affected. I don't know what "strategic mitigation" means and find it an unhelpful phrase. I am unable to suggest an alternative route but remain adamant that traffic problems will be exacerbated by the Stainton Way Western Extension.
Adam Merritt				<p>I am a resident of FARMSTEAD STREET on the new Stainsby Grange site in Acklam. I really like the development so far and am looking forward to seeing it progress in the coming years.</p> <p>However, I have two concerns. My first is quite simple. I don't want the Central hub to become a magnet for trouble with a massive pub chain and supermarket coming in. I think it should be a more local / community hub with small businesses in keeping with a village feel.</p> <p>My second and main concern is traffic and parking on Jack Simon Way (JSW). Your master plan states It is considered a Primary Road and has a max speed limit of 30 mph with a natural design to slow traffic. This is not working, with vehicles regularly travelling much faster, even without the through road aspect. This will become worse with traffic trying to beat the A19 or avoid Acklam Road when it links up with Mandale Road. You need to start considering traffic calming measures before somebody gets injured.</p> <p>In addition to ere are multiple vehicles parking on both sides of JSW outside the Belway Homes development. This part of JSW is already narrower than further down and due to visibility coming down the hill to the Beck crossing, it is causing motorists to take evasive action to avoid collisions, especially at night.</p> <p>As the masterplan below states on 6.0 URBAN STRATEGY, 6.7 Parking, all houses have at least two parking spaces and there is to be no parking on a Primary Road which JSW is. With this in mind could the council please follow their own guidelines and make sure nobody is parking on JSW before an accident occurs. This could be done by placing double yellow lines on the road. I know it's not a permanent surface, but it would solve the problem until it's adopted and get residents used to parking elsewhere.</p> <p>If the road is not yet adopted by the council, then the council needs to make the construction companies responsible and hold them to account for any failings. I don't wish to point it out, but residents on this estate are paying a very high rate of council tax and are not seeing much in return.</p> <p>I look forward to hearing from you in response.</p>

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Adrian Walker				<p>No road through Mandale!</p> <p>I write to object to the Stainton Way Western Extension as in my view a Country Park and Spine Road are not compatible. I am concerned about the impact on the existing residents and wildlife by the imposition of this new road.</p> <p>The 2014 infrastructure plan is well out of date and the exclusions are as significant as the non-delivered inclusions:</p> <p>https://www.middlesbrough.gov.uk/sites/default/files/PlanLib-Infrastructure_Delivery_Plan.pdf</p> <p>If the decision is made to approve the road in the face of strong public opposition, this should be limited to Local Traffic Only with a speed limit of 20mph as per the post on Facebook by The Mayor:</p> <p>“to make this new country park possible a new road could be necessary and it would join Mandale Rd near the A19. The road would have traffic calming measures and would have a speed limit of 20mph.”</p> <p>I am unhappy at the idea of the road and think that having the road is NOT a price worth paying for the new facilities.</p>
Alan Jukes	Nhs			<p>This plan should not go ahead, when it says a sustainable plan this is wrong on so many levels. The diverse ecosystems within the green belt will be destroyed all because of a road. I'm deeply saddened that despite the current climate of the planet we are considering destroying ecosystems beyond humans. Why do we think we are so important over other living creatures and plants. Probably because of money grabbing political ego driven people who can't think beyond themselves and their own aims. This is another step towards the realisation that politics is corrupt and can't recognise that the world is connected beyond human beings. So sad and I hope one day if this does get passed that the people who pass this think about the impact this will have</p>
Alan Liddle	Stainton and Thornton Parish Council			<p>The SWWE as outlined in the Masterplan has been shown to be an essential part of the proposed development and the through road allowing access and egress at both north and south points is important. The idea of a loop road whereby all traffic comes into and out the proposed development at Jack Simon's Way will put undue pressure on both the Low Lane/Acklam Road route into town OR the Low Lane/A174 route towards town and the A19. The through route must only be used to support the housing development and not be used to try and take pressure off other roads including the A19. The road should be wide enough to allow it to be used freely as a bus route with cycle-ways linking to the existing network.</p>
Alan Turley				<p>Necessity: The development of a new spine road only becomes necessary if the additional phases of housing development are permitted. The current phases are adequately served by Jack Simon Way giving quick access to the A174 and A19 via Low Lane. The A174 junction of the A19 is controlled by automatic traffic signals at peak times providing for free flow of traffic. The proposed new road will run almost parallel to the existing route and from what I can see no impact assessment has been conducted on the likely congestion at the Mandale end, a route that is already congested at peak times. The proposed new route will do nothing to alleviate the flow of traffic through existing routes such as the Oval, Trimdon Avenue and Acklam Road as many of the estates children attend schools serviced by those routes.</p> <p>Road Safety: The existing portion of Jack Simon Way from the Low Lane roundabout to the junction of Hampstead Way has housing with only parking situated at the rear. As a result many residents park their vehicles along this stretch of road in some cases double parked. This makes the road narrow for vehicles travelling in either direction and presents a danger to the children in the area who are frequently playing in the vicinity. There is no scope to widen this stretch of road without removing a footpath in front of the existing houses. To increase the volume of traffic and introduce busses to the route will only serve to increase the existing level of risk to both drivers and pedestrians.</p> <p>The existing section of road is poorly lit which again adds to the level of risk during the hours of darkness.</p>

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Allison Gibbons				Loop 1 road
Amanda Stevenson				I would prefer option 1, the link road. The mandale interchange is very congested now at peak periods and the proposed road will only add to the problem. I appreciate that the A19 / A174 junction is also congested but the proposed road will not elevate the problem it will only move it. Building more houses will obviously add add to the congestion, are these houses really necessary, not according to the NAO report of February 2019.
Amy Lord				A loop road that does not exit at Mandale roundabout would be preferable.
Andrea Spooner				there are plenty of areas within the town where new housing could be built other than on an already busy highway. The health of the current residents and any prospective new residents should be considered. The extra pollution in a built up area is detrimental to the residents of Acklam and Middlesbrough. Another green area which is highly popular with the residents of Middlesbrough should not be being considered to bring forward the Council's wider housing development programme. Areas in the town should be regenerated to make them more acceptable for housing rather than them becoming no go zones. The number of houses being built in this area is not in proportion to other surrounding areas and ruining the area and community that exists in Acklam.
Andy Brown				no new road!!
Andy Emerson				I object to the destruction of Mandale meadow by the proposed road. Private housing developers should provide access to A19 / A174 by buying more private farm land and this should have been arranged before houses were built.
angela barber				AS a resident of this area, I fear my house value will lower so close to this new road. There will be No where to walk my dog There would be Little or no space for children to play safety in area An area to exercise my self and enjoy picnics with the family Local wild life will loose there home and endangered species plants and animals will loose there environment Why should be suffer ill health because of further pollution We pay council tax for green space- will our council tax lower-expect not Increased noise during the day and night- lack of sleep and depression result All you want to do is provide new access road to the estate- why cannot they come around down downside and Acklam road instead as always has happened. I feel the new road will increase traffic into Middlesbrough through our area. I do not want or need the road as I do not drive- I doubt it will be included on the bus route, so it does me no good. In fact everything about it is BAD
Angela O'Byrne				Build the houses in the many available and much needed affordable housing areas. In today's world it is essential we have green space. You are destroying our children's legacy!!
Annmarie Barry				I do not support the building of the Mandale Meadow Spine Road.
Anthony O'Malley				I'd like the council to consider option to deter through traffic, the route should ideally be for people living on the estate, rather than taxis or others using it as a 'rat route'. Perhaps significant traffic calming measure can be considered and implemented from the outset; such as raised sections, signalled crossing points etc
Barbara Keville				The noise from the A19 is already unacceptable so any additional road running parallel only going to make that worse. Has anyone carried out noise level studies? I guess not. Alternative could be a circular road back onto Low Lane.

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Barry Jobson	South Tees Green Party			<p>It is good to see from the updated consultation documents that a number of improvements to the proposed scheme that have been incorporated since the previous plans. We thank the council for listening to the concerns of local residents and naturalists that, and welcome these improvements.</p> <p>Middlesbrough Council has previously stated that Stainton Way Extension/Jack Simon Way is considered vital to further developments to the south of Middlesbrough. Development of this site will mean the immediate loss of farmland in Stainsby, and will have a knock-on effect of farmland loss in the south of borough. This will impact on the council's proposed Green Strategy, which includes locally grown food.</p> <p>South Tees Green Party stress that it is vital that land cleared in the centre of town, Middlehaven, as well as Whinney Banks and Grove Hill are developed before any further loss of green field sites.</p> <p>Completing the Stainton Way Extension will create a bypass for the A174/A19, and will bring traffic from the housing in the south of the borough through the development and country park. Traffic regularly builds up along Mandale Road all the way from the A19 Mandale Interchange to Acklam Road; and also from the A19/A66 interchange down to the A19/A174 Parkway Interchange. It is logical to expect the same levels of traffic build-up along this road, and will therefore leave residents facing unacceptably high levels of stationary traffic at peak times, with associated noise and air pollution and their impacts on both physical and mental health.</p> <p>The original section of Jack Simon Way was not built to be an A-road, but rather an estate road, and as such is much narrower than the second phase. This, along with properties with front doors facing directly onto the road, is alarming considering the volume of traffic expected to use this route, as children could run straight out of their front doors onto the road. Thus, by extending this road, the council are creating a potential hazard to life.</p>
Barry pinkney				No to road
Barry, Ann and Craig Pearson	Stainsby Hill Farm	Michael Mealing	Michael Mealing Planning	<p>Having accepted the allocation of their land for housing development in the Housing Local Plan, my clients have no objection to the construction of the Stainton Way Western Extension. They would have preferred the new road to be located more to the east of their land ownership in order to retain ready access to the majority of their holding for the purposes of continuing a viable farming operation. The line proposed for the new road will make it difficult to access land to the east, with the need to cross the road with agricultural machinery and livestock likely to present day-to-day problems.</p> <p>This will create problems for the managed retreat from agriculture and have an adverse impact on the generation of income from the farm.</p> <p>My clients are supportive of simultaneously extending the road from both the north and the south, in order to expedite housing delivery and reduce the timescale during which they will have to manage already-increasing pressures on their farming operations from both trespass and construction activities.</p>
Barry, Ann and Craig Pearson		Michael Mealing	Michael Mealing (Planning)	<p>Having accepted the allocation of their land for housing development in the Housing Local Plan, my clients have no objection to the construction of the Stainton Way Western Extension. They would have preferred the new road to be located more to the east of their land ownership in order to retain ready access to the majority of their holding for the purposes of continuing a viable farming operation. The line proposed for the new road will make it difficult to access land to the east, with the need to cross the road with agricultural machinery and livestock likely to present day-to-day problems.</p> <p>This will create problems for the managed retreat from agriculture and have an adverse impact on the generation of income from the farm.</p> <p>My clients are supportive of simultaneously extending the road from both the north and the south, in order to expedite housing delivery and reduce the timescale during which they will have to manage already-increasing pressures on their farming operations from both trespass and construction activities.</p>
bernard smith				if you do not build housing developments you do not need road

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Bethany Thatcher				As a resident of Jocelyn Way, I completely agree that there needs to be a link road joining to the North of the new proposed development. It would be ludicrous to have just the one entrance at Jack Simon Way for such a large scale development. The traffic will include not only residents to over 2000 houses (including the large existing development), but also visitors to the country park and other amenities. Given this, I believe that, as well as the entrance from the Mandale roundabout, there should be further roads linked to the existing housing estates to the east of the proposed development. This would mean less traffic through Jack Simon Way and the new Northern entrance. If there are no further entrances added, then I believe the roundabout at the Jack Simon Way entrance and the beginning of Jack Simon Way road need to be looked at as the roundabout is unmarked and unsafe (people cut across the right hand lane when in the left lane) and Jack Simon Way is already not wide enough at the entrance for the current traffic. The addition of the newly proposed housing development will mean Jack Simon Way becomes a very busy road. This should certainly be limited to a 20mph zone. There are already issues with speeding on the estate and there is a large number of young children, so it is important that speed is addressed.
Bev Thomas				Please don't build anything on Mandale Field - especially a road! We back onto this field & regularly walk our dogs there - it's beautiful! I played on this field as a child before it was properly maintained! The traffic & noise pollution will be horrendous & the wildlife will suffer greatly! We currently have 2 foxes that visit our garden regularly, I hate the thought that all this will go! At the very least a loop road would be better than a spine road! Please give this some deep thought.

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Beverley cooper				<p>I would like to cite my objection to the proposals for the meadow to be turned into a through road to service the estate i now live upon. I feel it is unnecessary and is only being pursued due to financial penalties and the shall we say feathering of nest of a choosen few.</p> <p>The meadow is a place where people in the community can take their families and enjoy outside space which is few and far between.</p> <p>The meadow houses a multitude of wildlife including owls ,wood peckers, foxes,hedgehogs and deer which again will have to be pushed and moved or into the path of danger or death due to the building of an unnecessary road.</p> <p>I live on the new estate and have had no issues getting on or off the estate, the new road will faciltiate a massive up stream of lorries and wagons and none estate traffic making the estate more dangerous for the growing families on this estate.</p> <p>There are no conjestion issues to contend with and if anything more will be created due to bottle necking, all you are doing is moving the problem somewhere else. The medow has been present for many hundreds of years with trees older than most of us put together and yet it seems to hold no value even in current times of destruction of the planets eco system that protects us all. The meadow brings so much more to a community, more so than and concrete created road to serve in making some builders and councillors so more money and creating another hazard for the wider community.</p> <p>The only people who benefit from the proposal are the people involved in the building and proposal of it. I think it is an absolute tragedy that this would be allowed, especially now and the state of the planet. Outside spaces are important for the communitys mental health and wider to form a place social interaction for all. What would a road bring ... what people can get home quicker deliveries can save 10 minates on a journey wow thats truly amazing is it not.. never mind being able to see deer roaming wild , enjoying wild trees and flowers on a beautiful spring day, reasons for this road are total crap and the same rubbish which is spouted when people want there banks filling with money.</p> <p>This meadow cannot be replaced or should it be especially not with a road. This proposal is a joke and it should stay just that and never come to pass. The sacrifice for this road is far to great to even contemplate it.</p> <p>The real people who live in this area do not want this road nor do they need it. Lets hope some reason can be found and this plan be condemn to nothing.</p>
Brad Raistrick				<p>This will ruin what is coming to be a really nice estate. I feel this 'country park' is a smoke screen for the awful idea of a through road right through the estate! This will drive house prices down and make traffic horrendous along the already narrow Jack Simon way (due to side of the road parking).</p> <p>Shame we weren't told about this hideous idea when we purchased our property two years ago! If this and an even worse proposal I've heard of (a bus route) is added we will be forced to move as it will ruin the appeal of the estate in our opinion.</p>

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Brian Casson				I object to the location of this road. It destroys a valuable green space used for many years by many generations of the Middlesbrough public. Not only that, the current mayor was elected on a platform of not destroying this exact green space.
Carly Jahangiry				As a resident directly impacted by the plans it's appalling the lies and mis-information which has been told to us. We haven't had a chance to protest or seen any objection letters around the area. It's disgusting when you have houses and land with derelict houses in areas in Middlesbrough, knock them down and start again don't ruin Acklams nice reputation. The whole of Middlesbrough is full of rough areas and it's due to building houses. Leave the free space and keep the nice reputation.
Carol Newmarch				I suggest there is no road built, given we are in a climate crisis and need to look at alternative mode transport now, not sacrifice further ancient green space
carole elizabeth mitchell				Due to the ecological impact of the proposed road, and the congestion at Mandale, I am against it A loop road is preferable
Charlotte McAdam				The access point at Mandale roundabout is not necessary. A loop can be created for the new development with the access point being at Jack Simon Way, where the roundabout has already been developed, and access has been designed and approved. This has easy access to the A174 and A19 from here. The exit point on Mandale roundabout would significantly impact Mandale Meadow, the houses which back on to it, and all users of it. I walk my dog on the meadow, and with the road passing through, I would not feel safe doing so anymore with a road running through it/adjacent to it, regardless of its status as a "country park". A "country park" should not have a road running through the middle of it, impacting on wildlife, users of the area and residents already living nearby. Even if a 20mph speed limit was imposed, with traffic calming measures, this would not be sufficient. Hall Drive, for example, is 20mph will speed bumps, yet no one ever does 20mph. It would be a danger to people trying to enjoy the "park".
Chris Brown				The road is essential to this project and it will elevate traffic issues through the Trimdon area. Looking forward to the development
Chris King				Access to a comprehensive set of estimated traffic flow data coupled with a set of street maps would facilitate the above, but regrettably I don't have such information to provide an alternative access point. I know that once these plans have been formed they seem to acquire a sense of solidity, and we lose the possibility of looking for alternative sites. We are, however, continually told how important agricultural land is, but I suspect it is easier to convert rich productive agricultural land into housing than cleaning up brown field sites. Pity.
Christine Cooper				Loop road through Stainsby Estate

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Christine King				<p>There should be NO road built that cuts through any of Mandale meadows or woods.</p> <p>(With reference to document 'Initial Option Assessment Report, dated 25th Jan 2021, Version 1.1, page 9 -10)</p> <p>The council have already reached their target for house building. The 2014 Local Plan is out of date and should be reviewed and rewritten in light of what is actually needed at this time in this area and in line with current findings on the real value of greenspaces and the irreversible harm caused by building on them. I believe no more houses should be built in the areas of Coulby Newham and Stainsby which would result in no need for further road building, Option 1 would then be the best option and would preserve the greenspace of Mandale.</p> <p>If it was deemed that Option 1 is not viable, then the Option 4a and Option 4b (connecting to Option 3) should be reconsidered as these will allow Mandale meadows to be preserved.</p> <p>"Primary Road – Potential Expansion Position. An extension to the proposed road may be required to serve the site as a whole, accessing the site from its northern boundary. This is subject to area Traffic assessments and consultations with Highways." (Stainsbymasterplan pdf – Chapter 7 –Feb 21)</p> <p>Middlesbrough Council's independent review did not include consultations with the Highways agency so further consultations will be necessary. There are other options for the road, which need to be reconsidered when consultations with Highways are undertaken. Middlesbrough Borough Council is misleading the general public into accepting the independent review (consisting of council officers) in stating that the road is 'necessary.' Further consultations with Highways agency should result in considering all options that preserve natural greenspace and it should be in these consultations where it is decided what is 'necessary' and 'required'.</p> <p>NO ROAD THROUGH MANDALE MEADOW/WOODS</p>
Christine Ward				Option 1, the loop road, is a far preferable option.
Christopher Bieda				I do not object to the sighting of the road as it is required, however as a resident of a street that joins this road (Jack Simon Way) I would propose that the road limit will need to be set at 20mph MAX. This road has residential frontage with no grass verge and on street parking only (no driveways) The road is reduced to single carriage way when people are parked and also is busy with pedestrians walking to school and shops.
Christopher Dean				<p>I tried filling in the survey but web page inaccessible.</p> <p>I believe the road should go ahead to relieve congestion.</p> <p>Regards Parish Councillor Stainton & Thornton Christopher Dean</p>
christopher Eddon				<p>i do think that we should think again about trying to make the centre of middlesbrough the only place of work especially in view of whats happening now with covid and the amount of people who are now working from home, i used to travel out of middlesbrough up the A19 every day and used to feel sorry for everyone stuck in traffic heading into the boro all the way up to the wynyard turn off and from what i gather its just as bad on the southern routes so instead of building more roads in which to clog up why dont we encourage more out of town eco friendly office blocks with added amenities for the local communities to use. you could build one of these on stainsby field instead of yet more housing.</p> <p>ive also noticed that the car park on the east side of the A19 mandale roundabout was always full on a morning so if people are parking there and then walking to work or getting the bus why dont we create a park and ride stop in this location and have routes going to the town and maybe james cook and then create another one north of the river tees.</p>

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Claire Boyd				(With reference to document 'Initial Option Assessment Report, 25/01/2014) The council have now fulfilled their house building target. The plan is outdated and should be reassessed given current guidance on the value of green spaces and the harm that building on them would entail. There are countless new houses South of Mandale and no further building would mean that the road wasn't required. I strongly believe that Option 1 is the best in order to keep Mandale Meadow free of devastating building work. Option 4a and 4b would be the then preferred options in order to preserve the meadow. NO ROAD THROUGH MANDALE MEADOW/WOODS
Claire Di Cicco				You are planning to put a road through a meadow! You are about to destroy wildlife's habitats and a natural greenspace. You are about to take away a most valued greenspace which is enjoyed by many.
CLAIRE KANE				Yes all of the new estate should use Jack Simon Way and go out to the A19?parkway roundabout
Councillor Eric Polano	Middlesbrough Council			I believe the new proposed road from Stainton to Mandale roundabout, is a great idea.. The road will accept the excess traffic from the Stainton area ,then allow it access t A19 which is our main arterial road. Should the new road not be provided, It will cause a major traffic build up along Low Lane and at the Blue Bell roundabout ,also further down Acklam road.. Acklam road would become The same as Marton road, The MARTON CRAWL.. Thank you Best Regards ,, Councillor, Eric Polano
craig Llewellyn				No road less houses, nobody can afford all these houses so why keep building them we need affordable housing built on brown fields!
Damian McKinley				Simple NO HOUSES
Darren Middleton				Why does it state for the country park to go ahead the new road needs to be there? The road that will run up through Mandale meadow will destroy existing nature reserves.
Darren Pattison				I have great concern about the speed on the spine road at the moment. I live on Jack Simon Way, and with me working from home I can see the amount of traffic and the constant speeding up and down the road. We are in lockdown and we still have hundreds of vehicles everyday going up and down. The speeding on this road is really bad and it is only a matter of time before a kid is knocked over and killed. I have raised this with the Mayor and the council but they currently cannot do anything. The speed for this road should be cut to 20mph especially when we have a school. There should be traffic calming measures in place all along the spine road as I fear this will end up a rat run when traffic is heavy on the a19. Please can you arrange to have this road monitored before you go ahead with any plans, because if someone is killed I will keep this email as proof that you were warned in advance by someone who is seeing these problems on a daily basis and will only get worse the bigger the scheme gets. Please try and put something in place now to get drivers used to the new limits or the country park will be ruined by excessive traffic and speeding.

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Darren Peters	N/A			We need access cycling route through the land for exercise
David John Parker	Stagecoach North East			We are supportive that the masterplan affords vehicular access at the northern and south-eastern extents of the development site. Given the scale of the development, we support the suggestion that the roundabout link at Mandale Road/ Levick Crescent is enlarged, and is upgraded to signalised traffic control. We would also suggest that a similar methodology may well also be appropriate at the south-eastern corner of the development at Jack Simon Way/ A1044, as it could be expected that this will also require a strong degree of access/ egress.
Dawn Paton				I don't agree with the plan for a through road . It will impact on many people's mental health who use this green space for walks, dog walking , seeing wild life . It will kill off lots of wild life including animals plants and trees . It will cause chaos on the roads whilst under instruction and afterwards .
Debbie Branson				I think this would be awful. This is one of the few places we can go with pets, children and grandchildren to see wildlife that doesn't involve driving for miles and miles. Why can't you use the loop route that has been suggested? It seems that you say you are interested in safeguarding nature and the positive effects it has on everyone but it that is just lip service I am disappointed in this action and would like you to re think
Deborah Vickers				I believe the new road through Mandale meadow will have a major detrimental effect on the already congested road the private development of the area has already devalued property and taken away significant green space for current residents resulting in additional pollution and health risks
Denise Selway				WE say no to the Spin Road to Mandale Field. We need Green Space more than ever. Wildlife will suffer. We need this very important space. We say NO NO NO NO NO NO NO.
Dennis Mccabe				Having looked at the plan. I feel the road can be pushed towards the A19 and closer to Mandale Rd. By doing this it will reduce the amount of damage to the meadow, doing this it will retain some of the car park people use when walking around the meadow.
Dylan Currey				The planned route for the road would be building on one of the few, public, freely accessible green spaces in/near Acklam, which can be accessed on foot. As an alternative, a road connection from the A19 to Low/Barwick Lane should be considered. This may help mitigate Thornaby/Ingleby Barwick related congestion on the A1130, and would certainly be a preferable, and more beneficial alternative.

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Edna Reddy				<p>Re Mandale meadow and Stainsby housing development consultation</p> <p>I am very concerned that you need a computer in order to see what is planned.</p> <p>To respond to the plans you have to view them online and reply by email or print off a form or write a letter and post it, this means only the computer literate, the owners of computers (and printers) and people who have an email account can be involved,</p> <p>The consultation is therefore available to only a selected group of residents, clearly this is unacceptable.</p> <p>I know under present restrictions it might be difficult to publicly display the plans , large scale, however if I can go to ASDA and , socially distanced, peruse the shelves there, I could go to the large open space in ,say, the Acklam Green Centre and easily see the plans there. Everyone whose environment is affected should be able to see them and comment.</p>
Edna Reddy				<p>Instead of cutting out of the housing estate and across Mandale meadow the the spine road should loop back onto Low Lane and access A19 from there.</p>
Eileen Liggitt				<p>This area is a haven for both wildlife and people. By destroying this bit of nature in a high density populated area you are acting like a criminal. To have more houses and a road are going to deprive so many of a place that is a sanctuary for their mental health and well being. Concentrate your house building in central areas which need it. Don't rip up more green spaces as there are very little left.</p>
Emily quintt				<p>Don't build one. Why ruin our green space where we walk with our children and dog. Putting a road mixed in with a country park is asking for trouble.</p> <p>Children playing near moving vehicles? Dogs chasing balls and running through the trees? Someone is going to get killed.</p> <p>Why build a housing development near such little access roads? That's your fault for not thinking it through.</p> <p>Create access from the a19</p>
Fay Dawson				<p>I strongly object to the development of this land. Mandale Meadow and the rest of the land between Mandale Road and Jack Simon Way is an essential resource for both families and animals. It is the only green space in the area for many families in Acklam/Brookfield. I no longer live in Acklam but I still often walk the meadow from Mandale Road car park through to Trimdon Ave and beyond. It's stunning at any time of year. A real gem between the A19 and thousands of houses. It's needed for recreation and well-being for both humans and animals. To build on this is irreversible and will not be forgotten or forgiven by future generations. There must be suitable brownfield sites that can accommodate the housing that's allegedly required. The world has significantly changed since the Middlesbrough Plan was written in 2014. That's 7yrs ago. Surely the plan can be reviewed? Have all other options been considered and discounted? Widening the A174/A19? Deploying a one way system around Middlesbrough? (Eg Marton Road North bound only, Acklam Rd south bound only?) There has to be another way other than destroying acres of treasured green space.</p>
Francine Kenny				<p>Yes to Country Park</p>
Gemma				<p>I only bought my house 2 years ago from Miller Homes. My house currently faces Jack Simon way and I am outraged that this road is going to made into a main road that stems from the motorway. We already have the a174 at the back of our houses causing pollution so why do we need another? This is going to cause unnecessary traffic, noise, fumes right outside my house when I did not sign up for this. I also have a little boy who will be two in June and if I'd have known this would happen, I really would not have bought this property as I am now worried about the air and pollution that he will breathing in from this new road. I am furious at this. If you plan to build the park, then why do we need a road coming all the way through? Can you not just have the one entrance at mandale roundabout? I don't see the need for a road which is parallel to the A19, this makes absolutely no sense to me at all. You are just causing upset to the residents who have just purchased these properties. A main road in front of my property is not what I signed up for. I wanted to live in a nice quiet area where only residents come and go, not a through road for the whole of Middlesbrough.</p>

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Gemma				I only bought my house 2 years ago from Miller Homes. My house currently faces Jacksmon way and i am outraged that this road is going to made into a main road that stems from the motorway. We already have the a174 at the back of our houses causing poltution so why do we need another? This is going to cause unnccessary traffic, noise, fumes right outside my house when i did not sign up for this. I also have a little boy who will be two in June and if i'd have known this would happen, i really would not have bought this property as i am now worried about the air and polution that he will breathing in from this new road. I am furious at this. If you plan to build the park then why do we need a road coming all the way through? Can you not just have the one entrance at mandale roundabout? i dont see the need for a road which is parallel to the A19, this makes absoltely no sense to me at all. You are just causing upset to the residents who have just purchased these properties. A main road infront of my property is not what i signed up for. I want to live in a nice quiet area where only residents come and go, not a through road for the whole of Middlesbrough.
Gemma Thompson				A park is not needed. This development will ruin and disrupt natural wildlife, bring more pollution and road noise into the area. (If the spine road is also built) The meadows home a lot of natural wildlife and is already a beautiful walking route for residents and has been for years, why waste money on a development that isn't needed, Middlesbrough already has plenty of parks in the area. We already have a lot of road noise from the A174 and surrounding due to all the new housing developments. If you then build another road and park this will only add to this issue. Why is the council insistent on building on this green space, try spending the money on improving the town centre!
Geoffrey Payne				Option 1 on the 2085-34 ▪ Stainsby Link Road ▪ Initial Option Assessment Report 25 January 2021 ▪ Version 1.1 ▪ Issue seems the best route to prevent adding to the congestion already faced on Levick Crescent, Mandale Road and the A19. Levick Crescent and Mandale Road are also school routes for pedestrians with no controlled road crossings.
georgina olsen				I am appalled to see the suggestions for what you have the nerve to call a masterplan /country park. How on earth can it be a country park, when you are destroying the very nature of the word countryside. The destruction of wildlife habitat, the destroying of trees and plants, the devastation to the local people, of the few remaining green areas, where people can walk with families and their dogs. A promise was made to defend these areas, from the so called councillors and the mayor, the representatives of the people. It is bad enough that you have crammed houses on to the surrounding areas, causing traffic congestion and overcrowding of schools. The school traffic problem on Trimdon avenue, is a serious accident waiting to happen. Then the brilliant idea is to build a spine road, and have the nerve to call it a country park. In these difficult times, people need a sanctuary, and escape to walk safely with their families. This is nothing but an act of vandalism and a crime. Do the proposers live in the area, if you do please contact me and tell me how it benefits you. Would it not be better to re-develop areas where you have torn down houses and destroyed communities. I look forward to an early reply Regards Georgina
Gill Mollard				The SWWE is not necessary. The proposed housing developments need to be reviewed as they are not appropriate given the changing circumstances. The council has a 10 year supply of available housing supply and alongside changes to policies the allocation is no longer required. The council would be justified in deallocating the site. Changes to policy and green aspirations render this road unnecessary. Simply because the road was considered necessary in 2014, that was a very different set of circumstances. it is not 2014 any more and the council should consider a change in direction. Especially the climate declaration and ambition to 'protect green spaces'. Please take into full consideration the representation you have received from the Campaign for the Protection of Rural England and the open letter from Greenstuff Middlesbrough, which was written having taken advice from the Transport Action Network. The current road at Jack Simon should loop through the current housing. Further development should be halted at this site and also at other greenfield sites in Coulby Newham and Nunthorpe. This would then not impact on infrastructure and Highways England would not have any cause to disapprove.

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Gillian Robins				Wold prefer option 1. Am worried about work being disruptive and wildlife being damaged.
Glen Instone				How does a road through a new housing estate improve existing capacity issues on the A174/A19. Did someone really take time to write such nonsense? This is still close to my property so yes I am aggrieved!
Graeme Scurrah				I would like to object to any plans to build on stainsby meadow
Graham Hadfield				<p>Andy Preston, in his Facebook post at https://www.facebook.com/permalink.php?story_fbid=2131098370354142&id=3 says that "The road would have a speed limit of 20mph" but in the FAQs document (https://www.middlesbrough.gov.uk/sites/default/files/Stainsby-masterplan-FAQs-Feb21.pdf) it says "The speed limit on the Stainton Way Western Extension will be a maximum of 30 mph....." and in Chapter6 of the main document (https://www.middlesbrough.gov.uk/sites/default/files/Stainsby-masterplan-chapter-6.1-Feb21.pdf) on page 60 it repeats the 30mph maximum speed three times.</p> <p>Could you please clarify which limit is correct, 20 mph or 30 mph.</p> <p>Given the limited time available for responses to the consultation I would appreciate a timely reply to what should be a straightforward question.</p> <p>I hope the commitment to taking notice of the consultation responses extends to not building a road across any of the public land at all.</p>
Graham Hadfield				<p>We are informed, that the routing of the Stainton Way Western Extension (SWWE) can not be changed because it is set out clearly in the 2014 Local Plan, and is identified as a critical part of the town's future infrastructure.</p> <p>The 2014 Local Plan also requires that a new link road be created adjacent to Ormesby Beck between the B1380 Ladgate Lane and the A172/A1085 Longlands Road Roundabout.</p> <p>I understand that the Ladgate to Longlands Link Road has since been cancelled (or is, at least, being considered for cancellation) as it will no longer deliver the anticipated improvements.</p> <p>Obviously, that indicates that the provisions of the 2014 Local Plan are not set in stone and can quite properly be changed when circumstances change.</p> <p>What evidence does Middlesbrough Council have that the SWWE will deliver the improvements anticipated by its inclusion in the 2014 Local Plan and that it will not fail to do so by simply shifting traffic pinch points from one part of the borough to another?</p> <p>Once again, given the limited time available for responses to the consultation I would appreciate a timely reply to what should be a straightforward question.</p>

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Graham Hadfield				<p>The statement "In this section of the form, should you wish to suggest an alternative road alignment that has not already been considered by the Council, please provide comments below." reads as "if you wish to object to the findings of the "small working group, comprising representatives of Middlesbrough Council" tough, we aren't interested.</p> <p>The only two options supported by the "small working group, comprising representatives of Middlesbrough Council" are the two presented in 2018, both of which involve the destruction of Mandale Meadow, the destruction of mature woodland and the extensive culverting of Bluebell Beck (something which was specifically, supposedly, ruled out).</p> <p>This indicates that , far from the position being "there's no plan to put a road in", the decision to destroy Mandale Meadow to support a housing development has already been made by the "small working group, comprising representatives of Middlesbrough Council".</p> <p>That conclusion is reinforced by the Stakeholder Acceptance/Support statements in Appendix B of the Stainsby Link Road Initial Option Assessment Report which are biased to support the options which align with the desire of the previous administration and officers to have not just an access road but a road designed to be "A new link road that will run between A1130 Mandale Road and B1380 Low Lane, designed to relieve pressure on the A19/A174 Parkway Interchange and facilitate access to the Stainsby Housing site." (letter to Dixon's Bank residents from Rob Farnham, Principal Transport Planning Officer, 18 December 2017) – i.e. not just a 20 mph estate access road but a Primary Route (as it is described in other documents) intended to route commuter traffic through a housing estate and country park.</p> <p>The fact that options 4A and 4B would involve high complexity and additional crossing of A19, together with new connection into existing local network is neither here nor there. The developer could reasonable be expected to understand that when purchasing the land. It is not the function of Middlesbrough Council to destroy public amenities just to make the pursuit of profit by a private company easier.</p>
Graham Hadfield				<p>The Stainsby Link Road Initial Option Assessment Report completely ignores another option which would take advantage of existing infrastructure. An access road could be linked into the road from Stainsby Hall farm across the A174 to meet the B1380 alongside The Sporting Lodge (please see attached illustration). It would not provide a connection to the A1130 but there is no need for one given that it is only an estate access road – unless, of course, the aim to build "A new link road that will run between A1130 Mandale Road and B1380 Low Lane, designed to relieve pressure on the A19/A174 Parkway Interchange " is still a main item on the agenda.</p>

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Graham Hadfield				<p>This is an addendum to my comments submitted on 5 February and 8 February.</p> <p>We are told (Facebook post https://www.facebook.com/antonyhighmiddlesbrough/posts/71559348915694) that officers have advised the Mayor, Deputy Mayor and Executive Member for Regeneration that there are severe legal, control and financial implications for Middlesbrough Borough Council if the building of the SWWE through Mandale Meadow to meet the A1130 at the current car park (as opposed to using one of several alternative routes for a housing estate access road) does not go ahead. We are told that that route was duly confirmed as necessary by the Planning Inspector as part of the process of getting the Local Plan approved. However, we also know that the Local Plan included a second road - a new link road from Ladgate Lane to Longlands Road – which was equally fundamental to the approval of the Local Plan. As regards this second road we are also aware, from the Nunthorpe Infrastructure Document, written by Paul Clarke on 19th May 2020, that “the Ladgate to Longlands Link Road may no longer deliver the anticipated improvements and alternative solutions are being considered.”</p> <p>This begs two questions:</p> <ol style="list-style-type: none"> 1. If the Ladgate to Longlands Link Road “may no longer deliver the anticipated improvements” then exactly how is a 20 mph max (or even 30 mph max) extension to the 40 mph Stainton Way (running through a housing estate and country park) expected to deliver what it was supposed to? 2. How, exactly, can the Ladgate to Longlands Link Road (also identified as a critical part of the town’s future infrastructure) be cancelled without having the same severe legal, control and financial implications for Middlesbrough Borough Council? <p>Please note that I shall be looking for clear and complete answers to these fundamental questions in the final consultation report.</p>
Graham Liggitt				<p>There are other areas in the town more suitable for housing development and the proposed road would eliminate a area of natural bearish significant wildlife being destroyed in the process.</p> <p>The whole town and region needs a transport plan which looks at enhancing public transport and cycle lanes in order to reduce the amount of cars on the roads. I would argue that the 2014 plan is already out of date and should be put aside and updated.</p>
Graham Loughborough				Road must be 20mph with speed restrictions in place, speed islands/humps, zebra crossings and pelican crossings. Due to the high volume of traffic and you g families in the area.
Hannah Wiley				I strongly object to the building of a spine road through Mandale Meadow. This is an area of natural beauty, of natural habitat- we should be protecting such areas and not ploughing them down with roads and ‘country Parks’. The countryside does not need to be landscaped into a ‘country park’ to be beautiful; this is just a PR stunt to make money.
Helen McGregor				<p>A loop road into and out of Low Lane would be the only option I would consider. This would still allow traffic to circulate without disturbing our much-loved meadow and woods.</p> <p>The spine road would mean just 3 miles of less traffic on the A19 before rejoining it at Mandale roundabout. The resulting effect would be destruction of an ecologically precious area despite promises of new trees, landscaping, etc.</p>
Helen walton				The loop road
Hilary Morley		Hilary Morley	Resident	I don't know of an alternative, after all this isn't my job, but I do know that the area you are planning on destroying is perfect the way it is. A country Park would only be put in place to cover up the fact you are planning on building so many houses and a horrendous road which will be a nightmare for the current residents.
Ian Etherington				I would prefer the road to cross the A19 to use the existing bridge and ""farm track"" route rather than going through the meadow.

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Ian Jasper				<p>Definitely don't want the road running all the way through, the volume of traffic would be horrendous especially when you're trying to make a country park, the last thing in mind should be a road right in the middle, defeats the object if you ask me.</p> <p>Provide a route from both sides, but one that does not link them together would be much better in my opinion.</p>
Ian Marron				<p>An alternative suggestion would be for a link road going from the housing development South over the A174 utilising the existing bridge from Standby hall farm to Stainton vale farm linking up with the B1380 (low lane) near the sporting lodge hotel. This would give access to A174 interchange at Stainton way and also an option of going through Thornaby via the A1045 past Teesside industrial estate. This would ease an already congested Mandale interchange.</p>
Ian McCann				<p>I note with extreme and increasing concern the plans for a proposed spine road bisecting the safe, peaceful, healing, natural environment and ecological habitat of the Mandale Meadow/Field.</p> <p>I understand the 1,760 new houses proposed are projected to create some 12,000 daily car movements. The wildlife living in, moving around, and passing through this currently totally safe area will cross the road frequently, so it is inevitable that thousands of mammals and amphibians, and likely millions of insects will be killed every year by the traffic using the proposed spine road. Of course, the houses and increased local activity will also hugely reduce the natural habitat for wildlife and flora. The result will be a local ecological disaster.</p> <p>Perhaps, even more importantly, this spine road will result in the loss of the quiet, peaceful, healing space enjoyed by many local residents, including those who daily walk their dogs, enjoy family bonding time and the opportunities to introduce their children and grandchildren to nature but also all local residents who either regularly or infrequently, for reasons of their wellbeing, mental health and physical health, need to enjoy the space, nature and quietness afforded by this area, to escape the pressures, noise and speed of modern urban life, and all without needing to use a car they may not have. Of course, taking a car, if one is available, to a distant location for peace and healing is counter productive, costly, adds traffic and pollution to our environment, and increases the pressure on the places they would drive to, devaluing those areas for the people who are currently recreating there. Many simply do not have access to a vehicle and certainly not regularly so they will be denied the benefits currently afforded by Mandale Meadow/Field and beyond.</p> <p>Then there is the matter of the likely increase in new housing, which developers generally manage to add to agreed developments, which is typically in the order of 20%, so the numbers of houses, new people and traffic movements would all increase pro rata. Thus the traffic movements might well exceed 15,000 every day.</p> <p>The huge amount of additional traffic will of course add to the above ecological carnage but also increase the pressures on the existing roads in Acklam, Thornaby and surrounding areas and the residents in these other areas deserve consideration, too. This development, if it goes ahead as MBC seems to wish, will increase pollution in the area, especially as these journeys will typically be short, cold engine journeys, which are always very highly polluting.</p> <p>Consequently, we would lose air improving green spaces and gain air polluting houses and slow local traffic, increased vehicle movements around Acklam, and all the extra noise that these vehicles will generate. This combination will add to the urban blight that many currently are struggling with, putting increased pressure on the local mental health services and more importantly reducing the quality of life for all the local people, who bought or rent homes in Acklam because it is as it is, provisioned with green spaces, period character and a generally pleasant small town vibe. Massive increases in traffic density will also have a serious impact of safety as accidents will also increase on the busier roads around the area. Existing property values will very likely be adversely affected by the increased supply of houses, the deterioration of the quality of Acklam life, as outlined above, in a town that has an ageing population, and an outflow of people due to BREXIT, the chronic decline in traditional employment, the better employment and life chances offered by almost every other city and town in the country drawing all our best young adults away, including the non local graduates leaving our universities with their new qualifications. I also note that the number of people who it is likely will live in about 2000 new houses will be more than the current population of Acklam. This would be a massive change.</p> <p>It has been noticed that our elected politicians started out with a very secretive process to realize earlier plans and having been outed, are now having to be rather more open, and responsive, in fear of their seats no doubt, although frankly, one is now naturally very suspicious of their every move, after their underhand and</p>

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				<p>utterly arrogant earlier behavior. The reality is many residents even in Acklam are still not aware of what the council is proposing to do. And these changes only really affect the residents of Acklam, not so much the wider population of Middlesbrough. So, only the opinions of the few informed Acklam residents have any value and currency. My point is, in a town the size of Middlesbrough the number of complaints and letters expressing concern that MBC receives may seem modest. However, I am certain, relative to the small number of Acklam residents who have found out about this proposed spine road development, and the likely urban creep that one day may very well progress towards and across Mandale Meadow/Field, the number of outraged residents and complaint letters will undoubtedly be very high.</p> <p>So what are the alternatives? 1, MBC should forget the whole thing. This would be by far the best plan. After all, the fields were gifted to the local people in the trust of the council, forever. And the development flies in the face of the benefactor's intentions and agreed permissions, which prohibited building on our recreational land. 2, Failing the morally correct cancelation of the development, the loop road option at least avoids the irreversible damage to Mandale Meadow/Field and significantly reduces the risk of future creeping development along the spine road across Mandale Meadow/Field. It is only right that the roads needed by the residents of a new development should be in their community and not ruin the environment for others who do not live there, do not want it, and frankly will suffer in many ways outlined above, as a result of their development and their roads.</p>
Ian Rogers				<p>The road should be as shown in option 1, a loop, as the purchasers of houses on the new development were lead to believe. The development should be scaled back as there is no requirement for this amount of additional housing to be built on a greenfield site.</p> <p>The council's preferred option goes through Mandale Common, land which was bequeathed to the residents, has been enjoyed by many local residents and is home to several endangered species of flowers, insects and animals. Yet the council seems set to disregard its commitment to the environment and think that replacing a mature meadow with a sterile 'country park' meets its obligations!</p>
Ina Tullo				Loop rd through Stainsby estate
J M Lomas				<p>I support the present consultation plan providing that the primary road exiting at Mandale is kept as the present infrastructure cannot support this development plus others to the south of Middlesbrough.</p> <p>I also support the development of a new primary school from the beginning of the development as other schools in this area would appear to be at or close to capacity.</p>
Jack				Forget any new roads, you jokers need to sort your current roads out. Marton Rd and Stainton Way are an absolute disgrace
Jacqueline Jones				This is the only obvious road alignment. The road is necessary as the scheme is already being built and the traffic can not all be funneled to a bottle neck pinch point on low lane A174/A19 interchange. There is already an increase of traffic coming through the residential estate in Brookfield
Jacqueline Young				It also states in the report that highways department have yet to be consulted, therefore, how can the council and architectcs know if the proposed solution to to the correct one. The only solution is no road through Mandale Meadows or the woods, no road through the car park. Greenspace should not be destroyed for a road
James Gott				<p>What is needed is a loop road, and not a road going throught Mandale fields.</p> <p>Such destruction of green space that is vital to wildlife and local people should be the last thing offered as a solution.</p>
james wilson				i would like to express my serious concern's over the proposed loss of such a huge swathe of green belt land plus don't think you have noticed the zebra(lollipop) controlled pedestrian crossng at mandale road about 50 yard;s from the roundabout getting acklam grange pupil.s at this point will cause even more traffic problem;s than there already is especially at school time.which coincide with the morning rush hour going to work traffic.
jamie barber				option 1. mandale roundabout is congested enough as is. it does not need an extra road on it as well.

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Jamie Killington				<p>As a local resident I am very concerned about the future development. Acklam has very little green space as it is; with avenue of Trees being the only outlier, (Mill Hill also potentially being fenced/sold off in the future). In a time where people need outdoor space for physical and mental health I feel the decision to build on Mandale meadows is pretty tone deaf. I understand money needs to be made as funding is very low, but this could have many knock on affects.</p>
Janet Jackson	South Tees Hospitals NHS Trust (Radiographe r)			<p>I have no objection to the overall plan as detailed in the assorted supplied documentation apart from the selection of Option 3 as the preferred route for the link road. My reasons for this are:</p> <p>This routing, from when it crosses the beck into Mandale Meadow, will effectively cut the meadow area into 2 sections. This nullifies the ideology of a safe park environment for both people, animals and wildlife, especially given the amount of traffic anticipated and which has raised the need for the construction of an extension road. A 30mph speed limit also seems entirely inappropriate for a road passing through an area of recreational green space/parkland. I do not feel that this is acceptable at all.</p> <p>The sizing of the road and associated pathways and cycle tracks will make significant impact on to the visual appearance of this area, increasing road noise and pollution levels for the existing houses which back onto the Meadow, already suffering significantly from the noise from existing local roads and the A19, and also users of this particular area for recreational purposes.</p> <p>There is significant destruction of numerous large trees in the construction of this road. This will be detrimental to the appearance of the area, absorption of noise and pollutants from the A19 road traffic, and absorption of water from the surrounding land. Whilst this area may not be deemed as at great risk from flooding, during the wetter weather this area becomes very quickly soaked, being the lowest lying land at this point and this will be further exacerbated by the development of further housing on the adjoining farmland. Removal of the tree cover and their effect on stabilising the water table will add to this problem. It will take very many years for the proposed planting to have anywhere near the effectiveness of these trees creating additional problems of flooding due to standing surface water. I feel the impact of this has been underestimated by the current surveys, not done during the wetter months. There is also a comment for why one of the options is not considered as appropriate due to the increase of noise impact on the new housing development. This consideration should also apply to the existing developments in term of the selection of Option 3.</p> <p>The connection of the road at the Mandale Roundabout, already a busy roundabout, and its proximity to the main roundabout for connection to the A19 will create more problems than it resolves. At peak times there are already tailbacks of standing traffic at the access point onto Mandale Road. With further usage being encouraged by the inclusion of this link road, these tailbacks could quite possibly extend onto the 70mph section of the A19, increasing the likelihood of accident to this area. There have been significant improvements made at the A19/A174 interchange to accommodate increases in traffic which would adequately accommodate Option 1 of the Loop Road. It seems non-sensical to be rerouting traffic through parkland and away from an existing designated A route round the centres of population.</p> <p>I also feel that this green belt, extending from Low Lane through to Mandale Road, ought to be maintained in its entirety without further road divisions, for its uniqueness, benefits to the local population, as a natural and varied habitat. The introduction of a road is ugly, dangerous and will ultimately damage irreparably this area. Ultimately, I do not think it will be a successful option, will need additional development and will have damaged an area of meadowland for minimal benefit.</p> <p>My preferred option is option 4b, which I appreciate is more expensive to develop but will be less damaging to the area and more appropriate in the long term. From the arguments detailed in the documentation Option 3 is selected not because it is the best option from the point of traffic management but because it is the less difficult and less expensive option. I do not feel that the cost effectiveness of Option 3 outweighs the negative arguments associated with this proposed route. I would be most grateful if this part of the plan could be reconsidered.</p>
Janet Johnson				<p>I wrote yesterday to make clear my objections to a spine road being built across Mandale meadow. I want to add to that - the option of a loop road is my preferred choice. Please make a note that I chose option 1- The loop road and not the spine road that will destroy my favourite most beautiful green space.</p>

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Janet Johnson				<p>I strongly object to the spine road's planned route. It will cause too much congestion at the entry point of the roundabout. It will create more problems than it solves!</p> <p>The road is so close to my home that the noise, congestion and unhealthy emissions will be unbearable. It will de value my property as it's killing me.</p> <p>As I expressed before -the land it will destroy is so beautiful and home to many creatures , some of which are protected as well as lots of plants -some rare! This piece of land has been a safe haven for so many in this area Always and especially important during this covid pandemic. To lose access to this amazing space would be criminal. Why destroy something so beautiful? How can destroying a whole area that provides beautiful walks that are good to improve physical and mental health be justified. Surely road access can be found elsewhere!</p> <p>When I bought my house I bought it because I was assured that this land could never be built on!</p> <p>I am a foster Carer. Some of the children I care for have never been able to access areas like this. The joy and freedom they get from this wonderful space is healing for them and far more valuable than money!! I often have Mums and babies in my care that have mental health issues. This green space so close to my home has given us great opportunities to walk , look at the beauty and enjoy peace. This vastly improves their mental health.</p> <p>During this Covid pandemic this green space has been a life saving space for so many people to come enjoy the peace and tranquility at a time when the world is in turmoil. We need this space- not more roads, houses and developments. This area is priceless!! Please don't ruin it and destroy our lives , the animals habitats and the beauty of the trees and flowers that are saving our planet.</p> <p>Do not build the spine road here!</p>
Janet Johnson				<p>I have no suggestions for alternate route but strongly object to the spine road planned route. It will cause too much congestion and the road is so close to my home the noise will be unbearable. My main complaint that I expressed before is the land it will destroy is so beautiful and home to many creatures some protected and lots of plants. This piece of land has been a safe haven for some many in this area during covid and to lose access to this amazing space would be criminal.</p> <p>When I bought my house I bought it because I was assured that this land could never be built on!</p>
janice slater				<p>Why are two exit/entrances necessary?</p> <p>The route into the estate from low lane via jack simon way is perfectly adequate for a circular route from all parts of this new estate. As this estate is designed to be a country park housing estate, with the traffic calming in place a circular route is by far the safest and less likely to be the cause of "rat runs"and health problems.</p> <p>A number of small foot bridges for pedestrians and bicycles from the new estate over blue bell beck to Mandale and existing neighbouring estates would be adequate to allow healthier forms of travel in and out of the estate reducing travel time but without the knock on effect of extra road use.</p> <p>Estate traffic is one thing ...But when opening up this estate to an easement road there will always be the additional "cut through" traffic from the new stainton housing estate and further afield.</p> <p>This "so called" "country park" housing estate is pictured as a safe and healthy area to bring up children and to have families visit and enjoy... with an easement road I find it very hard to picture the tranquility and healthy status that is suggested.</p> <p>At the end of 2020 (16th December 2020) a coroner ruled that ELLA KISSI- DEBRAH DIED DUE TO AIR POLLUTION EXPOSURE; this is the first case in history to actually have this on their death certificate.</p> <p>This case was in the press during 2013/14 and the recent coroners report has sparked talks on the necessity for traffic reductions in and around childrens play areas and living areas.</p> <p>Surely with new builds for family homes and playing areas these factors should take priority over everything else.</p> <p>No matter what the developers or council state at the time of the planning ... we all know ... only too well how things change as years progress. Traffic calming methods can be removed because of any number of "problems" and then the Easement road takes on a different format.</p> <p>Ingleby Barwicks new estate commenced its building with promises of a number of "Village Green" aspects around the estates... these sharp turned into further new builds throughout the scheme and much of the greenery promised to be there for the residents has now been built on.</p> <p>If this road goes ahead it will exacerbate any breathing problem for children and adults alike on this new estate... The A19 with all its air pollution is running alongside the new estate and to then envelope it into a "sandwich filling" between the A19 and the Easement road is absolutely unbelievable, and downright despicable.</p> <p>I ask that this road is not made a through road but instead remains a one way system.</p> <p>NB to be quite honest who knows of any Country Park where a main road runs through the centre of it.</p>

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Janice Slater				<p>You state that the highways are at some point to be consulted on this plan.....</p> <p>Yet in this plan it states that the highways state that the SWWE and LLLR are fundamental to the plan</p> <p>If this is the case then when and how were the highways consulted to make this statement and why are we being informed that they haven't yet been consulted.</p> <p>Please ensure this why is answered when the consultation finishes. ...</p> <p>I await your response to this and my many other queries</p> <p>Addendum number 2 😊</p>
Jennifer A Alderson				To have road on the opposite side of a19, where there is an existing minor road in place.
Jennifer Jones				This spine road will be used as a rat run and alternative to the A19. There is absolutely no need for this road to connect right through to Mandale. It does not need to flow right through from one end to the other. Jack Simon Way is already a very busy road with cars parked on both sides that creates bottle necks in places and the speed of cars is already a major issue on the estate. Simply putting signs with a 20mph up will not work!
Jenny McCullagh				Declaring that the meadow and green space is being preserved but building a road through the middle of it is a complete contradiction. The 'country park' is just some grass and trees alongside what will be an extremely busy road, and just like Ingleby Barwick and other areas, these so called green spaces are neither safe nor enjoyable to use.
Jess Brown				Why is the road necessary? Why are there roundabouts? This implies future plans to add more roads. How will you protect the habitats that will be destroyed by the building of the road?
Jill Hadfield				<p>The statement "In this section of the form, should you wish to suggest an alternative road alignment that has not already been considered by the Council, please provide comments below." reads as "if you wish to object to the findings of the "small working group, comprising representatives of Middlesbrough Council" tough, we aren't interested.</p> <p>The only two options supported by the "small working group, comprising representatives of Middlesbrough Council" are the two presented in 2018, both of which involve the destruction of Mandale Meadow, the destruction of mature woodland and the extensive culverting of Bluebell Beck (something which was specifically, supposedly, ruled out).</p> <p>This indicates that, far from the position being "there's no plan to put a road in", the decision to destroy Mandale Meadow to support a housing development has already been made by the "small working group, comprising representatives of Middlesbrough Council".</p> <p>That conclusion is reinforced by the Stakeholder Acceptance/Support statements in Appendix B of the Stainsby Link Road Initial Option Assessment Report which are biased to support the options which align with the desire of the previous administration and officers to have not just an access road but a road designed to be "A new link road that will run between A1130 Mandale Road and B1380 Low Lane, designed to relieve pressure on the A19/A174 Parkway Interchange and facilitate access to the Stainsby Housing site." (letter to Dixon's Bank residents from Rob Farnham, Principal Transport Planning Officer, 18 December 2017) – i.e. not just a 20 mph estate access road but a Primary Route (as it is described in other documents) intended to route commuter traffic through a housing estate and country park.</p> <p>The fact that options 4A and 4B would involve high complexity and additional crossing of A19, together with new connection into existing local network is neither here nor there. The developer could reasonably be expected to understand that when purchasing the land. It is not the function of Middlesbrough Council to destroy public amenities just to make the pursuit of profit by a private company easier.</p>

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Joan Pollak				<p>Although the country park plan has some merit, now that there will no longer be any building on Mandale meadow, I was horrified and disappointed at the proposal for a road running through the country park. We live very close to a major junction on the A 19 and we are already affected day and night by the noise coming from it. Any further increase to that level as well as increased pollution levels would be totally unacceptable.</p> <p>The A 1130 in Middlesbrough is already often congested and thousands of extra vehicles per day would massively increase this.</p> <p>I thought that a country park was to promote physical and mental health as well as being good for the environment but traffic running through it would be unlikely to allow it to have green flag status.</p> <p>It is important that there should be access for the whole community, including cyclists, dog walkers, runners, walking groups, children of all ages, older people and the disabled and I feel that there should be a cycle path separated from pedestrians for safety for the vulnerable. There would be room for this if a road was not built here.</p> <p>Although the residents in the new homes will need to get on the road network, I feel that insufficient consideration has been given to the other options for roads on the development, as only a small group was involved in the decision. Option 1 is likely to be more than adequate as there are already traffic lights at the junction on Low Lane. This would be my own preferred option and would be by far the cheapest and most popular. Option 4 b could be seriously considered even though it would involve Stockton council but it could be worth pursuing if it is deemed essential to join the A 1130. This option would be more costly but would have the advantage of giving an alternative way to the A 66 for westward travel avoiding the A 19 and A 66 junction.</p> <p>I feel that many of the traffic problems encountered in this area arise from a lack of crossings over the river Tees. The A 19 is the first crossing inland from the coast which can be accessible with any size vehicle to the main road system. This is something which affects the whole Tees valley as well as the wider region.</p>
Joanne Coe				The road should exit at Low Lane and loop back round. A loop road is a very clear alternative to destroying the green space through Mandale Meadow. Most of the infrastructure is already in place for a loop road at the point of Low Lane. This can then Service the new estate WITHOUT destroying the meadow.
John				No issue on the road layout
John Baines				A link is needed from the new development onto the nearby A19
John campbell				Their existing access via the new roundabout/traffic light system at the west end of low lane is only lightly used and the extra traffic when new housing is built would suffice. This exit gives them direct access to the A174 for travel north or south. Another alternative would be to construct the road down the side (sporting lodge) of the A174 with an access bridge over the A174 into the new estate which would give them two exits from their estate and easier access for both north and south journeys and leave the meadow as it is.

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John David Duffy				<p>I am a member of the following groups on Facebook:</p> <ul style="list-style-type: none"> • Greenstuff Middlesbrough • Greenstuff Middlesbrough & Beyond • Friends of Mandale <p>I reason I joined these groups was due to information provided to me, of which I now can see provides a biases viewpoint. I am concerned that people might provide information through this consultation based on untruths and having not read the information provided by the Council. When replying to such consultations, you need to take a view from both sides.</p> <p>In an ideal world, no one would want the road to travel through Mandale Meadow, but it is clear that the Council have listened to concerns raised, and have assessed alternative solutions. I personally, having read the information provided also agree that the road is required.</p> <p>If I was selfish, I would here say “NO ROAD THROUGH MANDALE”. But people need to see the bigger picture. If the road is not delivered, Central Government don’t change the way in which local councils are funded, and the council cant increase its council tax base in the south of the town, then how on earth are the council going to fund the delivery of services such as Adult Social Care, Children Service, Bin Collections. God forbid people who oppose the road need to access any service in the future and the council are bankrupt because the voices of the few who shouted the loudest have their way.</p> <p>To answer the question, no I cant think of an alternative that would not have serious financial consequences. I believe the council have considered all the viable options available.</p>

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John Moses				<p>Notwithstanding your considered improvements to your previous plans, and regardless of sustainability issues, any development on this site will:</p> <p>a) lead to traffic congestion at commuting times in Mandale Road at the A19 Mandale interchange, which you cannot circumvented because of the A19 roundabout and dual carriageway</p> <p>b) likewise in Brookfield congestion at some point is inevitable because the area you want to develop is surrounded by roads and housing that will lead to Acklam Road or Low Lane (two very busy roads) and the A174 is busy and leads onto Low Lane at the site of already newly built housing (which has already created extra traffic flow on Low Lane) and is a possible junction with your proposed development.</p> <p>c) will still destroy some or quite possibly all the natural wildlife habitat and the ancient woodland that currently exists on the proposed site</p> <p>d) remove much of the the only 'air-lungs' of the Acklam area which would be a travesty since the area is mostly a mass of housing as it is and is hemmed-in by major roads (A19 A174)</p> <p>e) build on land which is prone to flood and will be damp to build housing on since it is 50 feet lower than existing housing on and at level which is about the same as that of the old Tees river, which used to run not far from here (and I feel sure this will increase building costs)</p> <p>f) in effect, remove a barrier between existing housing and the air pollution from the A19 the new housing being much closer to the air-pollution from the A19</p> <p>g) will inevitably increase the volume of traffic in the Acklam and Brookfield and Trimdon Road areas in general, which already are very busy</p> <p>h) will still reduce the area available for recreation, particularly for dog walkers - who have increased considerably during lock-down and are now quite sizeable and use the whole area of suggested development more regularly than ever.</p> <p>I am surprised there is a demand for this extra housing. Since so much has been developed in the area (from Acklam through Brookfield and into Stainton) over the last 6 years. In addition, you are already due to build new housing at Hemlington/ Marton /Coulby Newham.</p> <p>And why is the council so intent on developing this area when the council had promised that the area would remain a green belt in the town?</p> <p>I can't help but imagine with this proposed development being so unpopular in the locality that the town councillors will bear the blame and the brunt of any fall-out from going ahead with such development.</p> <p>It seems unwise to me no matter how much money the council can make from this development to jeopardise the remaining good will between voters and the council.</p> <p>The council need to think again on the idea of housing and development here. There needs to be some land left for future generations to enjoy in the area.</p> <p>Otherwise residents in the Acklam/ Brookfield area will have to drive to the North York moors or Redcar to appreciate space and fresh air which is not what is wanted in terms of Carbon foot print for Teesside and the UK. (In fact the whole development would not help our Carbon footprint from any inception it may have onward.)</p>

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				Regards John Moses 10 Cranford Gardens Acklam TS5 8AQ. John.Moses2@btinternet.com

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John Roebuck	Middlesbrough Alternative Planning Partnership (MAPP)			<p>MAPP does not support the proposal for Stainsby country park and masterplan as it stands. The removal of housing from the Stainsby North site, by the Council, was undertaken to fulfil promises to protect green infrastructure. However, it is still unclear whether the spine road's primary function is to support the housing development by providing two separate access points, or to relieve pressure on the A19 / A174 junction. Clarity on this matter, would help in the design of the spine road, if this is to go ahead</p> <p>MAPP welcomes proposals from local community groups to further address environmental issues at Stainsby, e.g.,</p> <ol style="list-style-type: none"> a. To consider what additional features could be incorporated into the design of the road and supporting infrastructure, to minimise the environmental impact at the Mandale Meadow and northern access point b. Include measures to improve air quality, reduce noise pollution and any other relevant climate change requirements as set by the Government c. Council to spell out how it will meet Government emissions targets, by 2035 d. Consider weight limits and ensure HGVs use the Strategic Route Network, other than essential vehicles requiring access to the housing development e.g. buses, refuse collection etc e. Applying the Council's draft GBI strategic intentions to the Stainsby development f. Speed limits, particularly in relation to the proposed school, where other parts of the area have 20mph speed limits imposed <p>The Council is recommended to undertake the following actions and re-assess the requirements for a Country Park and Masterplan:</p> <ol style="list-style-type: none"> 1. Traffic issues need to be fully addressed in an open and transparent manner: <ol style="list-style-type: none"> a. acknowledge that the Arup study upon which Policy H21 is based, is vastly out of date (Appendix 1) b. use a new strategic transport model, approved by the Department of Transport and TVCA c. produce relevant, up to date modelling information, particularly changes in demographics across the Tees Valley, using latest official ONS population and housing projections (2020 based projections are scheduled to be released in March 2021) d. address junction issues at the A19/A174 and A19/A1130 e. in co-operation with Stockton Council, in light of their potential housing and commercial developments, e.g., Little Maltby Farm f. take account of the lack of progress in developing the Longlands – Prissick link road and Nunthorpe Grange park and ride g. consideration of public transport subsidies, more cycle ways and bike storage in line with the priorities identified in the Middlesbrough Local Implementation Plan 2. Housing numbers for Stainsby should be re-assessed, with revised numbers produced for: <ol style="list-style-type: none"> a. Option 1 loop road b. Stainsby site, if the Council is unable to identify alternative mitigations for the Longlands – Prissick link road c. Impact on housing numbers of the Council decision to protect Mandale Meadow 3. Develop a full business case, producing a cost benefit analysis of each of the four road options, together with the primary school and local centre <ol style="list-style-type: none"> a. Full financial costs, in both capital and revenue terms, together with associated funding streams b. Provide evidence of viability / affordability of each of the options for respective developers of the site and the Council and any other funding partners 4. The initial stretch of the Jack Simon Way does not appear to be fit for purpose as a spine road, given that the access has not been planned to deal with the volume of traffic that a spine road may attract. Council should address: <ol style="list-style-type: none"> a. Modifications to the initial stretch of Jack Simon Way in line with those identified for the north access, as a minimum b. Extending the strategic cycle path to the entrance to Jack Simon Way from Low Lane c. Further environmental improvements to protect the public health of residents of Jack Simon Way
JohnBage				I fully support the master plan,Particulary the construction of the spine relief road.It is the only solution to traffic problems that will be created for Brookfield and adjacent areas

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Judith Harbron				No development
Judith Maunder	Friends of Mandale, Greenstuff, resident very local to Mandale Meadow			There is an area of scrub land beside the A19, next to the pavement but inside the green space which could serve as a road if all the excavated spoil from building were used to level the area. This would take the spine road much closer to the A19 with a right hand turn to exit at the roundabout next to the car park
Judith Smith				<p>I do not usually get involved with comments about planning proposals but must say I am really concerned about the one concerning the spine road.</p> <p>Enough will have been written about the green concerns by people who know better and more than I, so I will not repeat them.</p> <p>My point is that it occurs to me that the easier you make it for us motorists to move around an area, the less likely you are to get us to use a different method of transport. More regular, convenient and cheaper public transport should be provided as an alternative to roads, down which drivers will speed, as they do down all roads in the 'boro regardless of the allotted speed limits. Or maybe provide a car share area so that people can 'double up' and help to reduce the number of vehicles on the roads.</p> <p>Do not give motorists more roads. Do not damage a beautiful, peaceful area and replace it with noise, fumes and dangerous vehicles.</p> <p>That is all. I do not have time, or need, to complete the rest of this form. I have had my say. I hope you will consider it.</p>

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Julia Povey				<p>There should be NO road built that cuts through any of Mandale meadows or woods.</p> <p>(With reference to document 'Initial Option Assessment Report, dated 25th Jan 2021, Version 1.1, page 9 -10)</p> <p>The council have already reached their target for house building. The 2014 Local Plan is out of date and should be reviewed and rewritten in light of what is actually needed at this time in this area and in line with current findings on the real value of greenspaces and the irreversible harm caused by building on them. I believe no more houses should be built in the areas of Coulby Newham and Stainsby which would result in no need for further road building, Option 1 would then be the best option and would preserve the greenspace of Mandale.</p> <p>If it was deemed that Option 1 is not viable, then the Option 4a and Option 4b (connecting to Option 3) should be reconsidered as these will allow Mandale meadows to be preserved.</p> <p>"Primary Road – Potential Expansion Position. An extension to the proposed road may be required to serve the site as a whole, accessing the site from its northern boundary. This is subject to area Traffic assessments and consultations with Highways." (Stainsbymasterplan pdf – Chapter 7 –Feb 21)</p> <p>Middlesbrough Council's independent review did not include consultations with the Highways agency so further consultations will be necessary. There are other options for the road, which need to be reconsidered when consultations with Highways are undertaken. Middlesbrough Borough Council is misleading the general public into accepting the independent review (consisting of council officers) in stating that the road is 'necessary.' Further consultations with Highways agency should result in considering all options that preserve natural greenspace and it should be in these consultations where it is decided what is 'necessary' and 'required'.</p> <p>We can't control what happens on privately owned land but there should be NO ROAD THROUGH MANDALE MEADOW/WOODS</p> <p>The natural greenspace of Mandale meadows and woods should be preserved. The natural habitats it provides and the flora and fauna that exist there cannot be replaced by the proposed so-called 'country park'. Having looked in detail at the plans for the country park, it will in no way be a 'country' park : its design shows a town park, with numerous paths and cycle paths cutting through small patches of small, mown green patches of grass. Any manufactured open spaces will not sustain the wildlife that exists now in natural meadow land. Newly planted small trees cannot compensate for the loss of semi-mature trees and the habitats they provide. In the current climate and the knowledge we now have about creating an imbalance in our ecosystems, it is unacceptable and unforgivable to destroy these natural habitats only to make way for housing and roads, when there are alternative solutions.</p> <p>The increased traffic caused by the proposed road will bring further air pollution and noise pollution. The council should be aiming to improve air quality and the quality of lives of the residents by reducing air and noise pollution. Air pollution can cause people to suffer from a range of respiratory problems and can exacerbate the conditions of people who already suffer from such conditions. The council have a responsibility for the welfare of its residents and the future residents of the town – our children. The death of the 9 year girl in London, whose death was found to be caused by air pollution, should move all councils into finding alternative solutions to new roads that would certainly create more air pollution.</p> <p>The speed limit on the proposed road will be exceeded by drivers (as all other speed limits on roads are) and the speed that traffic will travel at will not create a safe place for children to play and people to exercise their dogs.</p> <p>The planned 'quiet areas' will never provide the peace and tranquility and consequently the benefits to mental health that a natural, open greenspace like the meadows can. The current pandemic should have taught us all that these spaces are more important than ever for our health and well-being.</p> <p>The area of Mandale meadows is publicly owned; it belongs to the people of Middlesbrough. A deed of covenant, given in perpetuity, should mean that we will always be able to enjoy its beauty and the creatures that depend on it for their habitat should always be able to do so. The children of Middlesbrough should be able to continue to enjoy a natural greenspace with their children and thier grandchildren etc; we already have manmade parks like Albert Park and other similar parks in the town with mown greens and paths. Natural greenspace shouldn't be something we have to travel miles to experience and enjoy!</p> <p>We can't control what happens on privately owned land but there should be NO ROAD THROUGH MANDALE MEADOW/WOODS.</p>
Julie Riley				NO ROAD THROUGH MANDALE MEADOW
Julie-Anne Stephenson				Save the green space. Please don't destroy the natural habitats of so many wild animals and birds. There are plenty of other spaces to built, why take away this space which is enjoyed by so many. Our family have spent so much time here.
Kabaser Ajaib				I would strongly advise to reconsider a through road from mandate meadows

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Kabaser Ajaib	Retailing	Kabaser Ajaib	Retailing	No road at all because of noise pollution due to A19 Also cause asthma COPD. Already busy due to school next to it Red and orange listed endangered species and destruction of trees we want 0 carbon emissions not more this encourages more cars.
Karen Watson				Regarding Mandale Meadows, it won't make it better by putting a slip road in ,
Kate Bowe				I do not have a degree in Town Planning or in Civil Engineering. I disagree with a new road being built through one of the last little areas of greenspace that we have in Acklam. The documents published by the Council show that the plan to put a road through the meadow is fait accompli for the reason that it is the best scenario possible. This plan fails to take into account of the local residents. My objection is that any road is a road, it does not matter how prettily it is dressed up on your brochure. the road will have 30 mile an hour speed limit with traffic calming measures. The road on which I live has a 20 mile an hour limit which is never observed. If the plan is to alleviate traffic and take it off of Acklam Road then surely putting a road back to the A174 would be a much better option.
Kathryn Elms				I believe there should be an option of a look road that will not cause the destruction of an established meadow. I appreciate that with all the 1000's of extra houses being built, on what little green fields we have left here, there needs to be some form of infrastructure however to destroy this meadow would be a sacrilege. I can also appreciate the offer of a country park but we already have one! It's seems crazy to destroy one natural meadow only to build another which will take years and years to mature and get established! At what cost? A great cost financially and a great cost to the environment. Please, please consider an alternative before any action is taken. Once it's been destroyed we will never get it back.

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Keith Russell				<p>Any road required to service the new housing on private land should not be routed to the detriment of current public open space especially one that is highly used and valued by residents both locally and from wider areas.</p> <p>Back in 2014 the local plan consultation was poor, the impact/intention of the road was massively under emphasized. The people of Middlesbrough are now being robbed of a highly valued greenspace because of poor decisions made in 2013/14 on the back of lacklustre and poor consultation. A consultation that MBC are seemingly unwilling to provide details on.</p> <p>Suggestion; Present to the public the evidence that the information used as part of the 2014 'consultation' was accessible, transparent and informative. Present the evidence that notices/advertising of the consultation captured the areas that would be the impacted by the road because it is clear to me neither happened. Therefore a consultation just around the road should be brought forward. This time done properly with full transparency on the size, scale, impact and honest reasoning for and against so the people of Middlesbrough have a fair chance of making an informed contribution which clearly wasn't the case in 2013/14.</p> <p>Since 2018, when the true intentions of 'access' 'relief' 'spine' road became public knowledge MBC has received thousands of objections against the development of Mandale Meadow public open space for both houses and a ROAD. Public sentiment against this road is so strong that objections have made on at least 4 separate occasions over a period of nearly 4 years. Despite this MBC proceeded to remove protective covenants from the land title without consultation.</p> <p>Suggestion; Explain to the public why the protective covenants have been removed from the land title without consultation and after receiving hundreds if not thousands of letters objecting to the proposed appropriation and disposal of Mandale Meadow.</p> <p>Other major questions remain unanswered. The inevitable increase of air and noise pollution the road will bring, especially to those living in close proximity along Heythrop Drive, Farley Drive and surround roads who already suffer high levels of noise and air pollution from the A19.</p> <p>Suggestion; Carry out a noise and air pollution study from the east boundary of Mandale Meadow, release the findings to the the public, be honest are they already a concern even without the new road?</p> <p>Best outcome for residents, limit the number of houses to be built on the private land, limit the number to suit the existing road infrastructure, loop the road within the the new estate at the very least bring forward a consultation just on the road so people have a fair chance of understanding the impact and providing an informed response.</p>
Keith Young				<p>NO ROAD THROUGH MANDLE MEADOWS/WOODS. Please refer to document "Initial Option Assessment Report, dated 25th January 2021, Version 1.1, Page 9 and 10 and reconsider Options 4a and 4b or option 3 connecting to option 4b these are suitable for the purpose of this housing estate and will protect Mandale as a whole. Option 1 if no further houses will be built. As the councils independent review did not include consultation with the Highways agency then consultation at this time is pointless as further consultation would be needed, evidence as per doc Stainsbymasterplan-chapter-7-Feb21 pdf whereby it states : "Primary Road - Potential Expansion Position. An extension to the proposed road may be required to serve the site as a whole, accessing the site from it's Northern boundary. This is subject to area Traffic assessments and consultation with Highways.</p> <p>Therefore I conclude there are other options for the road, highways need to be consulted, the consultation documents are not saying this road is required, its a "may be" and not "necessary" as per MBC statement above which is misleading the general public into accepting the independent review which consisted of MBC council officers, is there a document on this independent review which needs to be provided. NO ROAD THROUGH MANDALE MEADOWS/WOODS.</p>

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Kelly kitchen				<p>1. Through road without jack simmons way up to and including mandale roundabout. Cause a major traffic issue as anti social behaviour.</p> <p>Parking will be an issue as jack simmons way already is.</p> <p>Dog poo - already a major issue.</p> <p>Jack simmons way - no speed limits cars along 50mph plus at times.</p>
Kevin Allgood				The road cannot go across mandate meadow or near to blue bell beck. The road is not required or wanted.
Kirk Trainor				Ensure adequate traffic management to prevent this becoming a bypass to the a174 junction and a disruption to local residents
Laura Hutchinson				This is absolutely ridiculous. People walk around bluebell beck all the time. This is an excuse to build a giant housing estate. The infrastructure cannot currently handle this. It is a stupid idea. Rethink it.
Laura McGregor				I would suggest scrapping the entrance to the north of the scheme through Madale Meadow and look into a road that enters the site at different locations to the south of the site - one from Phase 1 as currently shown, and another in a different location to the south of the development - creating a loop. Perhaps from the A174 to the west of Phase 1. I feel strongly that the road going through Mandale Meadow will be a deterrent to all who live in the area, notwithstanding the biodiversity and wildlife.
Lauren Teate				This road should be a loop road around the estate and not a spine road that will be used as a rat run. We already have too many cars using the roads in Middlesbrough. We are supposed to be aiming for net zero carbon and encouraging more cars to use the roads is not going to do that. This road flies in the face of your green plan!
Leah Davey				<p>The SWWE is NOT necessary. The proposed housing developments need to be reviewed as they are not appropriate given the changing circumstances. The council has a 10 year supply of available housing supply and alongside changes to policies the allocation is no longer required. The council would be justified in deallocating the site. Changes to policy and green aspirations render this road unnecessary. Simply because the road was considered necessary in 2014, that was a very different set of circumstances. it is not 2014 any more and the council should consider a change in direction. Especially your climate declaration and ambition to 'protect green spaces'.</p> <p>Please take into full consideration the representation you have received from CPRE and the open letter from Greenstuff Middlesbrough.</p> <p>The current road at Jack Simon should loop through the current housing. Further development should be halted at this site and other greenfield sites in Coulby Newham and Nunthorpe. This would then not impact on infrastructure and Highways England would not disapprove.</p> <p>This housing allocation is not necessary and should stop.</p>
Leigh Martin Sayers				New roads generate traffic. Continuing to build new roads to accommodate anticipated extra traffic is unsustainable in terms of the local and global environment and unsustainable in practical terms in a town the size of Middlesbrough. In the early 1990's transport planners at Cleveland County Council recognized this and planned to introduce a new light rail system similar to other major conurbations (which have been very successful in those areas). When government funding was not forthcoming the strategy switched to "green" bus corridors. Here we are 30 years later and the local Council puts forward a plan to build a road through a precious green area of the town so that residents to the south of the town can "cut the corner off" when heading into town via the A19. I contend that the strategy should be 100% focussed on getting people around without the use of cars or, at least, with reduced dependency on cars. I STRONGLY OBJECT to the construction of the SWWE because it will destroy a valuable green space, much loved by local residents as a precious and important place and because it will prolong and exacerbate traffic and traffic-related impacts on the environment and quality of life in the longer term.

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Liam Harris				<p>As a horticultural professional it is plain to see that Middlesbrough council do not have the capability to manage a country park. They have no real horticulture experience or knowledge anymore, this is evident by the way in which the remaining green areas are managed, pruning is all carried out incorrectly and at the wrong time of year, the standard of grass cutting and weed control is appalling, Albert park is a prime example of this, a once prestigious area that has been left to go to ruin, poor grass cutting, poor pruning, large empty beds, masses of littler and dead plant material. Adequately managing an area this size will require a dedicated team of around 10 full time horticulturalists. The notion that you can manage a 300 tree orchard is a pipe dream, anyone with industry knowledge would be aware that this is a full time, skilled job (pruning, management of pests and diseases, ensuring correct plant nutrition, harvesting, weed control ect.) and for this to be carried out correctly will be a large expense. There's no mention of the intended size/age of new plant materials, for something of this scale to work you will need to be using trees 4-7 years old and shrubs 2-3 years old. There's no mention of where the plant material will be sourced from, will local growers be used or will plant material be imported at huge environmental and monetary expense? The artists impressions are deceiving to the general public as they show mature trees (30+ years old), and manicured areas which are unseen even in areas such as around the cenotaph. The costs involved in this project will significantly outweigh the legal costs of removing the developers from the land and cause a large imbalance to an already delicate ecosystem.</p> <p>It appears that this has been designed to get the residents of Middlesbrough on your side and allow builders to profit without resistance from the residents. With regards to the road, a new road is simply not required, I live on the estate and if I'm heading north, why would I choose 20-30mph roads when I can be on the A19 within 3 minutes? If I'm heading south the road is of no use to me. The roundabout at Mandale is already an extremely busy roundabout and adding another junction will compound this problem. If you truly feel the need to add a road spend the money wasted on the country park and add a bridge and roundabout to the A19 to avoid causing more upset to the already dwindling areas of natural land around Middlesbrough.</p>
Linda Barker				<p>The SWWE is absolutely NOT necessary. It may have been deemed as such once but is not any more. It needs further consultation.</p> <p>The proposed housing developments need to be reviewed as they are not appropriate given the changing circumstances. The council has a 10 year supply of available housing supply and alongside changes to policies the allocation is no longer required. The council would be justified in deallocating the site. Changes to policy and green aspirations render this road unnecessary. Simply because the road was considered necessary in 2014, that was a very different set of circumstances. it is not 2014 any more and the council should consider a change in direction. Especially your climate declaration and ambition to 'protect green spaces'.</p> <p>Please take into full consideration the representation you have received from CPRE and the open letter from Greenstuff Middlesbrough.</p> <p>The current road at Jack Simon should loop through the current housing. Further development should be halted at this site and other greenfield sites in Coulby Newham and Nunthorpe. This would then not impact on infrastructure and Highways England would not disapprove.</p>
Linda Hyde				I am not sure there is another way unless the road could have access directly onto the A19
Linda Hyde				Leading the road directly on to the A19
Linda Jowers				I think if there has to be a road through it should be as near to the A19 as possible, so as to keep as much wild life and people safe,

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Linda Whitwell				<p>I wish to object most strongly to the destruction of the above Meadow planned by yourselves under the development name of Stainsby country park. This meadow is one of the last open spaces available to the people of Acklam and is vital to the survival of the diminishing wildlife, flora and fauna. Building anything on this land is not acceptable, it does not belong to the council, they are mere custodians, and destroying it would not benefit the local people in anyway.</p> <p>Green spaces in Middlesbrough are disappearing at an alarming rate. Places for children to play and run are virtually non existent resulting in them being forced to play on the streets and roads which is dangerous. The Bluebell meadow also provides space for water to drain following heavy rain or snow. The extra noise and light pollution would also be unacceptable. The possibility of more traffic converging on Mandale road or roundabout is very worrying. The volume of traffic is already at breaking point, to add any more is negligent and could make the council liable if any accidents occur as a result of this. We were all made aware of the dangers of air pollution last year when a young girl died due to pollution from cars. Creating a road or buildings which could increase vehicle usage close to housing and a school will be detrimental to health. This proposed project would cause anger and distrust in an already shaky relationship between elected councillors, politicians and the general public who pay their salaries.</p> <p>I was extremely surprised and disappointed to learn that the council had persisted with these unwanted and unnecessary plans to build buildings of any sort and a road on this important green space. Have you learned nothing from this pandemic? The availability of outdoor space is now more important than ever. No housing estates should be built if they do not include vital green belts, parks, trees and wide open spaces surrounding them for people to exercise, relax and de-stress. This pandemic has spread faster in built up areas. People have had to travel in cars to exercise in other counties. The councillors and MPs in Middlesbrough should hang their heads in shame. You are not putting your people first. There is plenty of 'brown' land around the town to develop. St Hildas could have provided essential housing for those wishing to rent from housing associations, however, the planned buildings were not what people wanted to live in. There are plots in the town centre where terraced housing has been knocked down which could provide affordable housing. To wilfully destroy the one remaining piece of green space in Acklam is disgraceful. You have already decimated the rest of the green belt . I do not agree to you damaging any part of the Bluebell meadow. You need to re-think your plans. The value of houses will drop and working rate payers will be forced leave the area which will result in less income for the council long term.</p>
Linthorpe/Acklam Resident				IT AINT HAPPENING
Lynn Blagg				No road and no housing
Lynne Smith				This is one of the only areas of natural beauty remaining in Middlesbrough, used by many families for exercise regularly. To run a road through it would be criminal, and it would completely ruin the area for the many residents.
Lynne woodward				We were promised we would not have this road and we will fight it all the way the pond was constructed when wildlife was transferred from side of a19 and has newts and other endangered inhabitants
M jordan				SWWE will cause traffic problems at the Mandale end, which already backed up at rush hour. It will increase the already unacceptable level of noise for all those who already live there. It will be a 'rat - run' through this proposed 'country park'
Malcolm Chilvers				It has been stated numerous times that this road is to help alleviate congestion on Marton and Acklam Roads, if that is not the case, as presented here then it is only required for the new housing proposed in item a) above. Item b) obviously has a plan behind it which the council are not telling us. Is this merely a case of putting the infrastructure in for future developments along this green belt. There is already severe congestion at the roundabouts at Mandale end and this will feed more traffic into it.

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Malcolm Watson				<p>I have nothing against a road per-se. But I do have strong objections to the creation of a rat-run. The idea that the solution to congestion on a 3-lane dual carriageway is to build a road through a housing development frankly beggars belief. I am told that there are expected to be 12,000 cars using the "spine road"/"relief road" or whatever you want to call it. I am not a traffic-management engineer but I have spent time in a lot of traffic queues over the years and, apart from the pollution and simple danger to life of allowing that much traffic in a residential area, I cannot see how the Mandale roundabout can be designed to cope with that volume of traffic in addition to that which currently exists on that roundabout.</p> <p>I assume that there has been a traffic management/flow report/projection done for that roundabout and for the short stretch to the Mandale/A19 roundabout as part of the Stainsby design but I cannot see how it can be made to work - it seems to me that:</p> <ul style="list-style-type: none"> • 12000 cars a day is, say, 6000 in each direction • most of the traffic will occur in, say, 12 hours (7am -7pm), an average of 500 an hour with some lows and a couple of rush-hours • for simplicity let's say that rush-hour peaks at 600 cars an hour (which is on the low side, I am sure) • at rush-hour 10 cars a minute will be trying to exit onto Mandale roundabout, which I believe is going to have traffic lights installed • I have stood at the traffic lights at the other end of Mandale Road (the Coronation corner) with a stopwatch and see that the lights' cycle is about 30 seconds (amber to Acklam Road through to red on Mandale Road) and in that time an average of 9 vehicles from 1 lane managed to cross the lights • Mandale roundabout will have four (busy) access roads which suggests that the cycle for each access will have to be similar i.e. a total of one and a half minutes until green appears again. In those 90 seconds 15 cars will have arrived but only 9 are able to go, and while those 9 are going another 5 have arrived at the back of the queue. So the next time it is green there are 21 waiting to go. And the next time there are 32 etc etc • if pedestrian signals are added to any of those lights that will, of course, make it even worse • traffic backing-up to the A19 roundabout, preventing vehicles entering the Mandale roundabout, will also exacerbate the problem of queues <p>The Mayor has said that the road "would have traffic calming measures and would have a speed limit of 20mph" (31st January 2021) . However, there has apparently been a response to an online question to which the answer was that it will be 30mph. It is, we are told, an extension of Stainton Way which is 40mph. Why is the Mayor trying to tell us there will be a low speed limit and what safeguards are there in place to ensure that the road does not later get "upgraded" to 40mph to accommodate the Stainton Way traffic volumes ?</p> <p>(The above query about the road was sent to the Stainsby email address and Paul Clarke responded on February 18th that it had been sent to "others in the Council more appropriate to deal with such matters" but no reply has yet been received)</p>

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Margaret Elliot				Instead of joining up with Mandale Road, connect to A19 roundabout without crossing the A19 first and control with traffic lights.
Margaret Jackson				A lot has changed since 2014. Before proceeding with Stainton Way Western Extension has any consideration been given to a review of it's necessity? A greater awareness of the need for urgent response to climate change and the pandemic, which was not foreseen in 2019 let alone 2014, have changed people's priorities. It maybe that this is no longer needed.
Mark Dent				<p>Mandale Meadow is a haven for wildlife, such as roe deer, frogs and birds, highlighted by the useful Middlesbrough Council information board installed in this beautiful green area. Any building work through this green corridor would destroy the habitats of the animals, only to be replaced by more 'toytown' house developments which we see right across the council area.</p> <p>I believe the council should concentrate on addressing some of the eyesore areas of the town (e.g. Gresham) where development has been promised for numerous years without any apparent progress, rather than destroying natural areas which are enjoyed by many dog walkers families and residents looking to get some fresh air and exercise. Please reconsider your proposals and review your decision to destroy the area with more identikit housing developments.</p>
Mark McCardle				<p>The proposed spine road is far too close to the site of scientific interest, and will cause considerable congestion at Mandale road and therefore Acklam road and Trimdon Avenue.</p> <p>The road alignment needs to be changed considerably and exit the estate further down onto the A19 Southbound.</p>

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Mark Warman				<p>I understand that there is some opposition to the joining of the spine road in the development with the Mandale roundabout. Having looked at the proposals it would seem that if there was no connection at this point, traffic would have a much extended route to leave the development and access the rest of Teesside, particularly for those living at the North end. This would increase the carbon footprint of the vehicles using the development and increase the load on Low Lane and the Southern portion of Acklam Road. It is possible that the inconvenience would deter some people from living there. The viability of the development could be in doubt if there was no connection at Mandale roundabout.</p> <p>I, therefore, support the connection of the spine road with the Mandale roundabout if the development is to continue.</p>
Martin deacy				Option 1
Martin deacy				<p>There is no need for this road and houses .bthe green belt is more important .</p> <p>Increase public transport and cycle lanes and think long term about reducing carbon emissions</p>
Martin Hammond				The link road will sever the Bluebell Beck corridor, an area so well used and valued by local people. Adding some greenspace within the development won't mitigate the damage this does.
Martin Sheffield				We support the Stainsby spine road improvement.
Martyn ROSE				Middlesbrough is well-served with housing, and has lots of brownfield space waiting for redevelopment. It's a terrible idea to develop on greenfield land and flies in the face of present-day sustainability efforts
Matthew Doyle				Loop road around mandale meadow. Or repurpose old ulitiy Road which runs along a19 and widen the small bridge would still allow the access to the roundabout at mandale and steer all traffic. With out the need to impact the meadow at all.
Matthew rowe	Community champs Middlesbrough			I'm in full support of the master plan, no objections from me
Michael Starford				<p>I am concerned about the "streamlined" link road adding to congestion at both the Mandale Interchange roundabouts. This affects traffic to/from central Middlesbrough (also main hospital route) and also Thornaby. This will increase tail backs on Mandale road that block exit from Heythrop drive Estate. The residents of Heythrop drive Estate have had noise and pollution introduced since the A19 construction but the Woodlands part of Mandale meadow have attenuated this.</p> <p>The proposed road/cycleway bisects the Meadow/woodlands in an obtrusive way and brings the extra traffic very near the housing. Whilst most developments in Middlesborough have been held at one end access (ie Trimdon Ave) we would ask, that if 2 end road access is desirable, can the road be routed along side the A19 boundary and take a more angular course up along side the A1130 verge to the corner of the present car park? This way the road could be better screened for the safety of pedestrians/dogs in the recreation area.</p> <p>(My comment under section 8 contain a different view on road alignment)</p>
Michelle Hedger				<p>The loop road option.</p> <p>No road through Mandale Meadow. Use the loop road option. If a road needs to be there make it a narrower road of 20mph. My children cannot play near the huge 30mph road planned. They can play now in the meadow. They will be hit by a car in this 'country park'.</p>
Mike Conyard				Do not build on this land. This is much needed green space. There are no parks in this area and nowhere for people to walk, kids to play and people to live their lives. I find it disgusting that you propose to build on this beautiful piece of land. Also, who wants to live right next to the A19? These house will be worth literally nothing.. The land is also a buffer zone from the A19 to the residents of Heythrop Drive and beyond.

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Mike Thomas				I pick a loop road. No building of any kind on Mandale field . This proposed development is basically saying Middlesbrough council and its elected representatives doesn't care about the environment or its residents opinions and is putting money first
Mike Turley				All green spaces should be protected, there is no need to put a road through the meadowlands. Please listen to the local people who use this space everyday got exercise and mental well being.
Miller Homes and Avant Homes	Miller Homes and Avant Homes	Mr Neil Morton	Savills	<p>Miller and Avant ('our clients') are generally satisfied with the proposed road alignment as it relates to their land interests (the remaining undeveloped land within the southern ownership as shown on the plan on p.15 of the masterplan. On this, the owners have requested that the plan on p.15 should be amended in the final masterplan to remove personalised reference to family names).</p> <p>We agree with the Council that the SWWE serves not only the Stainsby development but also provides strategic mitigation to the Council's wider housing development programme.</p> <p>The timely delivery of the SWWE is therefore of paramount importance. In this regard, the 2014 Housing Local Plan requires the SWWE to be completed by 2025 (Policy H5(d)).</p> <p>Against this, the northward construction of the SWWE from the A1044 has already commenced which has been facilitated through planning permissions M/FP/0572/11/P (Bellway Homes) and 17/0045/FUL (Miller and Story Homes). Indeed, the Stainsby allocation is currently being delivered northward from the south and the SWWE has already been constructed to the northern boundary of 17/0045/FUL awaiting connection to the remaining southern ownership land. In turn, our Clients intend to imminently submit a pre-application enquiry on the remaining southern ownership land with a view to a planning application in mid-2021. That planning application would deliver a further northward stretch of the SWWE to facilitate connection to the Pearson land to the north. From that point, the SWWE could be continued through to complete the connection to Mandale Road in the north.</p> <p>It is understood that the Council intends to submit its own planning application for the SWWE and deliver the full connection. This is welcomed and given the strategic importance of the SWWE we would like to see this planning application come forward at the earliest possible opportunity.</p>
Mr Neville stubbs				<p>Not to use Mandel roundabout, it is already busy in is current layout never mind another 1600 houses using this roundabout!</p> <p>We MUST use another route</p>
Mr Pattison				<p>I have great concern about the speed on the spine road at the moment.</p> <p>I live on Jack Simon Way, and with me working from home I can see the amount of traffic and the constant speeding up and down the road.</p> <p>We are in lockdown and we still have hundreds of vehicles everyday going up and down.</p> <p>The speeding on this road is really bad and it is only a matter of time before a kid is knocked over and killed.</p> <p>I have raised this with the Mayor and the council but they currently cannot do anything.</p> <p>The speed for this road should be cut to 20mph expecially when we have a school.</p> <p>There should be traffic calming measures in place all along the spine road as I fear this will end up a rat run when traffic is heavy on the a19.</p> <p>Please can you arrange to have this road monitored before you go ahead with any plans, because if someone is killed I will keep this email as proof that you were warned in advance by someone who is seeing these problems on a daily basis and will only get worse the bigger the scheme gets.</p> <p>Please try and put something in place now to get drivers used to the new limits or the country park will be ruined by excessive traffic and speeding.</p>

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Mrs D M Hornby				<p>In principal I object very strongly to the building of a major road across public open green space. Masterplan Summary. Page 5 Zero Carbon. Just how will a new road “ensure adequate reduction in car usage”?</p> <p>Given the amount of vehicle journeys likely to be made each day from 1700 houses with 2 cars per house, to say nothing of the traffic generated by the new estates south of Hemlington, this road will generate an unacceptable amount of noise, air pollution and disturbance which will affect every living thing, plants, trees, insects, birds, mammals and humans alike. And added to the existing noise and air pollution from the adjacent A19 it will be a health hazard. It is well documented that air pollution causes COPD, asthma, cancers, heart and circulation diseases, and as a town with an industrial legacy of ill-health, and the Governement urging us to get out in green space for our mental health, building yet another new road with its attendant pollution over open green space is just WRONG.</p> <p>The 2017 OSNA Final document states that Middlesbrough will “increase the level of Amenity and Natural Green Space by 1.5 square metres per person”. Just how much of the present space will be lost by building this road and its associated cycleway and footpaths?</p> <p>Stainsby Masterplan background , Feb 21 Map Option 3; The purpose of the road as stated “will improve existing capacity issues at the A19/A174 junction” i.e. divert this traffic down a 'rat run' and move congestion to the Mandale Road roundabout, causing traffic to 'back-up' both Levick Crescent and Mandale Road in the morning rush, and down onto the A19 in the evening rush, which happens now, and will therefore be much worse. And do you seriously think that a 30mph speed limit will be adhered to?</p> <p>Sadly I fear this road has been decided upon, no matter what objections are put forward. If this is the case, then the line of the road as drawn is wrong. It needs to be much nearer to the existing tree line on the western to northern edges of the open meadow, (which seems to have changed its name from Mandale to Acklam Meadow somewhere along the way) thus causing less visual disturbance, and leaving more accessible and visible open space at the beginning of the fields. It would also be a little further away from the backs of the houses on Heythrop Drive. Making the roundabout more of an oval shape into the dual carriageway area would mean the road entry would realign to accommodate this entry onto Mandale Road, giving a better sight line to and of approaching traffic. Sharper bends may also slow the flow of traffic.</p> <p>I note that the small car park at present in situ has disappeared. This will cause walkers, dog exercisers and car-share commuters to park in Mandale Road, Levick Crescent, Heythrop Drive, Chalford Oaks, Asterley Drive, or anywhere they deem appropriate, more congestion!</p>
Mrs Dorothy Gale				Don't really want a road but as it's inevitable could it please be as near to the A19 as possible away from the houses on Farley Drive and Heythrop Drive.
Mrs Nichola Dent				Unfortunately, I do not feel there is a need for new housing or a new road on Stainsby country park in the area of Mandale meadow. This is a wonderful expanse of wild green space, much needed for the people of Middlesbrough. After walking there today and seeing how it supports a whole host of wildlife such as birds, insects and wild flowers, I saw today two Roe deer exploring the land during the afternoon. The quiet the meadow affords away from the suburbs cannot be found anywhere else in Middlesbrough so close to the suburbs, and would be such a loss, never to be regained. Please consider reversing the planning permission to end the development of this natural wildlife haven for the people of Middlesbrough.
Mrs Barbara Postlethwaite				This is a beautiful area much loved by dog walkers, joggers and children alike. It must be saved for the future generations.
Natalie Turley				<p>Dont build any road. You only need a road if the private developer who owns the land decides to build on it. If you don't build a road then hopefully it will dissuade the owner from building and will save our meadow.</p> <p>It will destroy wildlife and create more even more congestion (too much for a small roundabout to handle)</p>

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Nichola Davey				<p>The SWWE is not necessary. The proposed housing developments need to be reviewed as they are not appropriate given the changing circumstances. The council has a 10 year supply of available housing supply and alongside changes to policies the allocation is no longer required. The council would be justified in deallocating the site. Changes to policy and green aspirations render this road unnecessary. Simply because the road was considered necessary in 2014, that was a very different set of circumstances. it is not 2014 any more and the council should consider a change in direction. Especially your climate declaration and ambition to 'protect green spaces'.</p> <p>Please take into full consideration the representation you have received from CPRE and the open letter from Greenstuff Middlesbrough.</p> <p>The current road at Jack Simon should loop through the current housing. Further development should be halted at this site and other greenfield sites in Coulby Newham and Nunthorpe. This would then not impact on infrastructure and Highways England would not disapprove.</p>
Nicola Smith				<p>We DO NOT WANT IT!!!!</p> <p>WHY can't you build on areas where you have flatted hundreds of houses but not rebuilt? Keith Road or Marton Burn Road??? This is the only green space we have in Acklam and hundereds of residents rely on this being walking distance. Pre covid the traffic on mandale Road, Acklam Rd and levick Cresent is bad enough at peak times without further traffic. You have Acklam Grange Secondary school round the corner from the meadow with 1200+ students that also use these roads. This is already a very busy junction without diverting more traffic to it. What about adults and children's mental health?? Has the past year taught you nothing?? I think having a open greenspace with all the wildlife and plenty of room to exercise and get much needed fresh air and been an absolute saviour to hundereds of local residents. This project has not considered anything but money. Looking at the plans there will be very little green left and it will be surrounding a road and housing estate. Absolutely nothing like we have now. This is NOT ACCEPTABLE FROM ANYONE I KNOW WHO LIVES IN ACKLAM 😞😞😞😞😞😞😞😞</p>
Nicolas Mitchell				<p>I think the route you have suggested in the least offensive to residents, but I understand it may be built on stilts. Is this because the ground is very soggy. If we have to have a road can it be built like Trimdon Avenue over the BlueBell Beck, with a tunnel. There is wildlife in the woods nearby and it may help deer, etc get across to the Meadows and Foxes Wood without getting knocked down. But I still query the need for a road.</p>
Norma Meehan				<p>In 2014 residents were not made aware of a Masterplan so were not able to object. The current Masterplan still involves the road shown in both the 2014 and 2018 Masterplan – surely a loop road would be more beneficial. The list of reasons against putting a road through Mandale Meadow is endless I will name but a few:</p> <ul style="list-style-type: none"> *Obviously destroying the land *In the region of 12000 vehicles per day driving across the meadow *Exiting at Mandale Road roundabout will increase traffic congestion *Moving traffic from pinch points in South Middlesbrough to the pinch point on Mandale Road will increase problems not solve them *The Government's target of being carbon neutral by 2035 is unachievable if a road goes through Mandale Meadow. *An increase to health <p>I ask again if you will reconsider and please will you urge the Council to think again.</p>
Pamela robinson				<p>We have wastelands on grove hill and pallister park that has been sitting empty since Mallon ripped them down. Build houses there as at the moment it's an eyesore. Leave the green spaces we have for the wildlife and future generations of children. We don't need a cultivated park. We don't need houses there.. We need uncultivated trees and grassland for the wildlife and the environment. I understand we need better access roads into mbro but that can be achieved without building houses which will cause more congestion. We have beautiful green space around mbro and Acklam area. Leave it alone for future generations. We don't need a cultivated country park. We need wild space that the wildlife can enjoy as well as the residents. We donot need more houses there.</p>

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Patricia Atkinson				Plan for trimdon avenue to mandale road spine and housing. Please reconsider due to increase of traffic to mandale roundabout, which is already appalling at rush and school hours. Perhaps an alternative route exit to A19 or A174. All that Bluebell beck a...
Paul Crook				It is a disgrace that more of our green space is to be handed to developers for housing in an already congested area of Middlesbrough. From a child I have played where new proposed housing is going to be and now take my children and dog here to walk and enjoy the nature.
Paul Dunning				Provide an extra lane southbound A19 from mandale roundabout to parkway exit
Paul Rose				loop road into and out of Low Lane
Peter Davey				The SWWE is NOT necessary. The proposed housing developments need to be reviewed as they are not appropriate given the changing circumstances. The council has a 10 year supply of available housing supply and alongside changes to policies the allocation is no longer required. The council would be justified in deallocating the site. Changes to policy and green aspirations render this road unnecessary. Simply because the road was considered necessary in 2014, that was a very different set of circumstances. It is not 2014 any more and the council should consider a change in direction. Especially your climate declaration and ambition to 'protect green spaces'. Please take into full consideration the representation you have received from CPRE and the open letter from Greenstuff Middlesbrough. The current road at Jack Simon should loop through the current housing. Further development should be halted at this site and other greenfield sites in Coulby Newham and Nunthorpe. This would then not impact on infrastructure and Highways England would not disapprove.
Peter Smith				Council again not thinking of the people just their own pockets. This land should be left for families to walk up and down and play on. By the sneakiness of the council that has always been the same without conferring to the public as this is public land it should be left public land. As we have the worst council in the north east, who over the decades have got rid of a lot of history from this town by knocking it down. You just have to look Gresham road area what should of been rebuilt. By the sneakiness of this planning you more than likely already have the permission to do it and it is futile for anyone in the local area to object so if somebody down there has any common sense. There is a bridge halfway down the a19 between the two farms why don't you widen this bridge and put slip roads on and off both sides to the a19 as we already have enough issues on mandale road levick crescent as it is with tail backs and a lot of speeding you do nothing about.
Poppy Bowyer				This road was proposed as a means to alleviate congestion and trafficking at busy periods leading onto the A19. Not to meet transport needs for development or mitigation for wider housing development programmes.
Robert Pounder				I would like to lodge my objection to this access/spine road. I don't think enough has been considered in regards to excessive traffic merging onto the Mandale road roundabout and the excess pollution and noise from many cars. I live in Minsterley Drive and at certain times of the day the road noise from the A19 is already very bad so another road would make this even worse! I walk daily along the path on the A19 side of the Meadow which has a rich diversity of native trees and hedges plus ponds with protected newts and frogs, this would be completely wiped out which in my opinion is criminal. Please reconsider this proposed road.
Robert Teate				Slip road onto the A19 would be an obvious alternative, taking traffic away from the built up and very heavily congested roads at Mandale end. Alternatively option 1 would be a reasonable choice to reduce the impact to the public and wild life in this area.
Ron Arundale				I would like to add my full support to the scheme including the much needed spine road.

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Ron Arundale				<p>I have worked for around ten years with the friends of blue bell beck to improve access for those wishing to enjoy the beck valley. As a group and with the help of the rights of way officer and even Ray Mallon at one point we have created a network of paths, had new bridges fitted, raised funds for and sited seating for the less fit and a sculpture trail for the kids to enjoy. Many task days have been carried out to maintain things from Mandale to Low Lane – some considerable distance for us on the grass cutter ! as well as trimming back growth and even planting native species (1000 bluebells). Not only that I claim to be the only person to have actually physically worked on the meadows when I removed all the ragwort which is poisonous to animals so they could be harvested for hay (arranged by our Secretary)</p> <p>Having said all that and my obvious commitment to the area it is my view THE SPINE ROAD MUST GO THROUGH even at the expense of a small part of the meadow as it is a crucial part of the road network relieving pressure on Low Lane and the Parkway A19 interchange as well as Acklam Road.</p> <p>I very much look forward to the new country park joining the beck valley (bridges over water courses as required) but hope adequate parking is provided to prevent nearby housing having visitor parking problems.</p>
Ronald Dennison				<p>I object to the inclusion of the spine road.</p> <p>But the proposed North/South spine road is at odds with a place which will be developing parkland where visitors are free to walk and cycle.</p>
Rosie Gott				<p>I object to the SPINE road that is proposed to cut through the open green space of MANDALE FIELD in Acklam, Middlesbrough.</p> <p>My understanding is that the land is protected by a REAL CONVENANT put in place by Mr. Horn in the 1930s. Then it was placed in the 'case' of Middlesbrough Borough Council TRUSTEES.</p> <p>The word 'TRUSTEE' in the strictest sense encompasses persons who serve on the board of an institution for the benefit of the general public or a person in the local government, to manage the property or land as provided by the terms of the document that created this arrangement in the first place. Trustees have certain obligations and must submit honest reports and fulfill the terms of that trust given to them. Covenants necessitate and official seal to be rubber stamped for validity and carry legal impact. This authentication makes it necessary that when authority is passed onto 'new hands', that the old should seal should be destroyed and a new one made. Has this take place?</p> <p>I have come across a term called' fiduciary duty' and surely Middlesbrough Council hold a legal and ethical position and relationship of trust to the vulnerable situation the public find themselves in, with these people having previously trusted in the reliance and protection of this covenant.</p> <p>As a Christian I know God takes the promise of a 'covenant' very seriously. It relates to a string, solemn agreement between to parties, involving promises between families and bloodlines, and sometimes God himself. There is the covenant of promise and the promise of law. It is a life and death agreement showing Gods true and loyal nature. If he says he will do something, he will not break that promise. Yes to day we break promises and make promises that we don't intend to keep. I have read that 'real property' law (running with the land) meets test of wording and circumstance laid down in precedent, imposed duties or restrictions upon the land regardless of the owner. Any future owners must abide by the terms on how this covenant relates to the use or enjoyment of the land, as well as includes if benefits the land, rather than an individual?</p> <p>As an ordinary, lay person and being non-qualified in the legal system, this is a difficult road to navigate. I feel there need to be a more open and transparent process in planning proposals. I favour the loop road. I will be praying that a way will be found to honour this 1931 covenant made by Mr Horn, for the people.</p>
Sam Matthewman				<p>Instead of the new spine road joining Mandale roundabout, there should be a new intersection built over the A19. This allows nature to be preserved and traffic to flow much better. People will be able to travel much faster with the direct route to an A-road. It may be more costly, but you cannot put a cost on native species which will be lost if the road destroys the greenbelt.</p> <p>If a road must be built through Mandale Meadow then it should run alongside the A19. It should not cut through the meadow and it should follow the existing path bordering the A19 currently used for walking. Nature should be protected.</p>

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Sarah currey				The road should not go through mandale meadow. Once there is a road its only a matter of time before houses follow. Also the road will bring pollution, congestion and more traffic noise into an area already suffering. The country park is not necessary as it is already an area of natural beauty that I'm not convinced a developer can improve on and we all know that developers don't keep their promises so no doubt will fall by the wayside to more houses. So we'll end up with a road at the end of the garden and the outside space we use daily ruined.
Scott Elliot				Make the North exit directly on to the A19 (Mandale Interchange) roundabout without crossing the A19 first.
Scott Pearson				<p>Middlesbrough Council are absolutely shocking. It is disgusting how local residents are not being listened to, the Mayor and Deputy Mayor have already publicly said that a road is going to be built regardless, all this consultation appears worthless.</p> <p>You are not listening to the community, you are not listening to the people that elected you ... WE DO NOT WANT A ROAD THROUGH THE MEADOW.</p> <p>Having grown up in Acklam and now living in Brookfield I find it absolutely criminal that anyone would consider ploughing a road right through a meadow.</p> <p>I don't think Middlesbrough care, it's all well and good saying that the previous administration did this or that, does that mean that the council have already given up without seeking alternatives?</p> <p>It's simple, listen to us - we don't want it!!</p>
Smith				Make a loop road back through the Stainsby estate as the A174/A19 already links Stainsby, Hemlington, Coulby Newham to the rest of Middlesbrough. Thornaby Stockton & beyond. The A174/A19 is already in existence and will not be detrimental to the health of the people or the wildlife in the Acklam area.
Sofia Torre				You still don't understand, and its unbelievable that in 2021, "A nine-year-old girl's fatal asthma attack has been linked to illegally high levels of air pollution. Ella Kissi-Debrah lived 25m (80ft) from London's South Circular Road - a notorious pollution "hotspot". She experienced three years of seizures and hospital stays before her death in February 2013." You are now going to bring the traffic that would be on the A19, through the spine road, closer to Acklam grange school and to the residents around the Stainsby plot. The entry points to Acklam Grange suffer when school is open and we suffer from pollutin from A19, the black dust that is on the white double glazing says it all. How can you be so stupid and ignorant of what you are about to cause. There is no need to suggest another alignment, don't build on it and we won't need it.
Sonia Bowler				<p>Option 1 should have been given more consideration.</p> <p>No spine road through Mandale Meadow!</p>
Stacey baillie				I'd like to understand the full plan please not notice of a country park and an outline with no clear understanding of the potential impact

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Stephanie Atkin				<p>Option 1 - loop the road. No road through Mandale Meadow</p> <p>Sad to see costs being cited as a reason to reject routes. What a shame the hidden costs of decimating greenspace can't be calculated: The health impact that has to be picked up further along the line by services already stretched; the climate emergency; ecocide. If all of these things had a price tag that could be viewed from the outset it would clearly signal the need for a BIG rethink. There is a burgeoning movement that accepts the urgency of this nationally and it will eventually make its way into planning policy, no doubt after much wrangling with the parties who have been running their hands together all this time. Middlesbrough has a chance to be ahead of the curve but instead this plan screams 'Middlesbrough: Moving Backwards'.</p> <p>The vision for a country park is a smokescreen and a cynical attempt to deflect from the wanton destruction. Why must we always presume to 'manage' nature? To shoehorn it into a site in order to maximise the profit afforded by the land? Why are we not able appreciate the value of a truly wild and natural meadow without footpaths, cycle lanes and other contrivances? These questions are, of course, rhetorical because a large portion of the electorate already have the answers and are willing to voice them to those who will listen. All that is needed is someone in a position to exert some influence who is able and willing to do so.</p> <p>The wildlife have a greater right to this space than any of us. They are not being consulted. Let MBC be the voice rather than a mouthpiece!</p>
Stephen Connors				<p>None. We fought for months with the council to protect the green areas around Acklam to the point where the council cancelled their plans. In my opinion the Stainsby park is a pathetic excuse to make it sound as if the area will be untouched after building 100's of new homes. Middlesbrough council is a joke and you need to screw your heads on and realise this is not wanted & we want to keep our green areas green, Not taint them with houses. A housing estate with fielded areas is still a housing estate. Sort your selfs out.</p>
Steven Brown				<p>aligned to minimise its impact on the Meadow ! This would be achieved by not creating such a large road. Its obvious that the additional traffic is going to be created by the additional houses .at least 2 houses per property The A174/A19 interchange handles all the traffic at this present time, the only really busy time is morning rush hour</p> <p>Due to the road structure at Mandale Roundabout a backlog of cars would be created on the proposed exit road, it would be better to send the cars to the existing A174/A19 roundabout. Has a one way system been talked about ? in at one end and out the other ? to cut down on the inevitable temptation of using the road as a rat run Multiple access points on 2,3,4? two ? one more than option 1 Bearing in mind the amount of new properties that will also be being built on the new Hemlington,Stainton areas along with the possible village build on Stainsby Hill Farm . This is going to be a very busy area and the existing road structures also need massive investment to be able to handle all the additional traffic</p>
Steven Jackson				<p>As I do not have a comprehensive understanding of the road infrastructure I am unable to suggest an alternative road route to the one that will cut through Mandale Meadow but I am sure that MBC have enough intelligent people working for them that they can find an alternative route to ploughing through this beautiful, natural meadow which is rich with biodiversity and accommodates many wildlife species. Come on MBC, lets protect our greenspaces and wildlife and lets think outside of the box! This is totally unnecessary as surely you can source an alternative route to this - its heartbreaking and Ecocide!</p> <p>I know several ideas will have been suggested to MBC, such as a loop road similar to the one at Trimdon.</p>
Stewart Jones				Says all that needs to be said (referring to attachment)

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Stewart Jones				<p>I am becoming more confused the more I read. In fact I am becoming dizzy with all the detail scattered about in your documents. here is an extract from the current Master Plan currently being consulted on</p> <p>The location and design of the primary road has emerged following three consecutive rounds of community consultation. The road connects newly completed development at the south of the site through to the area of existing farm to the north. A further connection will be created through to an enhanced roundabout entrance from Mandale Road. This Northern extension of road, that passes over Blue Bell Beck, is essential in providing the adequate transport capacities to service the allocated development site. This entrance arrangement will be carefully considered to minimise any impact on to the Green Open space and creating a gateway to the site. The configuration of the road is otherwise considered to be the most appropriate solution.</p> <p>Connection from the development areas to the Primary Road will be wherever possible exclusively from roundabout situations.</p> <p>The design of the road is subject to junction capacity studies yet to be undertaken and will need to be developed in conjunction with detailed applications. Landscape edge conditions and abutments will be addressed within section</p> <p>So in 2018 the road was in principle accepted by MBC after enhanced consultation with the public Her is an apparent acceptance of this issue by the pressure group currently opposing the road</p> <p>MBC then embarked on the folly of engaging external consultants to look at alternative routes. A route to west would mean expensive crossing of the A19 then cutting through a protected green wedge of the Stainsby beck and exiting in another authorities land. Never in a thousand years would this be acceptable A loop road which is just plain "loopy" in suggestion</p> <p>the point I am making is if in 2018 the road was accepted in principle why have MBC failed produced detailed analysis of the road and the effect of its infrastructure on the local wild life site, which should be the topic of this consultation, providing the public with the proof of what impact it will have,, robust evidence Why is the deputy Mayor challenging the road ?</p>

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Name	Organisation (where applicable)	Agent	Organisation	SWWE - Please provide comment here:
Stewart Jones				<p>Can you please tell me where I can find MBC latest report on the access road at Mandale and its impact on the wider network and what infra structure will be needed on Mandale meadow through a local wild life site, and what impact this will have on the natural habitat</p> <p>This is from your HLAA 2018 so I expect a report after this date thank you . where road access is identified on the assessment schedules this is on the basis that a site has road frontage, but no assessment of the acceptability of the access point or of its capacity has been undertaken; and iv. the impact of the identified sites individually and cumulatively on the road network and other infrastructure has not been assessed.</p>
stewart jones				<p>Mandale was never consulted on in 2013/4 this land did not have a sustainability assessment and is part of the protected blue bell beck valley it contains a registered local wild life site,It is also a known habitat of the nationally protected water vole and is deemed a site of interest and nature conservation.Therefore i would like a full site survey and details made public of the exact route what damage will occur what mitigation steps will be taken at the beck crossing,How will the road be separated from the open space, a robust barrier will be required</p>
Sue Shannon	South Tees NHS Trust			<p>Love the plans for the park but the through road should not go ahead. It will just become a shortcut off the A19/used to avoid the A174 & spoil our nice new estates, surely you can see that!?!?</p>
The Countryside Charity North Yorkshire CPRENY	CPRENY			<p>Further, the masterplan sets out the positioning of the new primary road through the site connecting new development at the south of the site through to the existing farm area to the north. It goes on to say a further connection will be created through an enhanced roundabout entrance at Mandale Road – passing over Blue Bell Beck. CPRENY has significant concerns that this will cause significant detrimental impacts to the existing local wildlife site at this location. Whilst compensatory measures are a criterion of Policy H21 of the HLP, the level of construction work which will occur because of the development of the wider site and the timescales involved will likely mean habitats will be disturbed and are unlikely to recover. CPRENY has recently seen evidence of newts on site and urge the Council to consider the implications for protected species.</p> <p>It is further understood that the HLP was partly based upon the March 2014 Infrastructure Delivery Plan, which assumed the delivery of housing allocations in Brookfield, Hemlington Grange, Coulby Newham, Stainton, Nunthorpe Grande and Low Gill. The MC five-year housing land supply document sets out that no housing is planned at Coulby Newham or Nunthorpe Grange in the next five years in the same way as at Stainsby. As such, the new road is not required to deliver development.</p>
Tony slater	MJBON/A			<p>Access to the Mandale area of additional road links are likely to cause further traffic congestion and further pollution.Access to estate from the thornaby side may help.</p>
Town Clerk	Thornaby Town Council			<p>Thornaby Town Council formally objects to Middlesbrough Borough Council’s proposals contained in the Stainsby Masterplan to drive a road through Mandale Meadow – thus overloading the Mandale Interchange with huge increase in traffic that will once again adversely impact on Thornaby on Tees (as well as Acklam) and no doubt lead to increased congestion and accompanying pollution.</p>
Trevor Davis-Webb				<p>Please no to road through Mandale. Please consider a loop road</p>

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Trevor King				<p>There should be NO road built that cuts through any of Mandale meadows or woods.</p> <p>(With reference to document 'Initial Option Assessment Report, dated 25th Jan 2021, Version 1.1, page 9 -10)</p> <p>The council have already reached their target for house building. The 2014 Local Plan is out of date and should be reviewed and rewritten in light of what is actually needed at this time in this area and in line with current findings on the real value of greenspaces and the irreversible harm caused by building on them. I believe no more houses should be built in the areas of Coulby Newham and Stainsby which would result in no need for further road building, Option 1 would then be the best option and would preserve the greenspace of Mandale.</p> <p>If it was deemed that Option 1 is not viable, then the Option 4a and Option 4b (connecting to Option 3) should be reconsidered as these will allow Mandale meadows to be preserved.</p> <p>"Primary Road – Potential Expansion Position. An extension to the proposed road may be required to serve the site as a whole, accessing the site from its northern boundary. This is subject to area Traffic assessments and consultations with Highways." (Stainsbymasterplan pdf – Chapter 7 –Feb 21)</p> <p>Middlesbrough Council's independent review did not include consultations with the Highways agency so further consultations will be necessary. There are other options for the road, which need to be reconsidered when consultations with Highways are undertaken. Middlesbrough Borough Council is misleading the general public into accepting the independent review (consisting of council officers) in stating that the road is 'necessary.' Further consultations with Highways agency should result in considering all options that preserve natural greenspace and it should be in these consultations where it is decided what is 'necessary' and 'required'.</p> <p>NO ROAD THROUGH MANDALE MEADOW/WOODS</p>
Val Young				No road through the meadow. Build the loop over the A19
Vanessa collett				The stainton way extension plan is about to decimate an area of green space that it used by the community. It's a habitat of flora and fauna, and in today's world of looking after the environment and climate to do this is reprehensible. Children use the land to play, in an era where they are encourage to get out more for their well-being. Adults use the area to run, walk and cycle, it's an area we've come to use the area a lot.
Vicki Lester				Option 1
Yvonne Cotton				I do not believe that there should be any road or housing on this natural land. A 'country park' with a road through it isn't viable to keep and sustain the wildlife that is already there
Dave Blackburn				I do not want a spine road though my field. If I have to pick a option it is the loop road. I don't understand why you have to spoil this beautiful environment when you have a cheaper alterative that nearly ready at low lane. You will be creating a monster.
Doris Hammond				I have lived in Farley since 1954, and was given to underrated when I bought the house that there old be no building or roads on the field as it was protected and had backs. Please do not put a road through field. Please save it for the children. I choose the loop road. At the moment the trees out the back shield some of the a19 noise, please do not touch the trees. It will take years to grow new ones.
Gill Sullivan				<p>I am opposed to the intended spine road through mandale field for the following reasons</p> <p>MBC did not as their keep insisting conduct a full masterplan in 2014. It is disingenuous for them to say that as they did no such thing. Few people knew about it despite the massive road and thousands of houses on the new development. Not only should all the residents on Heythrop Minsterly Farley have been informed, so should all the roads that are going to be affected like Mandale Croft Acklam Road. This is MBC duty to the residents, their are soon to serve. You cannot get away with your bad mannered and dismissive attitude by saying yes we did our job for the residents. When it is blatantly obvious that is not the case. The resident have not been consulted as MBC say and now it is time to halt this and remedy some of this bad feeling by been grown ups and having grown up conversations.</p> <p>My pick is loop road as the only viable option.</p> <p>Their is also a case to argue that a full and transparent consultation was not completed in 2018 either. How could it be when MBC: Thwarted residents attempts to obtain public and relevant information. Refused to engage with residents regarding an alternative route. Even now check how MBC continue to say how open and</p>

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				<p>transparent they are when nothing has changed. Requests through clrs taking 2 to 5 weeks in a 4 week consult. FOI acts having to be used and having to keep ringing back for information. You use the old staff and system and expect change. It just wont happen regardless of having a new mayor and deputy.</p> <p>It took a post that the residents know that their was an alternative to MBC preferred option. Not only an alternative but:, a cheaper option low lane, had infra in place and would save madale. but this option at low lane or the developers want as their choice. both mbc and developers will need to take a cut in their anticipated funds and low lane residents will not be happy having a loop road estate.</p> <p>Damaged neighbourhood and field and upward of 4000 adult plus children and cars on a massive estate to share their lives and amenities with.</p> <p>Residents are well with their rights to challenge MBC their are not being selfish nimby's or trying to thwart the improvement of Middlesbrough their are acting from love of mandale and ultimately showing where mbc loyalty should lie. Being responsible adults and serving the fields for future generations. MBC insult our intelligence as they try to justify what will be wilful and destructive behaviour when their is an alternative. They have the audacity to suggest we will be getting like for like. this is not true.</p> <p>Acklam residents as they are sacrificed for the greater good of Middlesbrough will see the loss of open green space and gain a country park that they will not use that may not even be developed and a massive estate.</p> <p>The losers the residents not only for today will suffer for the rest of their lives and for generations to come. Surely MBC can understand short term gains does not warrant the damage to the only wild green space we have.</p> <p>I understand MBC are under pressure from GOV, to provide 7000 homes within certain number of years to avoid negative consequences but their is more up to date data that may make a difference. Gov guidelines drop by 19%. As their are 9000 housing available to planning it would leave a surplus of 2000 houses leaving the 7000 remaining. So a 5th of 7000 would bring us down to 5600 and as their are 9000 available it would leave 3400 over what was necessary. Mr Clarke can keep some of his surplus and then their would be no need to build on the farmers land. You are trying to draw in a certain wealth of householder but who wants to live in expensive new build on the site of A19? Their want old stock in the quiet.</p> <p>The big new road build is largely a distraction from the less glam job of fixing the road we have in as efficient and green was possible. Do the adjustments needed on the infrastructure already built much cheaper, better for road users, and our green fields and environment.</p> <p>MBC inflexibility on their preferred option is not helpful and is causing distrust and bad feeling.</p> <p>Acklam are not the only consideration here. As the relief road will exacerbate the congestion in thornaby and stockton. Steve Walmsley and his thirteen councillors all oppose this road. It would be reasonable to suppose that MBC would talk to them as it is all part of their duty. If it effects other boroughs or wards. It would appear to residents that this is not the case. Why not?</p> <p>As I understand it MBC legal requirement on this build is the infrastructure, but that requirement does not have to be the spine road when there are other alternatives. MBC use words like, critical and essential around the service road, THIS IS NOT CORRECT.</p> <p>MBC say they could face fines from developers and or the GOV, if they don't meet deadlines and this masterplan needs to be pushed through. This is not correct. There is a loophole in the 2014 plan, if MBC leaves the 2014 masterplan in place then developer cannot enforce fines. and the majority of MBC developments will be safe.</p> <p>Clrs that tell resident that this is inevitable and the road must go through are again been economically with the truth there is an alternative. The truth is this is driven by political will and political will can stop this. MBC could at this stage halt the consultation with their developments on the whole, safe. Leave the 2014 plan in place then their could be a wide consult on the service road and also Middlesbrough's wider infrastructure. exactly what should have been done prior to 2014.</p>

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				<p>These problems MBC are having with the residents have come from MBC own behaviour. Their are responses to MBC arrogant dismissive and high handed attitudes. MBC set the tone. MBC have been obstructive and difficult and they have clearly not understood their residents deep and abiding love of our open public green spaces. MBC state they have listened but he service road is still going through open green space. Well there are tow options here.. MBC have listened and not understood or MBC have listened and deliberately misunderstood and are choosing to ignore residents needs and wishes. In preference to their own and developers.</p> <p>MBC have only two ways to control developers here. Limit the infrastructure and limit by design. that beautiful work of art and design will soon be compromised when developers.</p> <p>Acklam does not need a doubling of residents, so limit greedy developers and do not provide the infrastructure. No Road Through Mandale. MBC received its biggest response in their history over the disposal of mandale field. You can say you have listened ... proof... field has had houses removed, field is no longer consider for sale. But is this because you understand your residents needs or this because it suits council, because the road is what was needed from the outset. Were the houses another ploy like Newfield crescent to distract us from what was MBC ultimate goal? We can only guess and speculate as their is no information from MBC. from where I see it, MBC has every intention in railroading their residents rights. You need the money I understand. 60% of your income comes from CT revenue a few years ago, and having to go cap in hand to the GOV for extra homes must be gaoling. This whole situation is unfair all around, but this public open green space we are talking about. Once its gone its gone forever. it was gifted in 1931 and has been kept safe until now. MBC head is turned by money, much needed money. But GOV remove covenantt or not there must surely be times when morally you should not. This is one of those times. A defining moment in its history. MBC please come down on the side of residents and save our field.</p>

Question 2 – Please provide any comments you may have on Chapter 1.0 Introduction

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Name	Organisation (where applicable)	Agent	Organisation	Please provide any comments you may have on Chapter 1.0 Introduction
Alexander Henegan				The avowed aim to create a “feeling of living in a park” will be hard to achieve, especially given the plan to build 1500 new homes. The number of cars that will occupy the area will be 2000 plus, which can hardly help to promote a park. Much of the language used in the document is inflated and unclear, using solecisms and masking what the development actually is—a very large housing estate.
Aiden Woodward				<p>The existing masterplan on page 12 has beck crossings to existing houses over to Acklam, which from what I can tell don't feature in the updated masterplan on page 13 and can't see any mention of anywhere else in the document.</p> <p>I think the crossings were a good idea, and if they aren't included in the updated masterplan should be reconsidered. For those with young children of primary school age now and in the near future, the children will have to continue to go to one of the other primary schools in the local area until the new school is built in a few years time, presumably at a time when the children currently in primary school will have already left and be in secondary.</p> <p>Obviously it is preferable to walk children to primary school where possible, traffic is reduced and pollution in the local area is reduced, however with the current paths one would have to walk to the entrance of the area near low lane to be able to walk to the existing schools, which as the development gets further in will become a longer and longer walk. Crossings would help ease this journey to encourage walking, and could be done much earlier than the new school would be built.</p>
Aji Varghese		Aji Varghese		Good
Alan Liddle	Stainton and Thornton Parish Council			The current road down Jack Simons Way does NOT have a designated cycle route a clear omission from the Zero Carbon/Sustainable Transport section of this chapter.
Amanda Stevenson				I found the whole document quite difficult to read, it would have been better if it was written in plain English. I am concerned that the authors of the masterplan do not understand the area. For example in their foreword at paragraph 1.1 they refer to Middlesbrough being a city, it is not. The document says that the population of England had increased by about 10% but does say what the increase is Middlesbrough is, are these houses actually needed or are they being built just to provide revenue.
Amy Lord				The country park element of this development has been promoted as a great selling point and the pictures depict a tranquil space for residents to enjoy. Anyone who walks regularly on Mandale meadow and the surrounding fields will know that traffic noise is already incredibly intrusive. These houses and the park will be even closer to the A19, with the new spine road on the other side. The traffic noise will be unbearable and constant, meaning this vision of the country park will never be fulfilled. Given the land is also marshy and prone to flooding, much of it will become muddy and flooded, making it unusable for large chunks of the year. The plan also talks of 'new habitats' that will be created. The estate and the road will destroy existing habitats, it may take years to replace them. In section 1.4, it states that the new housing should be of a density appropriate to the locality. Adding over 1,500 houses, on land right beside a major highway, in an area that already struggles with traffic, does not seem to meet that point. With so many new developments in and around Middlesbrough, some of which have not sold, why are another 1,500+ houses required? These area was added to the Local Plan in 2014. Seven years have passed. Climate change has subsequently been identified as a major impending crisis: a project that will destroy greenbelt land with diverse habitats in order to build houses and new roads is not the approach we need to take. We should be preserving out natural habitats, not destroying it for commercial reasons.

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Andrea Spooner				The problem with Middlesbrough Council is that they ruin every nice area and natural open green space that exists. The country park is a great idea to enhance the area for both local residents and for people to come from further afield to enjoy but is counterproductive as the cost is ultimately peoples health. I work within the cardiovascular division of the hospital and know only too well what Middlesbrough residents need for their lives to be healthier, longer and more fulfilled. I have watched the impact of poor health on the residents of Middlesbrough for the last 25 years and the devastating impact on families when they lose members of their family way too early.
angela barber				AS a resident of this area, I fear my house value will lower so close to this new road. There will be No where to walk my dog There would be Little or no space for children to play safety in area An area to exercise my self and enjoy picnics with the family Local wild life will loose there home and endangered species plants and animals will loose there environment Why should be suffer ill health because of further pollution We pay council tax for green space- will our council tax lower-expect not Increased noise during the day and night- lack of sleep and depression result All you want to do is provide new access road to the estate- why cannot they come around down downside and Acklam road instead as always has happened. I feel the new road will increase traffic into Middlesbrough through our area. I do not want or need the road as I do not drive- I doubt it will be included on the bus route, so it does me no good. In fact everything about it is BAD
Angela O'Byrne				Hands off our meadow!
Annmarie Barry				My concerns are about the Meadow, not the housing development. I think the plans for the country park will negatively impact the fauna and flora of the meadow. I do not support any house building on the meadow.
Barbara Keville				1.3.2 Landscape, Nature and a New Country Park The plan sounds idyllic but why do we need to formalise an area so much. New habitats created....what is wrong with just enhancing the ones that already exist? Planting orchards...to what end...ideal for vandals. The area is already a natural haven for flora and fauna and just requires management. 1.3.3 Community and Connectivity Primary school..yes. But why a community and commercial hub and formal sports facilities when Kader Community Centre was closed many years ago as it was deemed too expensive for the council to run. Formal paths....in the early days the council let the footfall determine where the paths would be and then put hardcore down where people had chosen to walk. Bridges followed and the whole existing network of paths has worked. Too formal will be a negative effect on existing wildlife etc.
Barry pinkney				Country park no
Barry, Ann and Craig Pearson	Stainsby Hill Farm	Michael Mealing	Michael Mealing Planning	Paragraph 1.3 – The current western alignment of the road and the distribution of the housing blocks proposed along its length, will not necessarily achieve the desired blurring between the western Country Park and residential development. In part, the alignment will create a firm physical barrier separating the Country Park from the general housing area. Where the road abuts the Country Park, it will interfere with both the visual and the functional relationship of the Park with the wider development, creating a hard edge to the Park and presenting a constraint to unhindered access by pedestrians and cyclists originating within the housing areas.

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Barry, Ann and Craig Pearson		Michael Mealing	Michael Mealing (Planning)	Paragraph 1.3 – The current western alignment of the road and the distribution of the housing blocks proposed along its length, will not necessarily achieve the desired blurring between the western Country Park and residential development. In part, the alignment will create a firm physical barrier separating the Country Park from the general housing area. Where the road abuts the Country Park, it will interfere with both the visual and the functional relationship of the Park with the wider development, creating a hard edge to the Park and presenting a constraint to unhindered access by pedestrians and cyclists originating within the housing areas.
Ben Stephenson	Persimmon Homes Teesside			<p>Persimmon Homes welcome the opportunity to submit representations on the Stainsby Country Park and Masterplan Document.</p> <p>It is hoped that these representations will assist the Council in the production of a Masterplan which is unambiguous, avoids overly prescriptive detail yet encourages and directs a sense of place and variety.</p> <p>We are therefore keen to work with the Council to ensure that it takes forward an implementable Masterplan which will help meet the aspirations of the town and its residents whilst providing landowners, developers and interested parties with a clear yet flexible framework from which to plan the development of the site.</p> <p>Persimmon Homes do not have a commercial interest in the site therefore the purpose of this letter is to act as a “critical friend” to the Council, highlighting any areas of concern that we have with the document that we, as a housing developer, believe may cause problems at a later stage of the planning process</p> <p>In the order in which they appear within the document, Persimmon Homes Teesside would make the following observations:</p>
Beverley cooper				Cant access plan
Carol Newmarch				I object to the whole plan

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Catherine Howell (Boyle)				<p>1.3 The development of housing on a current and well used green space diminishes the available green space accessible to residents.</p> <p>1.3.1 No road = no need for a change in driver behaviour. A change in behaviour is only required because additional roads are being built.</p> <p>1.3.2 Visitor parking - how many anticipated visitors? Reference should be taken from Flatts Lane, Wynyard etc. currently receiving high visitor numbers that cannot be accommodated with parking facilities. What provision would be made for sustainable transport options? New habitats would only be created at the expense of existing and precious habitats. The are currently hosts rare butterflies, flowers etc. How are these to be protected? How can these be protected if development takes place?</p> <p>1.3.3 'bluebell beck' = Bluebell Beck.</p> <p>1.3.4 The whole area is currently public space. Building on 47% of the area in fact reduces public space.</p> <p>1.4 2014 Local Plan. First, this was enacted with minimal public consultation. Second, the context is now entirely different, with a greater understanding of the Climate Emergency that is faced. (Acknowledged by Middlesbrough Council and manifested in the One Planet Living framework adoption). It is therefore no longer fit for purpose.</p> <p>Housing requirements - there are substantial numbers of empty houses in many central areas of Middlesbrough, which are far more suitable for people wanting to access the town for work and leisure. Given the current situation, with rising unemployment, is it appropriate to be developing in green spaces for commuters? There is a net fall in population in the town currently - is the development being considered in this context?</p> <p>1.4 i, k - any development will be detrimental to the current wildlife and habitats in this area. Loss of habitat is acknowledged (k): how does this sit within the One Planet Development framework adopted by the Council as its environment strategy?</p> <p>1.4 l Would the SUDS scheme etc be required if development was NOT to take place?</p> <p>1.4 CS5 (p10) This area is not part of Middlesbrough's built environment. It is a green space at the margins of the town. Middlesbrough Urban Design Guide (p10) a. The development does not contribute to the wider environmental benefits of the town. It will negatively affect the town by increasing air, water and noise pollution and damaging important habitat.</p> <p>1.4 OPL framework - Zero Carbon: spine road usage is anticipated as IRO 12,000 vehicles per day. "This development actually increases car usage. Opportunities for buildings to utilise renewable measures..." - this is not the same as actively ensuring that they do! Sustainable Transport: Cycling commuter route - again, if this is the case, why is the spine road deemed necessary? How will existing cycling infrastructure (currently appalling) be maintained/upgraded to cope with the increased usage the proposed development will bring? Why not develop land for housing closer to the commercial centre on brownfield land instead of developing in green spaces? Sustainable Materials: "A degree of sustainable materials" - totally arbitrary. How will this be calculated, evaluated, enforced, reported? What are the benchmarks for this? Local and Sustainable Food: "There may.." Again, arbitrary. Who will manage these spaces? How will food be allocated, distributed? How will the sustainability for this be ensured for the future? How will inputs be costed and budgeted? There are currently orchards at Nature's World and at Larchfield, 'farm shops' at Larchfield and Thornaby. These accommodate the local population adequately. Sustainable Water: there is a problem already with localised flooding in the area, with areas of standing water. Developing impermeable surfaces through development will massively increase these problems. Again, a SUDS scheme would not be required if the land was not being developed. Land Use & Wildlife - allotments: there are large allotment complexes across the town with waiting lists minimal if at all. There were allotments removed a little further north (Whinney Banks). The current space already supports the health and recreational needs of the local population; it does not need alteration to fulfil this function. "Allotments into self use" This doesn't actually make sense. "Volunteer groups supported in management and development of green spaces, key to the potential of a flourishing country park." Who will take responsibility for managing the volunteer groups and how will this be costed into future council budgets? From where will the volunteers be drawn? (The third sector is already under increased pressure and restricted budgets as a result of Covid). Equity and local economy - "Sustainable location..." What does this mean? Health and Happiness - this development, particularly the proposed spine road, 'Country Park' will directly and negatively affect the health and happiness of existing residents. The area is extensively used by local people and provides a quiet and restful space to relax and unwind, particularly important at the moment, and critical to people's mental and physical health.</p> <p>Engagements and events will increase traffic flow which, in turn, increases air, water and noise pollution.</p> <p>1.6 "Enhance local identity and placemaking." What does this actually mean? The language is unclear.</p>
Charlotte McAdam				<p>I am happy that the houses have been removed from Mandale Meadow in the new masterplan, however, I do not agree with the exit point for the access road onto Mandale roundabout, as previously explained.</p>

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Chris King				<p>The character of the residential area will be homes within a park and therefore the impact of roads will be reduced to change driver behaviour, lower speeds and shared spaces that promote sustainable transport and active travel.'</p> <p>It is clear that no matter how clever the design, some drivers refuse to follow the 30 mile an hour limit on current housing estate roads as we have observed on Heythrop (20 MPH limit) even when the road is filled with parked vehicles or indeed children walking home from local schools.</p> <p>'Parking is intended to be unobtrusive rather than dominating with ample spaces for cars, but avoiding bland parking courts, and visitor parking integrated with urban landscaping.'</p> <p>In those areas that are hidden from view, as well as in more obvious parking places such as the entrance of Acklam Grange Secondary School, we discover on certain mornings the little silver capsules of drug users. Unobtrusive parking may be aesthetically pleasing but will offer an ideal place for those wishing to use such areas for late night takeaway picnics and consequent littering or other more nefarious activities.</p> <p>Summary Point 'enhanced connectivity to and from the wider area via a network of paths.'</p> <p>I think we would all celebrate this notion but unfortunately, no matter how many cycle ways and foot paths are included, people will use cars whether entering or leaving the development. Any further cars accessing an already over-burdened road system on Mandale road will simply cause more chaos, frustration and pollution.</p> <p>'Is accessed from both the B1380 and A1130 creating a link road through the development. Provides any necessary off-site improvements to transport infrastructure to ensure that traffic generated by the development does not have a detrimental impact on the highway network.'</p> <p>Same comment as above.</p> <p>' A noise assessment will be required to take account of traffic noise from the A19 and A174. '</p> <p>Removing any woodland presently shielding the A19 will cause a significant increase in road noise. From our location on Heythrop Rd, the noise from the A19 is often very evident, particularly in the winter months when many deciduous trees have lost their leaves.</p>
Christine King				<p>We can't control what happens on privately owned land but we want NO ROAD THROUGH MANDALE MEADOW/WOODS.</p>

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christopher Eddon				<p>ive cut and paste this section from the 2014 housing plan which explains it all really and that you only want the road built to ease congestion elsewhere ie A174/ A19 roundabout.</p> <p>A19 CORRIDOR</p> <p>2.77 The A19 is a major corridor that provides an important communication link with areas to the north and south of the Tees Valley. Whilst it is a strategic route that has an important role to play in accessing the major development initiatives within the Tees Valley, it also acts as a local route. This dual role impacts upon the ability of the road to function in its strategic capacity. It is important therefore to examine ways in which local traffic can be removed from the A19, increasing capacity and reducing congestion. Thus, whilst improvements will be sought to the road and its junctions, a package of complementary measures should also be introduced into the surrounding road and public transport network to support the role and function of the A19. For example, it is considered that a link road from Low Lane to Mandale Road will relieve pressure on the A19/A174 junction.</p>
Claire Boyd				We can't control what happens on privately owned land but there should be NO ROAD THROUGH MANDALE MEADOW/WOODS.
Claire Di Cicco				No to a road.
CLAIRE KANE				<p>The plan claims that you are NOT building any more houses on that land, Then states this</p> <p>to provide strategic mitigation to bring forward the Council's wider housing development programme.</p> <p>It is all a misleading way of wording to say once passed that you WILL NOW build houses</p>
Claire Wilks				The houses are no longer planned so therefore the country park does not need to be developed. There is now no need to adapt the natural environment to create a blended setting with the houses that were to be built.

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Colin and Morag Bailey				<p>The masterplan still shows a spine road running through Mandale meadow which residents protested strongly against in 2018 because of the devastating impact a spine road would have on the varied habitats on Mandale meadow and on the red and orange listed endangered species which live in very specific habitats on the meadow. The council have failed to take into account how much life has changed since 2014 because of the covid 19 pandemic. Many people will not return to working from business premises which will reduce the volume of rush hour traffic when lockdown ends making the Spine Road an unnecessary piece of destruction of habitats on a richly biodiverse meadow.</p> <p>Building a road through Mandale meadow and destroying habitats runs counter to the 'Climate Change' and the 'One Planet Living' principles which the council has adopted</p> <p>The vision for the 'country park' at Stainsby includes far more houses than Middlesbrough is required to build by the government and more than is required as Middlesbrough's population has remained relatively stable over the decades. ONS population figures indicate there will be a reduction of approximately 1000 people over the next ten years. This population reduction could become even greater if people start to move outwards due to dissatisfaction over effectively losing Mandale meadow and the open agricultural views currently enjoyed. The Bluebell Beck valley was so heavily utilised over the lockdowns that the grass was actually worn out, proving how vital this semi wild natural area is to residents mental and physical health.</p> <p>Data shows Middlesbrough has an aging population and bungalows will be needed more than 3,4 or 5 bedroomed houses. The new estate will not be a balanced community as only 5% of the housing is allocated for affordable housing which, as a deprived industrial town, is one of Middlesbrough's greatest needs along with bungalows for the elderly. The landscaping strategies in the current masterplan could be used very successfully in the development of brownfield sites, providing people with quality houses in natural green spaces to enjoy and as these brown field sites will be closer to the town it will make them more attractive to both the young and the elderly.</p> <p>*p6, 1.1 the proposed development at Stainsby will not create a balanced community as 95% of the houses will be unaffordable nor is it clear if any will be bungalows for the ageing population.</p> <p>*p6, 'Middlesbrough Council, Housing Developers, Ward members and local people alike have contributed to the future vision for the site' local people have consistently said 'No Road Through Mandale Meadow'.</p> <p>*p7, 1.2 Desk top assessments are not reliable indicators of the wildlife or specific habitats on Mandale meadow, if they were they would have highlighted rare and endangered flora and fauna.</p> <p>*p8, the Bluebell Beck valley should remain as it is, you cannot 'enhance' natural beauty which has taken decades, if not centuries, to mature the way it has.</p> <p>*p8, 1.3.1 'standard' builds are not what is required in 2021 going forwards. All new builds should be designed to be green and energy efficient. This is something which was pointed out in 2018. In 2025 gas heating will be banned in new homes so all new builds at the proposed country park should have either air source heat pumps or ground source heat pumps installed or other green energy which is available. A speed limit of only 30mph means drivers will be going much faster and that is untenable in a 'country park'.</p> <p>*p8, 1.3.2, the 'jewel in the crown' is the existing open public space on the Bluebell Beck valley which should remain in pristine condition.</p> <p>*p9, 1.3.2, The vision of a 'country park' does not enhance the existing open public space, it actively destroys it by the encroachment of houses on what is currently much valued and appreciated open agricultural land seen from Mandale meadow. The construction of a road will destroy a variety of existing habitats containing red and orange listed endangered species and goes against the Council's commitments to 'Climate Emergency' and 'One Planet Living'. We need to start protecting nature before it is too late. There is very good documented evidence of newts, water voles, rare birds, butterflies and moths on Mandale, all of which are under serious threat if this unnecessary spine road goes ahead, they don't and can't "just move on" as some of the habitats provide the specific needs of certain flora and fauna. Desktop software such as MAGIC has clearly failed to assess the wildlife on site. There is next to no chance of the Council facing legal challenges if it drops the spine road from the LP as it has enough evidence to prove a change of direction is justified.</p> <p>*p9, 1.4 'Enhances the Local Wildlife Site in the north of the site and provides compensatory provision for any loss of habitat required for highway access' this is nonsense, building a spine road on Mandale meadow will destroy local wildlife sites, you can't compensate for wiping out Red listed endangered species. There is a name for that - ecocide.</p> <p>All the described benefits of a 'country park' can be achieved without damaging or building on Mandale meadow or the existing open public space on the Bluebell Beck Valley</p> <p>*p10,1.4, The 'country park' does not relate well to the surrounding geography and history of the land if a road goes through Mandale or if excessive</p>

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				house building ruins the open agricultural aspect currently enjoyed by residents. The habitats on Mandale which would be destroyed by a road cannot be replicated in the Stainsby development as they take decades to develop and there will be chemical residues on the farm land which will not be conducive to quickly forming new habitats. Mandale meadow has not been subjected to pesticides and other agricultural chemicals which increases it's value in terms of a place where nature can flourish. Mandale meadow is far from the 'mown field' it has been described as by the Council, it rather proves that site visits with knowledgeable locals as guides is more beneficial to understanding a location than maps which provide aerial views *p11, 1.4 The Ten One Living Planet principles. The spine road and unnecessary 'development' of the existing open public space on Bluebell Beck valley immediately goes against some of the principles adopted by MBC. Land use and wildlife, building a road on Mandale will wipe out endangered wildlife which have specific habitat requirements which are not replicated elsewhere on the Bluebell Beck Valley and some cannot be replicated by man because of complex soil fungi which nurtures specific plants in specific habitats which produce the correct environment for endangered wildlife. Zero carbon, MBC will significantly raise carbon emissions by releasing stored carbon from felling trees and churning up meadow land. Building a road through Mandale will raise carbon emissions in the Ayreome ward. Zero Waste, using excavated spoil to form mounds on rich meadow land is more ecocide. Do it on the proposed new estate/country park by all means but leave the Bluebell beck valley and Mandale alone. Sustainable water, SuDs on Mandale have not been successful so careful consideration needs to go into developing new SuDs, a large lake, similar to Hemlington lake, on the new estate/old farmland might be a way forward to prevent flooding issues towards Stainsby North. Culture and Community, damaging Mandale meadow will not create wellbeing for Ayresome ward residents, or the many people from other wards/areas who either live adjacent to the meadow or travel in from other wards, it will actually cause the opposite with more air and noise pollution close to homes which cannot be mitigated by planting 30cm whips. Indeed, the pursuit of greater council tax harvesting will make residents lives miserable and prevent some residents from accessing the meadow due to traffic pollution. Health and happiness, as above, this scheme to damage Mandale will make residents extremely unhappy and have a negative impact on both physical and mental health. Equity and local economy, there is no equity in taking away a much loved and used meadow from Ayresome ward residents in order to give more land to new estate home owners. The impact of building only has a short term benefit for the local economy
David John Parker	Stagecoach North East			Stagecoach wishes to extend its broad support towards the plan, which we recognise should support the Council's wider aspirations to meet projected demand for quality housing in the borough over the coming years.
Dennis Mccabe				I feel the plan is well worked out
Douglas Edge				Not much of a country park if its organised to the nth degree. Even more traffic on Mandale roundabout at peak times seems like a recipe for traffic jams back to Acklam road. Finally a short synopsis in normal English would help people make informed decisions.
Dr Enoch Akowuah				Its important that there is a diversity of housing styles within the development. The current mono tonal developments on the site as well as others on Strait lane etc are a poor way of enhancing the feel, look and overall reputation of the town Does the housing density have to be quite so high ? Middlesbrough lacks areas like high value premium locations like Wynyard and Leven Road in Yarm. Is this an opportunity to creat such an area ?
Dylan Currey				The updated plan contains no annotation/labeling of any kind, leaving it open to interpretation.
Edna Reddy				The emphasis is on high standards for the "largest single housing allocation in the town". I am all for high standards of housing and environment but it seems these are not the same standards accepted for the current residents who will be adversely affected by the spine road emergence across the Mandale meadow and into the Mandale roundabout. Do we not deserve the same high standards of environment as those buying into the new estate?
Emily quinnt				No road necessary. Do you want people and animals to get run over?
Francine Kenny				Yes to Country Park Yes to houses yes to road

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Geoffrey Payne				All maps seem to reflect that the road access will open out to the mini roundabout on the A1130. Option 1 on the 2085-34 ▪ Stainsby Link Road ▪ Initial Option Assessment Report 25 January 2021 ▪ Version 1.1 ▪ Issue seems the best route to prevent adding to the congestion already faced on Levick Crescent, Mandale Road and the A19. Levick Crescent and Mandale Road are also school routes for pedestrians with no controlled road crossings.
Gill Bradley				There isn't any information on the map sent to the public. It's outrageous that yet again the council are trying to appropriate public Greenland for a thoroughfare. I and my neighbours haven't received any such information and it's a terrible sly way of trying to steal away the only local greenlands from us. We already live with the Parkway dual carriageway running adjacent to this area. There would be a massive detrimental effect to local wildlife, already in decline due to tarmaced driveways and hedges being torn out. Must we be on a destruction to all our native wildlife this century? Its appalling.
Gill Mollard				This housing allocation is not necessary and should stop. Any 'country park elements' should remain within the site already under construction and NOT damage the area on Mandale Meadow, Newfield Meadow or the connecting green spaces by the addition of landscaping features, footpaths, cycle paths, play equipment or 'desire lines'.
Gillian Robins				This would be the preferred option.
Glen Instone				Is there really a shortage of high quality residential properties in the area? I don't believe so. Pg6: Part of the red outlined proposal is incorrectly included in the plan ie the existing paddock is not a proposed part of this so why do you include pictures of this and show as a large expanse of green belt? You are clearly misleading people with this. What are you suggesting with the septic tank that serves the Stainsby Grange residents? This is within your Country Park! Pg12: You are proposing a strategic pedestrian/cycle route all the way to the side of my property! I am pretty sure this will have several effects: 1) Reduced privacy 2) Reduced property value.
Graeme Brooks				My query is regarding who will be responsible for the upkeep of all the is new green area? Will it be the council who will cut the grass, tend to the flowers and cuts the bushes and be paid for through council tax? Or will it be through a service charge that the new house owners will have to pay as well as their council tax? If it is through a service charge will this be explained to the residents that their service charge could increase exponentially as there are currently no limits as to what they can charge?

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Graham Hadfield				<p>Page 9 states that a requirement is that the housing development “Is accessed from both the B1380 and A1130 creating a link road through the development.”.</p> <p>This is another indication that , far from the position being “there’s no plan to put a road in”, a road through Mandale Meadow is a REQUIREMENT of any forthcoming plan.</p> <p>Page 9 states that a requirement is that the housing development “Provides any necessary off-site improvements to transport infrastructure to ensure that traffic generated by the development does not have a detrimental impact on the highway network.”. A EIR response sent out on 21 February 2019 admitted that the traffic data on which the plan relied was flawed. In addition it was shown that diverting thousands of commuter journeys per day to access the A19 at Mandale junction rather than at the A174 junction would only shift the problem of congestion (of which the major cause is the A19/A66 pinch point) further north.</p> <p>Page 9 states that it is a requirement of the housing development that “Where crossings over the becks are required this shall be by clear span crossings rather than culverting.” The revised route of the road as shown on Page 13 follows the route of Bluebell Beck at its northern end, before turning east over the meadow. That can not be achieved without culverting an extensive section of Bluebell Beck.</p> <p>Page 9 states that it is a requirement that the housing development “Enhances the Local Wildlife Site in the north of the site and provides compensatory provision for any loss of habitat required for highway access.”. The revised route of the road as shown on Page 13 would necessarily mean the destruction of thousands of trees and shrubs (many of them decades old) and the destruction of land where rare orchids and food plants for rare butterflies and moths grow.</p> <p>On page 11 there is a statement “Zero Carbon- The design although on the edge of the Middlesbrough Conurbation will ensure adequate reduction in car usage”. Far from reducing car usage in the area, the mere fact that some 1670 dwellings are envisaged will mean that around 3350 cars, rising to over 5000 as families mature, will be imported to the area. On top of that, if the road runs through Mandale Meadow it will provide a rat run for the use of thousands of commuter vehicle journeys a day. Those factors will hardly “ensure adequate reduction in car usage”.</p> <p>Page 11 suggests the use of SUDS to minimise flooding issues on site. Existing SUDS fail to provide the drainage necessary to prevent significant pooling occurring on the gravel path between the beck and A19 from Acklam Meadow to Mandale Road? That is not a problem given the current nature of the path but it would be with a road of the type proposed. It is unbelievable that SUDS are suggested as an appropriate solution to minimise flooding issues when real life experience demonstrates that they are not.</p> <p>Page 11 suggests that the design will provide further Bluebell Beck Improvements in water quality and natural habitats and protection of the Beck from Impacts. The proposed route of the road would involve culverting the northern end of the beck and destroying mature woodland and areas of land which are home to rare orchids and food plants for rare butterflies. That will destroy, not improve, natural habitats and destroy, not protect, the beck from impacts.</p> <p>The final four paragraphs on page 11 discuss the benefits of a country park to the newly developed housing estate. They completely ignore the fact that, far from bringing benefit, the proposed road route through the publicly owned land to the north of the development would bring destruction, not benefit.</p> <p>Page 12 shows “New tree planting” next to the A9 Mandale junction. That area already contains mature trees & shrubs and land where rare orchids and food plants for rare butterflies and moths grow.</p> <p>Page 12 explicitly states “Bluebell Beck in culvert where Stainton Way crosses. This directly contradicts the commitment made on page 9 that “Where crossings over the becks are required this shall be by clear span crossings rather than culverting.”</p> <p>Page 12 shows Stainton Way as a “Primary Route”. The policy of Middlesbrough Council for the last several years has been that the speed limit on residential roads (including estate access roads) is 20 mph. What is the justification for breaching that policy by building a 30 mph link road, attracting thousands of commuter journeys per day, through a country park where children are playing and wildlife will live? What analysis has been made of the risks of causing road accidents by doing so?</p>

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Graham Hadfield				<p>This is an addendum to my comments submitted on 5 February, 8 February and 22 February. Please ensure that each of the contradictory/inconsistent points in the "Masterplan" which are listed below are fully resolved in the final consultation report.</p> <p>Commitment: "Where crossings over the becks are required this shall be by clear span crossings rather than culverting." Reality: The revised route of the road can not be achieved without culverting an extensive section of Bluebell Beck. Page 12 explicitly states "Bluebell Beck in culvert where Stainton Way crosses.</p> <p>Commitment: "Enhances the Local Wildlife Site in the north of the site and provides compensatory provision for any loss of habitat required for highway access." Reality: The revised route of the road would necessarily mean the destruction of thousands of trees and shrubs (many of them decades old) and the destruction of land where rare orchids and food plants for rare butterflies and moths grow.</p> <p>Commitment "Zero Carbon- The design although on the edge of the Middlesbrough Conurbation will ensure adequate reduction in car usage". Reality: The number of vehicles connected to 1670 dwellings plus the creation of a rat run for the use of thousands of commuter vehicle journeys a day will hardly "ensure adequate reduction in car usage".</p> <p>Commitment: The design will provide further Bluebell Beck Improvements in water quality and natural habitats and protection of the Beck from Impacts. Reality: See comments re culverting and destruction of trees and land above. Commitment: "New tree planting" next to the A19 Mandale junction. Reality: See comments re culverting and destruction of trees and land above.</p>
Helen McGregor				<p>Stakeholders will of course be interested in this site due to the possibility of financial gain. However, Middlesbrough will only be a vibrant place to live in if there are opportunities for people to de-stress and unwind, and I consider that the meadow and woods in its present state is the best way as there is a lot more wild life which has been proved to enable well-being. A manicured and manufactured park will not provide the same result.</p>
Hilary Morley		Hilary Morley	Resident	<p>The area you are planning on destroying is used by many local residents. We do not need a country Park it is fine the way it is and we definitely do not need a polluting road building</p>
Ian Etherington				<p>I agree with the need for more and better quality housing. Also there should be an emphasis on active travel whilst discouraging car use.</p>
Ian Marron				<p>Under section 1.4 Housing local plan policy H21 section e, please amend to read suitable access roads to B1380 in two places.</p>
Ian Rogers				<p>1.3.2 The country park appears, in the main to be on land that cannot be built on, due to the major pipes running beneath the site. I am concerned about 'enhancements to Blue Bell Beck, the water vole population dropped following the use of the beck valley, by construction traffic to access Acklam Grange when it was rebuilt. To think that more construction is planned close to the beck, I fear will possibly wipe out the remaining population of an endangered species, that we should be protecting. The 'country park' appears to be a way of placating residents, for the destruction of greenspace that they already enjoy, with a sterile space on land that is unsuitable for development! 1.4 In view of the continued threat of global warming and our destruction of mature habitats, them 2014 plan must be revisited. Middlesbrough is a densely populated area, that should be cherishing its mature meadows, not building on them! Well used greenspaces are parks, which are pretty sterile places and no replacement for a mature habitat, the protected species that this development will destroy cannot be replaced.</p>
Jacqueline Jones				<p>Very pleased with the overall proposal and the additional open space and landscaping</p>

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jamie barber				frankly, none of this should go ahead. you're planning to destroy natural green space and animal habitats that are at a premium in Middlesbrough.
Janet Johnson				I like the plan - just not the need for the spine road to destroy what is already a safe haven for many in order to complete it
janice slater				In my original submission i requested that the SWWE remain a loop road:entry and exit at Jack Simon Way . the following comments are an addendum to my original.
Jean Franklin				Page 11 One planet living, zero carbon. 'The design will ensure adequate reduction in car usage' The proposed link road from Jack Simon Way to Mandale Road the Stainton Way western extension contradicts this as it will carry huge amounts of through traffic, causing increased noise and pollution. Estimated traffic levels I understand are up to 12000 vehicles per day.
Jennifer A Alderson				Mentioned in the introduction section is grassland and woodlands. Does this relate to existing grasslands and wooded areas or new areas? If this is in relation to existing areas, is being done to protect existing grasslands and woodland and the species of wildlife that inhabit the areas already? If it is a new plan what guarantee can MBC give to protect the species that are inhabiting Mandale Meadow. In relation to the new amenities such as the primary school, where is this being sited on the plan? And will those that live in existing estates of Acklam be part of the catchment area. As someone who plans to have a young family in Middlesbrough it would be nice to know if this Primary school would be available for all in the area or those just solely living on the housing estate.
Jenny McCullagh				1.3.2 - How can this be a country park, with a highway through the middle of it? It is not creating setting and character. It is destroying the character already there.

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Jill Hadfield				<p>Page 9 states that a requirement is that the housing development “Is accessed from both the B1380 and A1130 creating a link road through the development.”.</p> <p>This is another indication that , far from the position being “there’s no plan to put a road in”, a road through Mandale Meadow is a REQUIREMENT of any forthcoming plan.</p> <p>Page 9 states that a requirement is that the housing development “Provides any necessary off-site improvements to transport infrastructure to ensure that traffic generated by the development does not have a detrimental impact on the highway network.”. A EIR response sent out on 21 February 2019 admitted that the traffic data on which the plan relied was flawed. In addition it was shown that diverting thousands of commuter journeys per day to access the A19 at Mandale junction rather than at the A174 junction would only shift the problem of congestion (of which the major cause is the A19/A66 pinch point) further north.</p> <p>Page 9 states that it is a requirement of the housing development that “Where crossings over the becks are required this shall be by clear span crossings rather than culverting.” The revised route of the road as shown on Page 13 follows the route of Bluebell Beck at its northern end, before turning east over the meadow. That can not be achieved without culverting an extensive section of Bluebell Beck.</p> <p>Page 9 states that it is a requirement that the housing development “Enhances the Local Wildlife Site in the north of the site and provides compensatory provision for any loss of habitat required for highway access.”. The revised route of the road as shown on Page 13 would necessarily mean the destruction of thousands of trees and shrubs (many of them decades old) and the destruction of land where rare orchids and food plants for rare butterflies and moths grow.</p> <p>On page 11 there is a statement “Zero Carbon- The design although on the edge of the Middlesbrough Conurbation will ensure adequate reduction in car usage”. Far from reducing car usage in the area, the mere fact that some 1670 dwellings are envisaged will mean that around 3350 cars, rising to over 5000 as families mature, will be imported to the area. On top of that, if the road runs through Mandale Meadow it will provide a rat run for the use of thousands of commuter vehicle journeys a day. Those factors will hardly “ensure adequate reduction in car usage”.</p> <p>Page 11 suggests the use of SUDS to minimise flooding issues on site. Existing SUDS fail to provide the drainage necessary to prevent significant pooling occurring on the gravel path between the beck and A19 from Acklam Meadow to Mandale Road? That is not a problem given the current nature of the path but it would be with a road of the type proposed. It is unbelievable that SUDS are suggested as an appropriate solution to minimise flooding issues when real life experience demonstrates that they are not.</p> <p>Page 11 suggests that the design will provide further Bluebell Beck Improvements in water quality and natural habitats and protection of the Beck from Impacts. The proposed route of the road would involve culverting the northern end of the beck and destroying mature woodland and areas of land which are home to rare orchids and food plants for rare butterflies. That will destroy, not improve, natural habitats and destroy, not protect, the beck from impacts.</p> <p>The final four paragraphs on page 11 discuss the benefits of a country park to the newly developed housing estate. They completely ignore the fact that, far from bringing benefit, the proposed road route through the publicly owned land to the north of the development would bring destruction, not benefit.</p> <p>Page 12 shows “New tree planting” next to the A9 Mandale junction. That area already contains mature trees & shrubs and land where rare orchids and food plants for rare butterflies and moths grow.</p> <p>Page 12 explicitly states “Bluebell Beck in culvert where Stainton Way crosses. This directly contradicts the commitment made on page 9 that “Where crossings over the becks are required this shall be by clear span crossings rather than culverting.”</p> <p>Page 12 shows Stainton Way as a “Primary Route”. The policy of Middlesbrough Council for the last several years has been that the speed limit on residential roads (including estate access roads) is 20 mph. What is the justification for breaching that policy by building a 30 mph link road, attracting thousands of commuter journeys per day, through a country park where children are playing and wildlife will live? What analysis has been made of the risks of causing road accidents by doing so?</p>

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Joanne Mitchell				<p>The A19 has already been widened at some cost in terms of physical and mental health and the general well being of residents. Issues exist which require resolution regarding noise and air pollution and ought not to be made worse still by this imposition.</p> <p>The road would destroy the meadow.</p> <p>These sad plans they have to ruin one of the most beautiful parts of Middlesbrough not to mention the habitat of wildlife that will disappear without the meadow. Greenspaces are very precious and need to be kept as they are. Years of conservation and preservation work has been invested. People moved close to the meadow being given to understand it is protected, a greenbelt where no building or any development may ever take place. It may never have been a designated greenbelt but it was the subject of a Deed of Covenant, the land having been gifted to the people of Middlesbrough in perpetuity. A road is being planned through the meadow to service a housing estate.</p> <p>At the end of the day destroying a pristine meadow which is rich in biodiversity and has never been 'treated' with pesticides and herbicides, is nothing short of ecocide and vandalism and makes a laughing stock of the council's One Planet Living and green strategies.</p> <p>There was always another option, the 'developers' could create an estate loop road similar to the one at Trimdon.</p> <p>The council has stated they won't even pay local residents a penny in compensation for taking our public amenity from us and subjecting us to increased air and noise pollution, damaging our health.</p> <p>the disruption, destruction, the irreparable damage, and the blatant disregard for people who have lived here and who have been involved during many years in the care and preservation, and who will have their lives destroyed, just beggars belief. Residents already suffer the constant excessive traffic noise...and the pollution inflicted upon them. To think that there are those with the power who lack the empathy and the understanding, or thought and consideration for people who use the meadow and those who live close by is unbelievable.</p> <p>The residents in the Ayresome ward would no longer be able to access Mandale meadow from Mandale Road. It will be impossible for people in the Ayresome ward, including those of us who live a 2 minute walk from the current entrance to Mandale meadow is because it would involve walking straight across two lanes of the proposed roundabout. The road on OUR meadow stopping us accessing OUR meadow is the issue. All this nonsense about "you are getting extra greenspace" is an insult when you are planning something which prevents us from accessing OUR green space.</p> <p>The remarkable thing about this is some of the language used in the document to describe the proposed plan: "To create a diverse ecosystem" etc when one already exists that will be destroyed by the building of so many houses. The plan to create this "diverse ecosystem" on land which has been polluted by herbicides, pesticides and pollution from the building process while destroying a pristine meadow which is genuinely a diverse ecosystem containing Red and Orange endangered species marked on this map of Mandale meadow. They know this because the map and the legend were included in the Greenstuff response to the 2018 LP.</p> <p>The area is home to rare species flora and fauna, which have taken decades to fully establish, all of this would be destroyed.</p> <p>The faux manufactured country park, cannot, and will never, as if by magic, replace this.</p> <p>Mandale Meadow is well used and loved by many, an accessible place in which we are able to walk and relax.</p> <p>Residents currently suffer excessive noise and air pollution levels 24/7 owing to the proximity of the A19...how can it possibly be considered in any way acceptable to bring yet another road even closer to homes.</p> <p>When the A19 was widened residents received compensation for the loss in value of their homes..but despite the proposed road being routed even closer we are told we would not receive any such award for all of our losses. ..the loss of peace, and of mental and physical health. .. These of course can never be compensated for. It is not what we want.</p> <p>The notice delivered to each resident shows a basic outline of the area concerned, part of which is Mandale Meadow at the top of the 'plan'. The notice gives the impression that the entire area will be Country Park, along with which would come 1700 houses, not forgetting the so called Relief Road, to 'relieve' congestion on the A19 and serve the 1700 new homes. The plan actually shows the road through Mandale Meadow which would discharge traffic onto Mandale Road, at a junction adjacent to residents homes where the current car park is now situated, actually at the foot of their gardens.</p>
John Baines				<p>I fully support the idea of a country park but without a new spine road as it will destroy the much needed green space and cause excessive traffic congestion on Mandale road.</p>

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John David Duffy				<p>As the development of housing is solely going ahead on private land, I believe that the masterplan is an excellent idea. Their needs to be something over an above the local plan to hold developers to account and to ensure the Middlesbrough public are left with a landscape led development, rather than developers maximising its profits and leaving the people of the town with whats left over..</p> <p>I fully understand the purpose of the document and support it.</p> <p>Need to ensure the One planet principles are adhered to and not a token for this documents.</p>
Judith Harbron				No development
Judith Maunder	Friends of Mandale, Greenstuff, resident very local to Mandale Meadow			<p>P7, 1.3. The vision for the allocated housing site at Stainsby is a sustainable, mixed and balanced community set within an outstanding and engaging landscape. The above statement is untrue. There will not be a balanced community as only 5% of the development will comprise of affordable housing. The remaining 10% to be delivered off site. Nor is the 'country park' representative of the semi wild natural green space which exists within the existing open public space at Blue Bell Beck Valley. Furthermore, describing Mandale Meadow as a 'mown field' is deliberately misleading as it is well established that Mandale Meadow is richly biodiverse and contains habitats specific to the requirements of some Red listed endangered fauna not found elsewhere on the BBB valley. I find the description of 'country park' totally misleading. what you intend creating is a town park much like Albert Park, not a natural countryside retreat.</p> <p>P9, 1.3.2, The 'country park' does not enhance the existing open public space, it DESTROYS it by building on wet meadow land, causing flooding requiring further useless, life destroying SuDs and removing vital habitats such as existing woodland and richly diverse meadow which attracts a vast array of fauna. It is also likely that flooding will be caused in the gardens of residents of Heythrop and Farley whose gardens back onto the meadow.</p> <p>p9,1.4 'Is accessed from both the B1380 and A1130 creating a link road through the development.' There should be no road exiting onto the A1130 which already suffers from traffic congestion causing tailbacks on Mandale Road, Croft Avenue, Levick Crescent and all the local roads which feed into them. The traffic is stationary between the roundabout and Acklam Road causing massive amounts of toxic air pollution on Mandale Road. It cannot take in excess of 12,000 more vehicles at rush hour, it is seriously damaging to the health of residents.</p> <p>p10,1.4 'a) relate well to the surrounding geography and history of the land, and contribute to the wider environmental benefits for the town' This outrageous plan causes major environmental damage for the town as local residents will have their own natural open public space removed from them and covered in a toxin producing road, no ability to use their own covenanted green space if they are susceptible to toxic traffic pollution, it will cause asthma for a number of local residents. The noise pollution from the A19 is already considerable and will be increased exponentially by another road at the bottom of residents gardens. Causing them to suffer greatly from both noise and air pollution. Any plan which kills existing flora and fauna, including mammals such as water voles, newts, hedgehogs and deer is ecocide and not worthy of a Mayor who is greenwashing his policies while causing environmental destruction.</p> <p>p10, 1.4, 'd) respect and exploit already established important local and strategic views and vistas' This can't be achieved by destroying Mandale meadow with a road and a 14ft 9" wide cycle/pedestrian path and numerous other paths and dumping excavated spoil on Mandale meadow.</p> <p>p10.1.4, 'i) create a site with identity' the existing BBB valley already has 'identity'. Leave it alone.</p> <p>p10, 1.4 'a) creating or redefining a clear hierarchy of routes, streets and spaces'. fine in the new development but leave Mandale meadow alone.</p> <p>p10, 1.4 'h) incorporating memorable public art at landmark locations'. Fine at the new development but leave our natural space at BBB valley alone. a couple of benches on Mandale would be sufficient.</p> <p>p11.1.4 the principle of One Planet Living have been completely ignored when it comes to causing misery to the residents of Ayresome ward and committing ecocide on Mandale meadow. I think MBC have forgotten to read their own greenwashed propaganda.</p> <p>ZERO CARBON emissions!!!! how does bringing an extra 12,000 + vehicles onto a small residential roundabout create zero carbon? It massively</p>

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				<p>increases it. It has been demonstrated that less than 50% of vehicles will be EVs by 2040 which means that MBC stand to be heavily fined by the government for breaching carbon emissions in the Ayresome ward as the new target is 2030 NOT 2050. THINK again and make an estate loop road not a rat run through Mandale meadow.</p> <p>Also, all the construction will create carbon emissions and chopping down trees and destroying meadowland releases all the stored carbon they have absorbed. No road through Mandale.</p> <p>'Zero Waste- The creation of the country park will ensure that on site cut and fill could be reused'by dumping it as 'land forms' on Mandale, disgusting as it kills existing flora and fauna on a pristine meadow.</p> <p>'Sustainable Water- SuDs will be employed in a number of ways and as part of the wider development in order to create low run off rates minimising flooding issues on site.' SuDs are death to wild life. They overflow in wet months, wildlife breeds in them, then dry weather arrives and the wildlife dies - think of tadpoles and invertebrates. They are an ecological disaster.</p> <p>'Land use and wildlife'. is that meant to be a sick joke???? you intend to build on a richly biodiverse site yet dare to talk about wildlife?</p> <p>Another sick joke? 'Culture and Community- The creation of the country park should assist with creation of a community and also improve wellbeing with access to high quality open space. Stopping people from accessing Mandale because of the busy road, air pollution does the opposite of improving wellbeing. Your plan is causing me nightmares and depression because you intend to take away my only accessible greenspace. I have chronic asthma and bronchiectasis so I will be prevented from going to my safe space if you build a road on the meadow. A meadow which was covenanted to the people in perpetuity to improve their health and wellbeing will be destroyed by a road.</p> <p>Another sick joke running counter to the principles of One Planet Living:</p> <p>'Health and Happiness' will NOT be created by destroying a meadow with a road and creating excessive air and noise pollution and traffic congestion in fact, Middlesbrough has the highest rates of some diseases and it is known that toxic traffic pollution creates asthma, COPD, and heart disease which can lead to vascular dementia yet still you include this road in your 'masterplan' despite all the protests and objection letters sent to the council in 2018. p12, 1.5 when people objected to the 'building' on Mandale they were objecting to houses and the road but MBC have failed to listen and put the road back in the plan.</p>
Julia Povey				We can't control what happens on privately owned land but there should be NO ROAD THROUGH MANDALE MEADOW/WOODS.
Julie Riley				NO ROAD THROUGH MANDALE MEADOW
Kabaser Ajaib	Retailing	Kabaser Ajaib	Retailing	No through road
Kate Bowe				Please take the views of the local residents into account. They got this current council elected and will not easily forgive being lied to.
Keith Russell				No road through Mandale Meadow
Keith Young				No road through Mandale, you can do what you like on the private land

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Lauren Teate				<p>Page 6 - Introduction, Chapter 1.1, paragraph 1 - Foreword: We do not need more "high quality housing". We need social housing and you have stated that only 15% of these houses are even affordable. How is this helpful to Middlesbrough?</p> <p>Paragraph 2: We are NOT a city and nobody wants us to become a city.</p> <p>Paragraph 4: The "country park" is being created, not for people or wildlife but so that the houses are worth more and are more likely to be bought. This is unacceptable. A country park is not what the existing locals want or need and the people already living here are being completely ignored for the sake of more council tax. As always wildlife and the environment has come last in your "vision for the future".</p> <p>Chapter 1.4 page 11 - One Planet Living... Creating more housing, more roads and encouraging more cars on the road is going directly against that proposal. Surely instead of putting a road in, it would make more sense to only put in a cycle route. People will always choose to use a car over cycling, unless the cycle routes are quicker and easier than using a car. A direct cycle route would make it much more attractive than battling traffic and lights in cars and therefore more people will use it.</p> <p>"Sustainable Materials- Although largely controlled by other regulations it is likely that builders using standard house types will use a degree of sustainable materials." - Does this mean that you do not know for certain what materials are being used? If not, then shouldn't this be followed up? How are materials obtained? Are they imported? Are they created here in the U.K? These are questions that must be answered.</p> <p>Nothing is mentioned about how the housing will mitigate the impact on the wildlife in the area. Nothing is mentioned about making the gardens accessible to hedgehogs, for example, by creating a hedgehog highway. Neither are nest boxes or "bricks" mentioned for birds and bats to roost and nest on after all the hedgerows are pulled up and concreted over. It says that the green spaces are to be used for mental health and recreational purposes but once again nothing is mentioned of the existing wildlife here and how the green spaces will benefit the ecosystem or support the biodiversity that is already found along Bluebell Beck. How does this plan mitigate the influx of visitors? An increase in people and dogs using this areas will have a negative effect on the flora and fauna found here, some of which are extremely sensitive to disturbance.</p> <p>"Health and Happiness- Improve the natural and urban environment to create healthy and sustainable opportunities to improve economic and social opportunities for community and volunteer engagement. The design and creation of a country park will provide a solid foundation for community engagement and events." - What evidence is there that this housing estate will provide health and happiness? How can an extra 3000+ cars on the roads in this area possibly have any health benefits to the local population? A housing estate that is encircled by roads is not a healthy or happy place to live. This "Country Park" is not going to stop the noise, air and water pollution from these roads. A "Country Park" with a road running through it is not a happy or healthy place to spend time.</p>
Leah Davey				<p>This housing allocation is not necessary and should stop.</p> <p>Any 'country park elements' should remain within the site already under construction and NOT damage the area on Mandale Meadow, Newfield Meadow or the connecting green spaces by the addition of landscaping features, footpaths, cycle paths, play equipment or 'desire lines'.</p>

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Leigh Martin Sayers				I wanted to comment mainly about the road. But the introduction is so full of consultant-speak it insults the intelligence. Examples: "Opportunity to create a vision ... enhances the natural environment.."? "Creating a unique quality of place"? That is what Acklam residents are trying to tell you they have now and you are proposing to destroy. "M'bro to be continued to be moulded as a vibrant, modern and modern city"? - It is not a city ... "Competing against the NE"? Most Middlesbrough residents are proud to live in the North East but the only way we want to compete is at football. It also says that population growth has been achieved to the south of Middlesbrough - presumably that is because it is where you have built the houses! Middlesbrough is a tiny borough. QUESTION: How long can population growth be sustained in Middlesbrough before we run out of green space to build houses and roads? "Stabilising the population is essential if Middlesbrough is to consolidate its status as the city centre of the Tees Valley"? The Tees Valley isn't a city so it doesn't have a city centre. If we had an elected authority for the Tees Valley we could decide where the main centre would be. I have no idea how this proposal to build a few houses and roads on one of the few remaining green spaces in Middlesbrough can somehow enhance our status as a city.
Liam Harris				Please see sheet 1
Linda Barker				This housing allocation is not necessary and should stop. Any 'country park elements' should remain within the site already under construction and NOT damage the area on Mandale Meadow, Newfield Meadow or the connecting green spaces by the addition of landscaping features, footpaths, cycle paths, play equipment or 'desire lines'. This site allocation will cause SIGNIFICANT detrimental impacts to the existing wildlife site at this location.
Linda Jowers				No road through Mandale meadow, i dont see why the need for a country park, the trees and wild life are all well established, its a beautiful area without any alterations
Lynn Blagg				Leave site as it is to enable residents and others to use it in its natural state for exercise and pleasure and avoid destruction of wildlife habitats and ageing flora and fauna and avoid excess traffic in the area.

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M. E. Bailey				<p>The masterplan still shows a spine road running through Mandale meadow which residents protested strongly against in 2018 because of the devastating impact a spine road would have on the varied habitats on Mandale meadow and on the red and orange listed endangered species which live in very specific habitats on the meadow. The council have failed to take into account how much life has changed since 2014 because of the covid 19 pandemic. Many people will not return to working from business premises which will reduce the volume of rush hour traffic when lockdown ends making the Spine Road an unnecessary piece of destruction of habitats on a richly biodiverse meadow.</p> <p>Building a road through Mandale meadow and destroying habitats runs counter to the 'Climate Change' and the 'One Planet Living' principles which the council has adopted</p> <p>The vision for the 'country park' at Stainsby includes far more houses than Middlesbrough is required to build by the government and more than is required as Middlesbrough's population has remained relatively stable over the decades. ONS population figures indicate there will be a reduction of approximately 1000 people over the next ten years. This population reduction could become even greater if people start to move outwards due to dissatisfaction over effectively losing Mandale meadow and the open agricultural views currently enjoyed. The Bluebell Beck valley was so heavily utilised over the lockdowns that the grass was actually worn out, proving how vital this semi wild natural area is to residents mental and physical health.</p> <p>Data shows Middlesbrough has an aging population and bungalows will be needed more than 3,4 or 5 bedroomed houses. The new estate will not be a balanced community as only 5% of the housing is allocated for affordable housing which, as a deprived industrial town, is one of Middlesbrough's greatest needs along with bungalows for the elderly. The landscaping strategies in the current masterplan could be used very successfully in the development of brownfield sites, providing people with quality houses in natural green spaces to enjoy and as these brown field sites will be closer to the town it will make them more attractive to both the young and the elderly.</p> <p>*p6, 1.1 the proposed development at Stainsby will not create a balanced community as 95% of the houses will be unaffordable nor is it clear if any will be bungalows for the ageing population.</p> <p>*p6, 'Middlesbrough Council, Housing Developers, Ward members and local people alike have contributed to the future vision for the site' local people have consistently said 'No Road Through Mandale Meadow'.</p> <p>*p7, 1.2 Desk top assessments are not reliable indicators of the wildlife or specific habitats on Mandale meadow, if they were they would have highlighted rare and endangered flora and fauna.</p> <p>*p8, the Bluebell Beck valley should remain as it is, you cannot 'enhance' natural beauty which has taken decades, if not centuries, to mature the way it has.</p> <p>*p8, 1.3.1 'standard' builds are not what is required in 2021 going forwards. All new builds should be designed to be green and energy efficient. This is something which was pointed out in 2018. In 2025 gas heating will be banned in new homes so all new builds at the proposed country park should have either air source heat pumps or ground source heat pumps installed or other green energy which is available. A speed limit of only 30mph means drivers will be going much faster and that is untenable in a 'country park'.</p> <p>*p8, 1.3.2, the 'jewel in the crown' is the existing open public space on the Bluebell Beck valley which should remain in pristine condition.</p> <p>*p9, 1.3.2, The vision of a 'country park' does not enhance the existing open public space, it actively destroys it by the encroachment of houses on what is currently much valued and appreciated open agricultural land seen from Mandale meadow. The construction of a road will destroy a variety of existing habitats containing red and orange listed endangered species and goes against the Council's commitments to 'Climate Emergency' and 'One Planet Living'. We need to start protecting nature before it is too late. There is very good documented evidence of newts, water voles, rare birds, butterflies and moths on Mandale, all of which are under serious threat if this unnecessary spine road goes ahead, they don't and can't "just move on" as some of the habitats provide the specific needs of certain flora and fauna. Desktop software such as MAGIC has clearly failed to assess the wildlife on site. There is next to no chance of the Council facing legal challenges if it drops the spine road from the LP as it has enough evidence to prove a change of direction is justified.</p> <p>*p9, 1.4 'Enhances the Local Wildlife Site in the north of the site and provides compensatory provision for any loss of habitat required for highway access' this is nonsense, building a spine road on Mandale meadow will destroy local wildlife sites, you can't compensate for wiping out Red listed endangered species. There is a name for that - ecocide.</p> <p>All the described benefits of a 'country park' can be achieved without damaging or building on Mandale meadow or the existing open public space on the Bluebell Beck Valley</p> <p>*p10,1.4, The 'country park' does not relate well to the surrounding geography and history of the land if a road goes through Mandale or if excessive</p>

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				<p>house building ruins the open agricultural aspect currently enjoyed by residents. The habitats on Mandale which would be destroyed by a road cannot be replicated in the Stainsby development as they take decades to develop and there will be chemical residues on the farm land which will not be conducive to quickly forming new habitats. Mandale meadow has not been subjected to pesticides and other agricultural chemicals which increases it's value in terms of a place where nature can flourish. Mandale meadow is far from the 'mown field' it has been described as by the Council, it rather proves that site visits with knowledgeable locals as guides is more beneficial to understanding a location than maps which provide aerial views</p> <p>*p11, 1.4 The Ten One Living Planet principles. The spine road and unnecessary 'development' of the existing open public space on Bluebell Beck valley immediately goes against some of the principles adopted by MBC. Land use and wildlife, building a road on Mandale will wipe out endangered wildlife which have specific habitat requirements which are not replicated elsewhere on the Bluebell Beck Valley and some cannot be replicated by man because of complex soil fungi which nurtures specific plants in specific habitats which produce the correct environment for endangered wildlife. Zero carbon, MBC will significantly raise carbon emissions by releasing stored carbon from felling trees and churning up meadow land. Building a road through Mandale will raise carbon emissions in the Ayreome ward. Zero Waste, using excavated spoil to form mounds on rich meadow land is more ecocide. Do it on the proposed new estate/country park by all means but leave the Bluebell beck valley and Mandale alone. Sustainable water, SuDs on Mandale have not been successful so careful consideration needs to go into developing new SuDs, a large lake, similar to Hemlington lake, on the new estate/old farmland might be a way forward to prevent flooding issues towards Stainsby North. Culture and Community, damaging Mandale meadow will not create wellbeing for Ayresome ward residents, or the many people from other wards/areas who either live adjacent to the meadow or travel in from other wards, it will actually cause the opposite with more air and noise pollution close to homes which cannot be mitigated by planting 30cm whips. Indeed, the pursuit of greater council tax harvesting will make residents lives miserable and prevent some residents from accessing the meadow due to traffic pollution. Health and happiness, as above, this scheme to damage Mandale will make residents extremely unhappy and have a negative impact on both physical and mental health. Equity and local economy, there is no equity in taking away a much loved and used meadow from Ayresome ward residents in order to give more land to new estate home owners. The impact of building only has a short term benefit for the local economy.</p>
Malcolm Chilvers				<p>Like many people I have neither the time nor inclination to be able to read and absorb the information contained in all these chapters unlike yourselves who are being paid to do it, however the second paragraph contains a glaring error - Middlesbrough is not a City. It does not matter how many times you call it a city, it has not been granted City status.</p>

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Margaret Jackson				<p>Page 9 " Provides any necessary off-site improvements to transport infrastructure to ensure that traffic generated by the development does not have a detrimental impact on the highway network." This solution to access for the new housing development will certainly have a detrimental impact on A1130 which is already busy and can become congested at peak times.</p> <p>"Enhances the Local Wildlife Site in the north of the site and provides compensatory provision for any loss of habitat required for highway access." Unfortunately, though this may be the aim, the "Local Wildlife Site" will not be enhanced by any man-made development, as anyone visiting the natural beauty of the meadow and woodland will see, and nothing can compensate for "any loss of habitat" which once destroyed will be gone forever.</p> <p>Page 11 "Zero Carbon- The design although on the edge of the Middlesbrough Conurbation will ensure adequate reduction in car usage..." It is difficult to imagine how building more houses on the edge of town can result in a reduction in car usage. If this were the case there would definitely be no need to expand the road system. Although, " A key consideration will be a cycling commuter route throughout the development to allow quicker access to the town centre.", there is already resistance to having defined cycle lanes down Linthorpe Road.</p> <p>Page 13 "To do this a series of Site analysis and desktop studies have been undertaken to inform a naturalised approach to the landscape driven elements. These studies and parameters then inform a framework to define a series of Urban principles in response creating hierarchies and features to define and enhance local identity and placemaking. "</p> <p>For whom is this written? It is difficult to understand what is being explained in this paragraph. What are "landscape driven elements"? If this is a document open for public scrutiny it should be written in a way that is accessible. (Probably not considered a reason to object to the scheme but nevertheless, I feel, a valid observation.)</p>
Martin Allen				<p>Document attached regarding comments on the Stainsby Masterplan. Overall – inspirational and pioneering regarding the integral use of species-rich grassland combined with SuDs, abysmal ideas on tree planting that cannot stay as planned without reputation loss to council and design consultants, and I think you need to rearrange the position of the primary road and adjacent car-park at the northern end for health and safety reasons. Apologies I've been last minute sending this but it's been difficult to fit in timewise. I've tried to produce positive existing local examples where I have criticised or to compliment the ideas in the plan (rather than the international concepts shown in the plan) and I haven't had time to praise many of the positive things within the design, but do ask if you need further information from me or if parts of my notes don't make sense.</p>
Martin deacy				I would vote for this option
Martin deacy				There is no need for this road just because it was promised in 2014 it can still be stopped if there is a political will
Matthew Shippey				Don't want this. I walk in mandale meadow. Middlesbrough is lacking in green space and you want to build a road through to facilitate new housing. Middlesbrough Council you are a disgrace.

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Michael Lowe				Adopt option 1. The loop road through the stainsbt estate
Michael Miller	Natural England			Please see general comments under chapter 8 responses.
Michael Starford				1.9 The proposed road and cycleway route is obtrusive and bisects the Meadow and woodlands. A screened off road will deter the practice of off-road joy riding.
Michelle Hedger				The loop road option. No road through Mandale Meadow. Use the loop road option. If a road needs to be there make it a narrower road of 20mph. My children cannot play near the huge 30mph road planned. They can play now in the meadow. They will be hit by a car in this 'country park'.
Mike Conyard				Do not build on this land. This is much needed green space. There are no parks in this area and nowhere for people to walk, kids to play and people to live their lives. I find it disgusting that you propose to build on this beautiful piece of land. Also, who wants to live right next to the A19? These house will be worth literally nothing.. The land is also a buffer zone from the A19 to the residents of Heythrop Drive and beyond.
Miller Homes and Avant Homes	Miller Homes and Avant Homes	Mr Neil Morton	Savills	<p>We welcome the recognition in the foreword of the major opportunity which the Stainsby site allocation provides for Middlesbrough. Our clients confirm their commitment towards the timely delivery of the next major phase of the site's delivery in respect of the remaining southern ownership land. In this regard their intention is to work up a planning application in dialogue with the Council with a view to submission in mid-2021 if not sooner. This will be preceded by a formal pre-application enquiry which will be submitted shortly.</p> <p>We note the final paragraph at para 1.1/p.6 that future planning applications will be tested against its high standards. Whilst we clearly have no objections to the masterplan setting a strong vision and design principles, and our Clients are committed to delivering a high quality, well designed development, the masterplan represents design guidance and it should not overly restrict other forms of development which also achieve high standards and accords with Policy H21 even if this raises some conflict with the more detailed guidance set out in the masterplan. This is noting our Clients' general comments on the document (see later), which have also been made during previous consultations, that the later chapters generally go beyond the level of detail that is required of design guidance and appear overly prescriptive. Policy H21 and its criteria represents the development plan policy for the site and any "intentionally high bar" or prescriptiveness which the masterplan sets which exceeds that required by the plan would not be justified.</p> <p>We return to the Updated Masterplan itself in our response on Chapter 8, but we are generally supportive of the masterplan shown on p.13 insofar as it relates to the remaining southern ownership land. We welcome the inclusion of the Residential Zone on the northward side of the link road and south of the playing fields and feel that the broad disposition of land uses across the allocated site is now about right.</p>
MrsBarbara Postlethwaite				Don't carry this out.
Nichola Davey				This housing allocation is not necessary and should stop. Any 'country park elements' should remain within the site already under construction and NOT damage the area on Mandale Meadow, Newfield Meadow or the connecting green spaces by the addition of landscaping features, footpaths, cycle paths, play equipment or 'desire lines'.

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Nicola Smith				<p>We DO NOT WANT IT!!!!</p> <p>WHY can't you build on areas where you have flatted hundreds of houses but not rebuilt? Keith Road or Marton Burn Road??? This is the only green space we have in Acklam and hundereds of residents rely on this being walking distance. Pre covid the traffic on mandale Road, Acklam Rd and levick Cresent is bad enough at peak times without further traffic. You have Acklam Grange Secondary school round the corner from the meadow with 1200+ students that also use these roads. This is already a very busy junction without diverting more traffic to it. What about adults and children's mental health?? Has the past year taught you nothing?? I think having a open greenspace with all the wildlife and plenty of room to exercise and get much needed fresh air and been an absolute saviour to hundereds of local residents. This project has not considered anything but money. Looking at the plans there will be very little green left and it will be surrounding a road and housing estate. Absolutely nothing like we have now. This is NOT ACCEPTABLE FROM ANYONE I KNOW WHO LIVES IN ACKLAM 😞😞😞😞😞😞😞😞</p>
Nicolas Mitchell				<p>In 1.4 j... you recommend a clear span crossing rather than culverting. Why is this? It would less damaging to maybe use culverting over the beck, as happens in Low Lane, Malvern Drive, Trimdon Avenue.</p> <p>How in K. will the local wildlife site be enhanced by driving a road through the middle of it?</p>
Peter Davey				<p>This housing allocation is not necessary and should stop.</p> <p>Any 'country park elements' should remain within the site already under construction and NOT damage the area on Mandale Meadow, Newfield Meadow or the connecting green spaces by the addition of landscaping features, footpaths, cycle paths, play equipment or 'desire lines'.</p>

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Poppy Bowyer				<p>The name of Saphwood Beck has been spelled Saffwood Beck.</p> <p>What clientele is this site and houses designed for?</p> <p>This road was also supposed to relieve traffic build up? This doesn't seem to address this here.</p> <p>How do you feel that this development will work for the surrounding area?</p> <p>Will it improve the natural and urban environment directly?</p> <p>Have the locals been consulted and have confirmed that these ideas in particular are what would improve their environment?</p> <p>Is the development of habitat areas (glades, wetlands, meadows, drainage etc) going to be lead and overseen by Ecologists, Canal and River Trust employees, Flood mitigation specialists?</p> <p>Will these specialists be on site to make sure promises around elimination of harmful runoff and sedimentation into the water system is adhered to throughout the building and development of the project?</p> <p>What is the plan for long term grounds maintenance of the "Country Park"?</p> <p>Will the local residents be given training and instruction on how to manage an orchard?</p> <p>Has there been results from a local survey informing the council that a new school and sports facilities are necessary and needed within the area? What is this idea based off?</p> <p>How does this development create more public space than there is now?</p> <p>The plan details that the housing development will take approximately 8/9 years alone, how may this impact traffic management of multiple access roads to and from the A19?</p> <p>If only 5% of the housing on-site is to be affordable, where is the other 10% affordable housing going to be located?</p> <p>What is the structural landscaping described to screen the A19?</p> <p>How will this project manage to 'Enhance the local wildlife', over leaving more of the area to naturally regenerate in the North site?</p> <p>Who will audit the project and make sure the execution is finished to the standard that is promised?</p>
Rob Atkin				No road through Mandale.
Robert Teate				1.3) In the introduction, you call the site a country park and refer to the vision for the allocated housing site at Stainsby is a sustainable, mixed and balanced community set within an outstanding and engaging landscape. Having driven to the housing estate at the Brookfield end the estate is anything but a country park, resembling Ingleby Barwick,s concrete jungle than a country park.
Sean Marshall				Brilliant idea for the local community and the town

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Simon Grundy	Stockton on Tees Borough Council			<p>Thank you for consulting this Council on the 'Stainsby Country Park and Masterplan' document. This email provides a response from officers of Stockton on Tees Borough Council.</p> <p>It is noted that the site has been allocated in the existing Local Plan (2014) and that the latest version of the emerging Middlesbrough Local Plan intended for this allocation to be retained. Given the existing Local Plan designation, officers do not have any comments on the principle of the development and trust that any subsequent development proposals will be considered against the policy and legal framework in place at the time of any application.</p>
Sofia Torre				<p>You want Middlesbrough to be the same as other town in modern aspect living, but you all seem to be so backward in thinking into the future, you will allow developers decide what they use to build the properties. You just can't see it, no wonder Middlesbrough is in the state it is in. Every property should recycle its rainwater and use minimal from Northumbrian with underground tanks, each house must use solar panels, and store excess generated energy to be used in the evening, the only cars on site are electric with electric charge points, use air pumps as a method to heat water and heating systems mandatory. Social housing small houses not your 4 to 5 bedrooms, you are creating a them and us situation a discrepancy that will only incite rather than create communities. You do not expect small children because you will only build a school if there is a need, a retail centre only if there is a need. This is so not for those who earn £32,000 to £38000 a year this is for people who are earning a lot more people who are not from Middlesbrough, for southerners to buy and rent out, and potentially have the properties trashed and then a derelict site, potential slums. Instead of focusing with the properties we have in Middlesbrough. Re-cycling those properties and with new developments close t the city center they won't need to have a car. You set out bus links, how about improving the service for existing residents, that run frequently so people can stop using cars. You are going to increase car usage and don't care for the existing residents.</p>
Sonia Bowler				<p>The road design shows it coming out at a small roundabout on the A1130 which already suffers from a lot of traffic congestion that makes getting out of Heythrop Drive extremely difficult, particularly at the exit nearest the A19. This will only be exacerbated by the new road. I can already wait up to 10 mins at this junction to get out and get to work on a morning.</p> <p>All this building and concrete will have detrimental effects to the surrounding becks and their flora and fauna.</p> <p>No road through Mandale Meadow!</p>
Stacey baillie		Stacey.baillie@icloud.com		<p>What are you introducing, there is no details</p>
Stephanie Atkin				<p>No road through public greenspace. Mandale meadow was gifted to the town under a deed of covenant in perpetuity. The need for this protection is greater than it has ever been and yet it is being stamped all over?</p> <p>The vision for a country park sounds sterile, contrived, idealised to the point of lunacy. Where are the cars in the glossy images? 12,000 a day? Which other country parks have such a central feature? This is purely and simply the loss of semi-mature woodland for 'any number' (for it really doesn't matter) of sapling whips along a stretch of concrete.</p>
Stephen Connors				<p>Thoughts previously mentioned.</p>

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Steven Brown				<p>Page 10 Middlesbrough Urban Design Guide Sections d) respect and exploit already established important local and strategic views and vistas; Five foot wooden fence between my property and the new builds</p> <p>e) relate to the scale (height and massing) of the surrounding buildings; Large 5 bedroom houses being built next to my single storey property.</p> <p>g) consider the current building line; Large 5 bedroom houses being built next to my single storey property.</p> <p>h) ensure satisfactory measures are incorporated in the design and location of the development to minimise the effects of noise from any existing or potential sources nearby; Major road proposed to pass near my property</p> <p>The residents have a septic tank that discharges into Weightman land, how do we continue to use our tank if there are going to be properties built on the discharge area</p> <p>Legibility, clear definition of public/private realm Buildings and the spaces between them should work together to create key recognisable events and places that are easy to navigate through. Spaces are most successful when it is easy to identify who is meant to use them.</p> <p>k) forming easily recognisable and clear delineation between private and public land through the use of boundary treatments where appropriate.</p> <p>How will the access be gained to the new development with respect to residents of Stainsby Hall Farm . The residents of this area have paid for the road upkeep</p>
stewart jones				Super

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Thomas Maunder				<p>Residents raised serious concerns in 2018 over the devastating impact a spine road would have on the red- and orange-listed endangered species which live in very specific habitats on Mandale Meadow. The council have failed to take into account how much life has changed since 2014 due to COVID-19. A much greater level of working from home will persist after lockdown, rendering the spine road an unnecessary destructive force on a richly biodiverse meadow.</p> <p>This approach will contribute to climate change and contradicts the council's 'One Planet Living' principles.</p> <p>The vision for the 'country park' at Stainsby includes far more houses than Middlesbrough is required to build by the government and more than is required; ONS population figures indicate that Middlesbrough's population will decrease over the next ten years. Destroying the environment of a much-loved communal space may in fact increase this population loss. These semi-wild natural areas have been used heavily over lockdown and are critical to residents mental and physical health.</p> <p>Data shows Middlesbrough has an ageing population and bungalows will be needed more than 3,4 or 5 bedroomed houses. The new estate will not be a balanced community as only 5% of the housing is allocated for affordable housing which, as a deprived industrial town, is one of Middlesbrough's greatest needs along with bungalows for the elderly. The landscaping strategies in the current masterplan could be used very successfully in the development of brownfield sites, providing people with quality houses in natural green spaces to enjoy and as these brown field sites will be closer to the town it will make them more attractive to both the young and the elderly.</p> <p>*p6, 1.1 the proposed development at Stainsby will not create a balanced community as 95% of the houses will be unaffordable nor is it clear if any will be bungalows for the ageing population.</p> <p>*p6, 'Middlesbrough Council, Housing Developers, Ward members and local people alike have contributed to the future vision for the site' local people have consistently said 'No Road Through Mandale Meadow'.</p> <p>*p7, 1.2 Desk top assessments are not reliable indicators of the wildlife or specific habitats on Mandale meadow, if they were they would have highlighted rare and endangered flora and fauna.</p> <p>*p8, the Bluebell Beck valley should remain as it is, you cannot 'enhance' natural beauty which has taken decades, if not centuries, to mature the way it has.</p> <p>*p8, 1.3.1 'standard' builds are not what is required in 2021 going forwards. All new builds should be designed to be green and energy efficient. This is something which was pointed out in 2018. In 2025 gas heating will be banned in new homes so all new builds at the proposed country park should have either air source heat pumps or ground source heat pumps installed or other green energy which is available. A speed limit of only 30mph means drivers will be going much faster and that is untenable in a 'country park'.</p> <p>*p8, 1.3.2, the 'jewel in the crown' is the existing open public space on the Bluebell Beck valley which should remain in pristine condition.</p> <p>*p9, 1.3.2, The vision of a 'country park' does not enhance the existing open public space, it actively destroys it by the encroachment of houses on what is currently much valued and appreciated open agricultural land seen from Mandale meadow. The construction of a road will destroy a variety of existing habitats containing red and orange listed endangered species and goes against the Council's commitments to 'Climate Emergency' and 'One Planet Living'. We need to start protecting nature before it is too late. There is very good documented evidence of newts, water voles, rare birds, butterflies and moths on Mandale, all of which are under serious threat if this unnecessary spine road goes ahead, they don't and can't "just move on" as some of the habitats provide the specific needs of certain flora and fauna. Desktop software such as MAGIC has clearly failed to assess the wildlife on site. There is next to no chance of the Council facing legal challenges if it drops the spine road from the LP as it has enough evidence to prove a change of direction is justified.</p> <p>*p9, 1.4 'Enhances the Local Wildlife Site in the north of the site and provides compensatory provision for any loss of habitat required for highway</p>

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				<p>access' this is nonsense, building a spine road on Mandale meadow will destroy local wildlife sites, you can't compensate for wiping out Red listed endangered species. There is a name for that - ecocide.</p> <p>All the described benefits of a 'country park' can be achieved without damaging or building on Mandale meadow or the existing open public space on the Bluebell Beck Valley</p> <p>*p10,1.4, The 'country park' does not relate well to the surrounding geography and history of the land if a road goes through Mandale or if excessive house building ruins the open agricultural aspect currently enjoyed by residents. The habitats on Mandale which would be destroyed by a road cannot be replicated in the Stainsby development as they take decades to develop and there will be chemical residues on the farm land which will not be conducive to quickly forming new habitats. Mandale meadow has not been subjected to pesticides and other agricultural chemicals which increases its value in terms of a place where nature can flourish. Mandale meadow is far from the 'mown field' it has been described as by the Council, it rather proves that site visits with knowledgeable locals as guides is more beneficial to understanding a location than maps which provide aerial views</p> <p>*p11, 1.4 The Ten One Living Planet principles. The spine road and unnecessary 'development' of the existing open public space on Bluebell Beck valley immediately goes against some of the principles adopted by MBC. Land use and wildlife, building a road on Mandale will wipe out endangered wildlife which have specific habitat requirements which are not replicated elsewhere on the Bluebell Beck Valley and some cannot be replicated by man because of complex soil fungi which nurtures specific plants in specific habitats which produce the correct environment for endangered wildlife. Zero carbon, MBC will significantly raise carbon emissions by releasing stored carbon from felling trees and churning up meadow land. Building a road through Mandale will raise carbon emissions in the Ayreome ward. Zero Waste, using excavated spoil to form mounds on rich meadow land is more ecocide. Do it on the proposed new estate/country park by all means but leave the Bluebell beck valley and Mandale alone. Sustainable water, SuDs on Mandale have not been successful so careful consideration needs to go into developing new SuDs, a large lake, similar to Hemlington lake, on the new estate/old farmland might be a way forward to prevent flooding issues towards Stainsby North. Culture and Community, damaging Mandale meadow will not create wellbeing for Ayresome ward residents, or the many people from other wards/areas who either live adjacent to the meadow or travel in from other wards, it will actually cause the opposite with more air and noise pollution close to homes which cannot be mitigated by planting 30cm whips. Indeed, the pursuit of greater council tax harvesting will make residents lives miserable and prevent some residents from accessing the meadow due to traffic pollution. Health and happiness, as above, this scheme to damage Mandale will make residents extremely unhappy and have a negative impact on both physical and mental health. Equity and local economy, there is no equity in taking away a much loved and used meadow from Ayresome ward residents in order to give more land to new estate home owners. The impact of building only has a short term benefit for the local economy.</p>
Tony slater	MJBON/A			<p>Second paragraph of the introduction,when did Middlesbrough become a city?</p> <p>The last time I looked at this now updated report there have been significant changes,trends that appeared to be going down are now going up.</p> <p>Where did these figures come from?</p> <p>Have the concerns regarding atmospheric pollution suddenly disappeared and it is now safe to breath the increased exhaust gases.The maps situated at the final chapters are not clear enough but strongly suggest that a large road is going to be added to the roundabout located at Mandale.Is this road a dual carriageway?What will its speed limit be?How close is it to the rear gardens facing the road?This report is simply duplicating concerns evidenced from the previous master plan.Its worth repeating,many people didn't want a through road to be associated with the country park.An alternative response would be to have the road conclude at the park.This would reduce the amount of vehicular traffic significantly affecting the levels of pollution instead of increasing them.</p>
Trevor King				<p>We can't control what happens on privately owned land but we want NO ROAD THROUGH MANDALE MEADOW/WOODS.</p>

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Val Young				No road through the meadow.
Vanessa collett				You skirt around the subject, your selling an ideal but failing to make clear the number of houses on the site. The headline of stainsby country park with designs showing the green space but not where the houses are. I feel you are trying to blindside us in to thinking this is the development of green space to provide leisure space for the community when in fact your building almost 2000 homes, a major road through the middle that isn't going to relieve the pressure on the transport system. To try and flower it you talk about adding a community orchard which will be the odd fruit tree dotted through the site.
	ML Retail Ltd	Matthew Sobic	Savills UK LTD	The Vision included at Section 1.3 of the Masterplan confirms that: 'The vision for the allocated housing site at Stainsby is a sustainable, mixed and balanced community set within an outstanding and engaging landscape, that includes the creation of a new country park, providing over 1500 new homes. This iteration of the masterplan seeks to flesh these out and provide greater detail and clarity around these themes.' It is important to note that the Masterplan area for Stainsby does not include all of the land either within the Brookfield allocation as defined by the HLP. The Masterplan covers the section outlined in red in Figure 1 below, and not the residential development that has been delivered at the south east section of the defined Brookfield allocation, which has provided 343 homes under Permission Reference M/FP/0572/11/P.
Dave Blackburn				If people want a park, they can go to Stewarts park. Most people have cars now so it wont be problem. People come all over Middlesbrough to use our field. From the town to Thornabyto Stockton. They do not want a park. They want land that is wild and free like I do.
Doris Hammond				My feeling is that the children do not need another park, what they need is the opportunity to play like mine did in the safety of mandale outside our back. I need lifts now but each time I pass the carpark it busy with dog walkers and children so it is well used. I feel as if the big boys with the power always win.
Gill Sullivan				While the country park on paper is a good idea that might be all it is for the following reasons Middlesbrough have earmarked another country park at whinney banks but all the residents received were a green and a children's play park. Again In coulby residents were promised a duck pond planting and a village green. What they received was some planting, no green and a soaker pond. MBC cannot grantee a country park. Nor can they guarantee housing numbers. So you want acklam to give up mandale field on a gamble that you can make developers to honour MBC wishes. They will not and well MBC know this. They will carve up the fields and then mandates will be maximum profit. Their agents will manage the green spaces not the Council as they add their routine extra 20% housing, Mr Clarkes work or art will soon be compromised

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				by added volume and density. Everybody knows that who work in planning but as usual unless you go hunting for it residents are left in the dark. MBC cannot guarantee what developers will do with that space. Limiting the infrastructure will contain them. Please mbc limit the infrastructure.

Question 3 – Please provide any comments you may have on Chapter 2.0 Site Analysis

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Alexander Henegan				Attention is rightly given to bio-diversity and wildlife. Risk of flooding is also mentioned. In January and February of this year the area nearest to Mandale Road has been very wet and marshy. Air quality will be a major consideration given the proximity of the A19 to the development and noise from major roads will be hard to exclude from areas where people are living and taking exercise.
Aji Varghese		Aji Varghese		Good
Alan Liddle	Stainton and Thornton Parish Council			One omission from the site analysis is the fact that the development NEEDS to be connected via footpath and cycle-way from the development OVER the A174 to link up with the footpaths to Stainton Low Wood footpath MID011 and Low Lane.
Alan Turley				<p>Environmental Impact: The Masterplan document states “The character of the development will be defined by the landscape setting and the importance of nature. It will enhance and extend existing habitats” What evidence have the council got that this is achievable ? The land currently utilised as farm land serves to provide a natural habitat for a variety of species including deer and foxes. Both species are naturally suspicious of humans and avoid contact wherever possible. To suggest that they will happily co exist alongside this development is clearly inaccurate. They will be displaced from the last remaining natural habitat. Local reports already indicate that deer have been displaced and sighted in urban areas in the neighbouring Borough. Deer alongside a road with a high volume of traffic is a recipe for disaster for both them and motorists.</p> <p>The existing hedgerows that provide and have provided a home for other animals for over a hundred years will be wiped out as they have already been on the existing phases.</p> <p>The building of houses in place of farm land will never alter the carbon footprint of the town for the better. The current fields provide much needed natural drainage which will be lost should the development go ahead. It’s also safe to assume that each house built will have at least one car and will be drawing resources such as gas and electricity. How does any of that equate to meeting the councils reduced carbon targets.</p> <p>Historical Impact: The existing land has been farmed for over a hundred years and is the last remaining example of the agricultural heritage of the Acklam area. The council has already permitted building on other previously farm land in the town stretching from the borders of Hambleton to Thornaby. To allow this development to take place will rob future generations of the opportunity to see the land being worked and the nature that co exists within the land.</p>
Amanda Stevenson				The proposed road through Mandale Meadow will have a devastating impact on the wildlife and habitat and will prohibit wellbeing and enjoyment of the public who currently use it.
Amy Lord				2.4 site views states that Mandale meadow is mown grass - this is not the case. This meadow and the surrounding fields are usually left wild, with the grass cut once or twice a year. They are wild meadows, not maintained parkland. On page 21, you note 'bat species are expected within Bluebell Beck.' I live beside the beck and we have seen bats in the area.

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Andrea Spooner				Site analysis is never objective when trying to promote something that you are trying to sell it all about the income generation. Look at it from another angle-currently, we have a wide open unspoilt wildlife habitat space used by many local residents for exercise and mental wellbeing but looking at the site analysis we lose a vast area of this area of natural beauty.
angela barber				AS a resident of this area, I fear my house value will lower so close to this new road. There will be No where to walk my dog There would be Little or no space for children to play safety in area An area to exercise my self and enjoy picnics with the family Local wild life will loose there home and endangered species plants and animals will loose there environment Why should be suffer ill health because of further pollution We pay council tax for green space- will our council tax lower-expect not Increased noise during the day and night- lack of sleep and depression result All you want to do is provide new access road to the estate- why cannot they come around down downside and Acklam road instead as always has happened. I feel the new road will increase traffic into Middlesbrough through our area. I do not want or need the road as I do not drive- I doubt it will be included on the bus route, so it does me no good. In fact everything about it is BAD
Angela O'Byrne				Hands off our meadow!
Barbara Keville				2.0 Site Analysis. Fig 8 Species As a bird serious watcher who has spent a great deal of time during Lockdown walking around the proposed housing developments I have seen a multitude of species not listed to name just a few.....Siskin, tree creeper, bullfinch, chaffinch, sparrow, great spotted woodpecker, sparrow hawk, buzzard, jay , blackcap, chiffchaff and white throat. You mention redshank and I doubt very much that there will ever be habitat suitable to support a wader species.
Barry pinkney				Country park no
bernard smith				where all the water going
Beverley cooper				Cant access plan
Carol Newmarch				I object to the whole plan
Carolyn Dodds				2.10. Noise pollution is a cause of great concern and mitigation needs to be scrutinised.

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Catherine Howell (Boyle)				<p>2.3 The MBC owned land was under covenant to be kept as green open space for the people of Middlesbrough. This covenant was lifted without adequate public consultation at the time.</p> <p>2.5 The MBC owned land is frequently flooded and impassible. Use of heavy machinery will compact the earth and cause greater flooding problems.</p> <p>2.6 Site access - the access car park to the north is currently used as an informal 'park and ride' facility too. How will this be accommodated? Additionally, pedestrians use this route to move between Middlesbrough and Thornaby for commuting and to access shopping facilities. How would this be accommodated? The route is currently very unsafe due to the number and speed of vehicles using the dual roundabouts. Cycling across the Mandale Roundabout is very dangerous.</p> <p>2.8 "Retain trees where possible" - arbitrary. MBC is undergoing a programme of extensive tree planting. It is not acceptable to remove any mature trees given the climate emergency already acknowledged by MBC and this is not compatible with either the proposed Green Strategy or the OPL framework.</p> <p>2.9 The traffic noise and air pollution from the A19 and A174 are already unacceptable to local residents. The fumes on Mandale Roundabout make it impossible to walk or cycle comfortably. The traffic noise can be heard from my residence at all times (approx .5 miles from the A19). The easiest, cheapest and most effective mitigation for these issues is to not develop the space at all.</p> <p>2.10 "Site is not in a conservation area" but it is a site of high conservation value.</p> <p>p20 "Low site biodiversity." Sightings of (amongst others) dingy skipper butterflies, newts and bee orchids are noted locally.</p>
Charlotte McAdam				<p>2.6 - page 17 - you state that the North access onto Mandale roundabout "has the potential to be enhanced to serve as an additional entrance to the proposed Country Park. It already serves as access to the green space for dog walkers and the local community and will be retained and enhanced in its function." This suggests that the access point is not necessary, and that the singular access point on Jack Simon Way would be sufficient. The idea that this space would be retained and enhanced in its function is laughable; a road running through the area would render it almost useless in its purpose as an area for dog walkers. It would make it unsafe for dogs and wildlife, regardless of the speed restrictions put in place. It would devalue the area for local residents. The diagram below shows one access road onto the roundabout on Low Lane, with two divergences. A loop could be created there for the new houses, leaving the Meadow as it is.</p>

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Chris King				<p>'There is a small area of the site that has been identified as a flood risk zone, predominantly along the course of Blue Bell Beck and particularly just before it is culverted under the A19; at the highest point of the tidal reach.'</p> <p>Anticipated sea level change as a result of global warming, coupled with an expected increase in annual precipitation is likely to make this area of potential flooding increase significantly in time.</p> <p>'North Access The northern access point is from the existing Mandale roundabout where there is already an access to a small car park. This car park has the potential to be enhanced to serve as an additional entrance to the proposed Country Park. It already serves as access to the green space for dog walkers and the local community and will be retained and enhanced in its function. '</p> <p>As mentioned above, the proposed access point is already over burdened with traffic and as a commuter myself I can confirm that it is often very busy and challenging to negotiate particularly at times such as when parents bring their children to school or people are travelling to work. Adding a further entrance with yet more traffic is clearly a very poor solution and likely to compound existing congestion.</p> <p>2.9 Air Quality and Easements ' The most significant issues regarding air quality is anticipated to be traffic noise from the surrounding roads. The A19 & A174 both have high volumes of traffic.'</p> <p>The quality of the air in the Acklam area is often filled with noxious odours and the smell of burning (industrial). It will be interesting to see how the new development will prevent any further contribution to current atmospheric pollution, particularly in respect of proposed new industrial units. Providing new areas of woodland as part of the development will clearly be helpful but on balance at what cost to the existing woodland areas.</p> <p>I notice with interest the limited area that has been identified as being subject to noise pollution. As someone who lives on Heythrop Drive, we are well aware of the present roar of traffic from the A19 and I would therefore respectfully suggest that the areas indicated are likely to be much, much greater.</p>
Christine King				We can't control what happens on privately owned land but we want NO ROAD THROUGH MANDALE MEADOW/WOODS.
Claire Boyd				We can't control what happens on privately owned land but there should be NO ROAD THROUGH MANDALE MEADOW/WOODS.
Claire Di Cicco				No to a road.
CLAIRE KANE				The road will destroy a meadow the a19 is in close proximity to use

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Claire Wilks				<p>2.8 Accessing the site to construct a road will result in heavy machinery destroying natural habitats for many wildlife across the open spaces. Although the trees are not perceived as being 'mature' they are very old with an abundance of wildlife that may not have yet been discovered.</p> <p>2.11 As mentioned with the map from 1853, the landscape has thankfully not changed. Imagine the amount of habitats for wildlife, trees and flora would be destroyed by changing this area. Sadly, the ancient woodland on the Middlesbrough side has already been built in as the A19 and roundabout running through it already and is now disconnected from the site.</p> <p>2.5 The building could also cause more flooding along the existing</p> <p>*no figure 10 to see 'development of peripheral areas' pg21</p> <p>Species: there are many species of birds such as the 'tree creeper' which I saw recently, that will not remain if machinery and development of the road do ahead. There are so many species that are unknown across the whole area.</p>
Colin and Morag Bailey				<p>*p15, 2.3 Farmer Pearson should not be forced into selling the farm he loves through compulsory purchase, it's just plain wrong and the views over his agricultural land enhance the green wedge which we have.</p> <p>*p16, 2.4. it is deliberately misleading to refer to Mandale meadow as "a mown field". It is a meadow and the photo (view 1) was taken at hay harvesting.</p> <p>*p18, 2.5. Flood risk is raised at the lower reaches of the beck even more as agricultural land is built on.</p> <p>*p18, . 2.6 For the reasons mentioned in the response to the Introduction there is no necessity for a road to go through Mandale meadow and exit onto the A1130, a loop road for the estate is entirely sufficient due to changes in lifestyle since the covid pandemic..</p> <p>*p18, 2.7, some of the undocumented PROWs through Mandale meadow are highly prized by residents precisely because they are narrow, not surfaced and bring you closer to nature with good views of a variety of insects, one is so enchanting it is named 'the fairy path' by residents. This path cannot be seen on aerial maps. There is a gravel path from Mandale to Newfield which adequately serves the local community. Any path building on the existing open public space will degrade the site.</p> <p>*p19, 2.8. The ribbon of trees starting from Mandale may only be semi mature but it has taken 30 to 40 years for them to achieve that growth. Felling any of them is unacceptable for a number of reasons, a) you can't replace semi mature trees with small whips (many of which will die) as they cannot soak up water, absorb carbon, emit oxygen or provide easement for noise and air pollution from the A19.</p> <p>*19,2.9 The trees which provide easement on the Northern sections, i.e. Farley Copse and Mandale woodland don't ease the traffic noise as it is and residents already find it too intrusive. Building a road through Mandale meadow will massively increase the noise pollution and even more so if you fell the trees which provide a small degree of easement. A road even closer to residents properties will make life intolerable for them.</p> <p>*p19, 2.10 Mandale meadow should be awarded Conservation status because of the endangered species on it.</p> <p style="text-align: center;">p3</p> <p>*p20, 2.11, Fig 2, It isn't true that most of the existing woodland would remain if a road is ploughed through Mandale meadow as much of it would be lost.</p> <p>Fig 5, it is totally untrue to say there is low biodiversity on the northern section as Mandale meadow is the most richly biodiverse are of the whole of the Bluebell beck valley as can be seen from the greenstuff response to the 2018 consultation.</p> <p>Fig 8, this proves how inadequate a desktop survey is. It is only showing tree sparrow and lapwing on Mandale meadow. Please see the wildlife maps and keys on Greenstuff file section.</p>
Dennis Mccabe				My only problem is the location of the road onto Mandale Rd.
Emily quinnt				No road necessary. Do you want people and animals to get run over?
Francine Kenny				Yes to Country Park Yes to houses yes to road

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Geoffrey Payne				Page 18 2.6 Site Access: No mention is made of the congestion problems on the A1130, it merely concentrates on the car park.
Gill Mollard				This housing allocation is not necessary and should stop. Any 'country park elements' should remain within the site already under construction and NOT damage the area on Mandale Meadow, Newfield Meadow or the connecting green spaces by the addition of landscaping features, footpaths, cycle paths, play equipment or 'desire lines'.
Glen Instone				Pg 17: View 5 is not part of the plan so why is it shown? This is a privately owned paddock. Pg 24: 2.17 Retained views for who exactly? Are you suggesting any additional tree planting to at least keep some privacy?
Graham Hadfield				<p>On page 15 it is stated "Due to bordering the A19 and A174 the site itself is isolated from the neighbouring green field land and is currently inaccessible to public access." That is patently untrue as there is daily public access by the public, taking exercise and enjoying the existing beauty of a real (as opposed to manufactured) country park. The blatant falseness of the statement is confirmed by the statement alongside "Middlesbrough Council own the public accessible land to the East which incorporates the Green Open Spaces, Woodland and Blue Bell Beck.". Further confirmation is provided by the statement on page 18 "Within the council owned Public Open Space land there are a number of footpaths that are well used by the local community".</p> <p>Page 18 states "There is a small area of the site that has been identified as a flood risk zone, predominantly along the course of Blue Bell Beck and particularly just before it is culverted under the A19; at the highest point of the tidal reach.". It makes no mention of the fact that the meadows become saturated and marshy following the regular periods of prolonged wet weather that occur. It makes no mention that this is a feature of the wider landscape including the Acklam Grange School field and the lower lying area of the public field bordering the A19 to the north of Mandale Road. How can it be claimed that flood risk has been properly considered when those factors are not even mentioned?</p> <p>Page 18 states that "There are only two potential access points to the site.", one of which is via Jack Simon Way (which connects to the A1044). That is patently untrue as it ignores the existing farm access roads which exit the site via a bridge over the A19 to the A1130 west of the A19 and via a bridge over the A174 to the B1380 alongside the Sporting Lodge. Those exits are obviously potential access points to the site as they already exist and fulfil that function. For example, the single track road bridges could either be replaced by wider bridges or could be light controlled (as are many other bridges in England).</p> <p>Page 18 states that the existing car park at the north end of the site will be retained. It completely fails to address the fact that the car park has recently (since the publication of the 2018 Masterplan) had wooden fences replaced by metal ones. One reason that the fences were installed was the use of the meadows by drug couriers using off road motorcycles and quad bikes which the beck valley was plagued with for years because of poor maintenance of fencing at Mandale car park. That lack of maintenance was taken advantage of by travellers in August 2019, which caused distress to residents and other users of the meadows and expense to the council to fund the clearing up of the resultant mess. Those problems have subsequently been overcome by the erection of the metal fences.</p> <p>It would be a mistake to think that the problems have gone away though as a motor cyclist was witnessed trying to gain access as recently as 2 November 2020, having come from the north along the path through the field between the A19 and Downside Road. Building a road which exits Mandale Meadow at the Levick Crescent roundabout would enable further such incursions, attendant expense and damage to the new country park. No account appears to have been taken of these factors in proposing that the road be built from the site of the current car park.</p> <p>Page 19 claims that "Although there are the occasional mature trees much of the woodland is relatively new". Whilst the woodland bordering Mandale Meadow may not be hundreds of years old it is many decades old so it is totally misleading to describe it in any way as "new", whether relatively or not. What is certain is that it would take many years for new planting to reach the same level of maturity were the existing trees to be destroyed by driving a road through the existing woodland.</p> <p>Page 19 claims that the proposals will mitigate noise pollution. No provable evidence to support that claim is provided. Real life experience shows that the same sorts of measures along the A19 both north and south of Mandale Road do not prevent road noise disturbance to properties in Downside Road, Heythrop Drive and surrounding roads.</p> <p>Page 19 states that "Two large sewer pipes pass under the site and no development can be built within their easements.". Note that the</p>

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				<p>statement “no development” is not qualified and building of a road is obviously development. It is admitted on the same page that “Easement around sewer pipe throughout the site will restrict certain uses and landscaping approaches.”. The revised route of the road as shown on Page 12 would involve it being built over both pipes, obviously within their easements and on ground which becomes saturated and marshy following the regular periods of prolonged wet weather that occur.</p> <p>Page 20 states, without qualification, “Existing woodland will be retained and enhanced”. That commitment can not be achieved when the proposed route of the road will destroy mature trees at the north of the site.</p> <p>Page 20 states “Bluebell Beck however has the potential to deliver significant environmental benefits in the form a of a wide range of habitats including woodland, wetlands, grassland and riparian habitats and has a high priority.” Bluebell Beck already incorporates such habitats. The destruction of the existing habitats hardly enhances the potential to deliver significant environmental benefits.</p> <p>The note on species on page 21 mentions a mere fraction of the wildlife species which currently inhabit the publicly owned land. It is obvious that insufficient work has been done within the exercise undertaken by Gradon Architecture and Valley Environmental to establish a complete and accurate inventory of existing wildlife species (as opposed to those which just happen to have been recorded on the DEFRA MAGIC website)?</p> <p>Page 24 claims that the plan “highlights potential conflicts and opportunities to be resolved by further design.”. The various points above illustrate that the highlighting is lacking in many respects (including, apparently, the initial specification given to Gradon Architecture and Valley Environmental) and can not be relied on as a solid foundation for further design without significant revision to remedy its shortcomings.</p>

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Graham Hadfield				<p>This is an addendum to my comments submitted on 5 February, 8 February and 22 February. Please ensure that each of the contradictory/inconsistent points in the “Masterplan” which are listed below are fully resolved in the final consultation report.</p> <p>Statement: “Due to bordering the A19 and A174 the site itself is isolated from the neighbouring green field land and is currently inaccessible to public access.” Reality: There is daily public access by the public, taking exercise and enjoying the existing beauty of a real (as opposed to manufactured) country park. This is confirmed by the statement alongside “Middlesbrough Council own the public accessible land to the East which incorporates the Green Open Spaces, Woodland and Blue Bell Beck.” and the statement on page 18 “Within the council owned Public Open Space land there are a number of footpaths that are well used by the local community”.</p> <p>Statement: “There is a small area of the site that has been identified as a flood risk zone, predominantly along the course of Blue Bell Beck and particularly just before it is culverted under the A19; at the highest point of the tidal reach.” Reality: As current ground conditions demonstrate, the the meadows become saturated and marshy following the regular periods of prolonged wet weather that occur. There is no mention that this is a feature of the wider landscape including the Acklam Grange School field and the lower lying area of the public field bordering the A19 to the north of Mandale Road.</p> <p>Statement: “There are only two potential access points to the site.”, one of which is via Jack Simon Way (which connects to the A1044). Reality: The Stainsby Link Road Initial Option Assessment Report shows more than two access points and ignores the access over the A174 to the B1380 alongside the Sporting Lodge.</p> <p>Statement: “Although there are the occasional mature trees much of the woodland is relatively new”. Reality: Whilst the woodland bordering Mandale Meadow may not be hundreds of years old it is many decades old so it is totally misleading to describe it in any way as “new”, whether relatively or not.</p> <p>Statement: Page 19 claims that the proposals will mitigate noise pollution. Reality: People in existing houses to the east of the A19 suffer noise pollution already.</p> <p>Statement: “Two large sewer pipes pass under the site and no development can be built within their easements.”. Reality: Building of a road across the route of the sewer pipes (which is obviously development) is proposed.</p> <p>Commitment: “Existing woodland will be retained and enhanced”. Reality : The proposed route of the road will destroy mature trees at the north of the site.</p> <p>Statement: “Bluebell Beck however has the potential to deliver significant environmental benefits in the form a of a wide range of habitats including woodland, wetlands, grassland and riparian habitats and has a high priority.” Reality: Bluebell Beck already incorporates such habitats which will be destroyed by culverting.</p> <p>Statement: The plan “highlights potential conflicts and opportunities to be resolved by further design.”. Reality: That statement, in itself, highlights the fact that the current design is based on drastically insufficient evidence.</p>
Helen McGregor				<p>While the fields have relatively low biodiversity, a road through the back area would be detrimental to the much higher level of biodiversity there. Drainage into the beck would also be reduced as the lower lying end of the meadow and woods are predominantly water-holding, hence why the ponds were built.</p>

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Henry Cumbers	Historic England			<p>The Scheduling of Stainsby Medieval Village and open field system, includes the deserted remains of the medieval village of Stainsby and part of its surviving open field system, situated on the east bank of the Stainsby Beck</p> <p>The earliest known reference to a settlement in the area is contained in the Domesday Book of 1086 when it is recorded as 'three carucates at Steinesbi'. The village survives as a series of earthworks and buried remains in the fields south and east of Stainsby Grange Farm. At present there is no consideration of how development of Stainsby Park would impact on the setting of Stainsby Medieval Village this asset within the draft masterplan. Paragraph 193 of the NPPF states that "when considering the impact of a proposed development on the significance of a designated heritage asset, great weight should be given to the asset's conservation (and the more important the asset, the greater the weight should be). This is irrespective of whether any potential harm amounts to substantial harm, total loss or less than substantial harm to its significance."</p> <p>Paragraph 194 states that "any harm to, or loss of, the significance of a designated heritage asset (from its alteration or destruction, or from development within its setting), should require clear and convincing justification." It will be necessary for the Council to determine the level of harm (if any) that may arise from development of the site on the Scheduled Monument, and act accordance with paragraphs 195 and 196 of the NPPF.</p> <p>On site non-designated archaeological interest</p> <p>Whilst there are no designated heritage assets on the site, the potential for unknown buried assets of archaeological interest should not be disregarded. Indeed the local area has a range of sites of high archaeological importance, including the already mentioned Stainsby Medieval village but also other archaeological sites within the local area, some of which include:</p> <ul style="list-style-type: none"> • Roman villa site at Ingleby Barwick (3.25km west as the crow flies) • Larchfield Farm where evidence of an Iron Age or Roman-British farmstead was found (3.2km to the south east) • Acklam Hall and Scheduled medieval fishponds (ID 1018944) and the undesignated but potentially nationally important moated site (1.25km to the north east) <p>Stainsby Park is a large greenfield site which may be of high archaeological importance. Through evaluation the site may be found to be linked to the deserted medieval village of Stainsby but also offer the potential for Prehistoric and Roman settlement. Footnote 63 of the NPPF to paragraph 194 states that "non-designated heritage assets of archaeological interest, which are demonstrably of equivalent significance to scheduled monuments, should be considered subject to the policies for designated heritage assets."</p> <p>We therefore consider that it is important that prior to finalising any masterplan that an appropriate level of assessment is undertaken to determine potential for archaeological interest. This may in turn influence deliverability of parts of the site.</p> <p>Finally, we should like to stress that this opinion is based on the information provided by the Council in its consultation. To avoid any doubt, this does not affect our obligation to provide further advice and, potentially, object to specific proposals, which may subsequently arise where we consider that these would have an adverse effect upon the historic environment.</p>
Hilary Morley		Hilary Morley	Resident	I don't think this information is reader friendly but I will refer back to my previous comments. In chapter 1.0
Ian Marron				In section 2.4 include a new view (6) of the Bridge over the A174 adjacent the Stainsby hall farm to Stainsby vale farm link road. In Section 2.6 as an alternative route include the second option of a South link road onto B1380 and remove the North link road to Mandale roundabout. With the green cycle route remaining through to link up with Mandale interchange.
Ian Rogers				2.9 Air Quality - How will building a spine road for up to 12,000 vehicles per day through a meadow, adjacent to the A19 help improve air quality? You will effectively be moving more vehicles closer to peoples homes, particularly around the proposed junction with Mandale Road. Mandale interchange is already very busy, to add thousands of cars to this junction at peak times is madness. It will increase congestion on Acklam Road, possibly delay traffic exiting Trimdon Avenue, as it backs up and will undoubtedly increase noise and air pollution for many Acklam residents.
Jacqueline Jones				I can see no ecological report and the negative effect on the local wild life site

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Jacqueline Young				What proof is available that this analysis has been carried out. Please include these reports
jamie barber				frankly, none of this should go ahead. you're planning to destroy natural green space and animal habitats that are at a premium in Middlesbrough.
Jamie Killington				Option 1 or Option 4 would be best for traffic control.
janice slater				<p>2.3 site ownership Middlesbrough Council own the public accessible land to the East which incorporates the Green Open Spaces, Woodland and Blue Bell Beck.</p> <p>Report of: Executive Director for Growth and Place: Kevin Parkes Executive Member for Economic Development and Infrastructure: Councillor Lewis Young Submitted to: Single Member Executive, 23rd January 2019 Subject: Stainsby Draft Master plan and Next Steps Summary Proposed decision(s) That the Executive Member approves: a) the draft master plan of the Stainsby scheme; b) the re-advertisement of the appropriation of land from open space to planning purposes to demonstrate the changes to proposed developable land and road corridor at Stainsby North; c) the re-advertisement of the disposal of land held as public open space to demonstrate the changes to proposed developable land and road corridor at Stainsby North; d) the development of a detailed master plan; e) the completion of all the required site investigations; and, f) the submission of a Planning Application for the Stainton Way Western Extension.</p> <p>in this report ...the following point was made.....</p> <p>39.The proposed disposal and appropriation area originally showed the whole red line area for Stainsby North as being sold and appropriated for planning and development. There is a material change in the area to be disposed of and the first notices are therefore inaccurate. Whilst members of the public have been able to put their opinions forward on the draft master plan they won't be given the opportunity to put forward their views or objections on the actual area of Public Open Space to be disposed of if the Council don't re-advertise.</p> <p>This fact was mentioned by many in the responses at consultation level to the Stainsby plan ... As the " appropriation and change of use for Mandale Meadows " was attempted during the Newfield housing fight. When it was discovered the residents included the protection of Mandale in their fight too, and no local advertising for this change of use was forthcoming.</p> <p>in the following meeting..... Report of: Richard Horniman - Director of Regeneration and Culture Councillor Ashley Waters - Executive Member for Regeneration Submitted to: Executive - 22 December 2020 Subject: Stainsby Country Park (formally known as Stainsby/Stainsby Detailed Master plan) 51 & 52 Public Consultation</p>

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				<p>Impact of Recommended decisions Legal</p> <p>51.The Public Open Space Disposal and Land Appropriation process previously conducted for the Council land at Stainsby North was not formally implemented after advertisement and the land remains held as statutory open space under the Open Spaces Act 1906.</p> <p>52.To facilitate the development of the SWWE, the Council owned public open space required for the road corridor will need to be appropriated under S122 (1) of the Local government Act 1972. As such this will firstly require advertisement under S122 (2A) of the Local Government Act 1972 and then consideration of objections received prior to any appropriation being implemented by the Council. As the Council will not be looking to dispose of any land, the Public Open Space Disposal process under S123 of the Local Government Act 1972 will not be required.</p> <p>as this is a formal requirement and as such members of the public are entitled to pass their opinions on such recommendations I wish to formally request details of any present or future advertisement of POS under S122(1) and (2A) appertaining to the SWWE road and / or land at Mandale meadows which is still held by the council as Statutory Open Space under the OSA 1906.</p> <p>Any attempts for change of use or sale or appropriation of any of the land held by MBC in this area should be advertised not only in a local gazette but also by as many media sites as possible because of the implications that have already been uncovered during previous consultations.</p>
Jean Franklin				<p>Page 18 Blue Bell is a flood risk zone just before it is culverted under the A19. Creating a road in this area is going to increase the risk of flooding particularly as there is concern over rising sea levels and this area is tidal.</p> <p>Page 19 'Traffic noises it quality and noise pollution from the A19 and A174 will need to be mitigated.' From our garden on Hesleden Avenue the noise level from the A19 is already very loud and intrusive. It has increased greatly in the last couple of years. Adding in an extra road will greatly exasperate this problem bringing the noise and pollution nearer to residents. This makes using outdoor space a stressful experience</p> <p>Page 20 Only a desk top analysis of the habitat and diversity of the site has been done. This area currently has a rich diversity of wild life and species, these will be driven out by this new development</p>
Jenny McCullagh				<p>The privately owned land is a large enough site without destroying the beck and meadow areas as well. There are so many areas of derelict land in Middlesbrough without ruining one of the only nice public spaces we have left.</p>

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Jill Hadfield				<p>On page 15 it is stated “Due to bordering the A19 and A174 the site itself is isolated from the neighbouring green field land and is currently inaccessible to public access.” That is patently untrue as there is daily public access by the public, taking exercise and enjoying the existing beauty of a real (as opposed to manufactured) country park. The blatant falseness of the statement is confirmed by the statement alongside “Middlesbrough Council own the public accessible land to the East which incorporates the Green Open Spaces, Woodland and Blue Bell Beck.”. 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How can it be claimed that flood risk has been properly considered when those factors are not even mentioned?</p> <p>Page 18 states that “There are only two potential access points to the site.”, one of which is via Jack Simon Way (which connects to the A1044). That is patently untrue as it ignores the existing farm access roads which exit the site via a bridge over the A19 to the A1130 west of the A19 and via a bridge over the A174 to the B1380 alongside the Sporting Lodge. Those exits are obviously potential access points to the site as they already exist and fulfil that function. For example, the single track road bridges could either be replaced by wider bridges or could be light controlled (as are many other bridges in England).</p> <p>Page 18 states that the existing car park at the north end of the site will be retained. It completely fails to address the fact that the car park has recently (since the publication of the 2018 Masterplan) had wooden fences replaced by metal ones. One reason that the fences were installed was the use of the meadows by drug couriers using off road motorcycles and quad bikes which the beck valley was plagued with for years because of poor maintenance of fencing at Mandale car park. That lack of maintenance was taken advantage of by travellers in August 2019, which caused distress to residents and other users of the meadows and expense to the council to fund the clearing up of the resultant mess. Those problems have subsequently been overcome by the erection of the metal fences.</p> <p>It would be a mistake to think that the problems have gone away though as a motor cyclist was witnessed trying to gain access as recently as 2 November 2020, having come from the north along the path through the field between the A19 and Downside Road. Building a road which exits Mandale Meadow at the Levick Crescent roundabout would enable further such incursions, attendant expense and damage to the new country park. No account appears to have been taken of these factors in proposing that the road be built from the site of the current car park.</p> <p>Page 19 claims that “Although there are the occasional mature trees much of the woodland is relatively new”. Whilst the woodland bordering Mandale Meadow may not be hundreds of years old it is many decades old so it is totally misleading to describe it in any way as “new”, whether relatively or not. What is certain is that it would take many years for new planting to reach the same level of maturity were the existing trees to be destroyed by driving a road through the existing woodland.</p> <p>Page 19 claims that the proposals will mitigate noise pollution. No provable evidence to support that claim is provided. Real life experience shows that the same sorts of measures along the A19 both north and south of Mandale Road do not prevent road noise disturbance to properties in Downside Road, Heythrop Drive and surrounding roads.</p> <p>Page 19 states that “Two large sewer pipes pass under the site and no development can be built within their easements.”. Note that the statement “no development” is not qualified and building of a road is obviously development. It is admitted on the same page that “Easement around sewer pipe throughout the site will restrict certain uses and landscaping approaches.”. The revised route of the road as shown on Page 12 would involve it being built over both pipes, obviously within their easements and on ground which becomes saturated and marshy following the regular periods of prolonged wet weather that occur.</p> <p>Page 20 states, without qualification, “Existing woodland will be retained and enhanced”. That commitment can not be achieved when the proposed route of the road will destroy mature trees at the north of the site.</p> <p>Page 20 states “Bluebell Beck however has the potential to deliver significant environmental benefits in the form a of a wide range of habitats</p>

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				<p>including woodland, wetlands, grassland and riparian habitats and has a high priority." Bluebell Beck already incorporates such habitats. The destruction of the existing habitats hardly enhances the potential to deliver significant environmental benefits.</p> <p>The note on species on page 21 mentions a mere fraction of the wildlife species which currently inhabit the publicly owned land. It is obvious that insufficient work has been done within the exercise undertaken by Gradon Architecture and Valley Environmental to establish a complete and accurate inventory of existing wildlife species (as opposed to those which just happen to have been recorded on the DEFRA MAGIC website)?</p> <p>Page 24 claims that the plan "highlights potential conflicts and opportunities to be resolved by further design.". The various points above illustrate that the highlighting is lacking in many respects (including, apparently, the initial specification given to Gradon Architecture and Valley Environmental) and can not be relied on as a solid foundation for further design without significant revision to remedy its shortcomings.</p>

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Joanne Mitchell				<p>The A19 has already been widened at some cost in terms of physical and mental health and the general well being of residents. Issues exist which require resolution regarding noise and air pollution and ought not to be made worse still by this imposition.</p> <p>The road would destroy the meadow.</p> <p>These sad plans they have to ruin one of the most beautiful parts of Middlesbrough not to mention the habitat of wildlife that will disappear without the meadow. Greenspaces are very precious and need to be kept as they are. Years of conservation and preservation work has been invested. People moved close to the meadow being given to understand it is protected, a greenbelt where no building or any development may ever take place. It may never have been a designated greenbelt but it was the subject of a Deed of Covenant, the land having been gifted to the people of Middlesbrough in perpetuity. A road is being planned through the meadow to service a housing estate.</p> <p>At the end of the day destroying a pristine meadow which is rich in biodiversity and has never been 'treated' with pesticides and herbicides, is nothing short of ecocide and vandalism and makes a laughing stock of the council's One Planet Living and green strategies.</p> <p>There was always another option, the 'developers' could create an estate loop road similar to the one at Trimdon.</p> <p>The council has stated they won't even pay local residents a penny in compensation for taking our public amenity from us and subjecting us to increased air and noise pollution, damaging our health.</p> <p>the disruption, destruction, the irreparable damage, and the blatant disregard for people who have lived here and who have been involved during many years in the care and preservation, and who will have their lives destroyed, just beggars belief. Residents already suffer the constant excessive traffic noise...and the pollution inflicted upon them. To think that there are those with the power who lack the empathy and the understanding, or thought and consideration for people who use the meadow and those who live close by is unbelievable.</p> <p>The residents in the Ayresome ward would no longer be able to access Mandale meadow from Mandale Road. It will be impossible for people in the Ayresome ward, including those of us who live a 2 minute walk from the current entrance to Mandale meadow is because it would involve walking straight across two lanes of the proposed roundabout. The road on OUR meadow stopping us accessing OUR meadow is the issue. All this nonsense about "you are getting extra greenspace" is an insult when you are planning something which prevents us from accessing OUR green space.</p> <p>The remarkable thing about this is some of the language used in the document to describe the proposed plan: "To create a diverse ecosystem" etc when one already exists that will be destroyed by the building of so many houses. The plan to create this "diverse ecosystem" on land which has been polluted by herbicides, pesticides and pollution from the building process while destroying a pristine meadow which is genuinely a diverse ecosystem containing Red and Orange endangered species marked on this map of Mandale meadow. They know this because the map and the legend were included in the Greenstuff response to the 2018 LP.</p> <p>The area is home to rare species flora and fauna, which have taken decades to fully establish, all of this would be destroyed.</p> <p>The faux manufactured country park, cannot, and will never, as if by magic, replace this.</p> <p>Mandale Meadow is well used and loved by many, an accessible place in which we are able to walk and relax.</p> <p>Residents currently suffer excessive noise and air pollution levels 24/7 owing to the proximity of the A19...how can it possibly be considered in any way acceptable to bring yet another road even closer to homes.</p> <p>When the A19 was widened residents received compensation for the loss in value of their homes..but despite the proposed road being routed even closer we are told we would not receive any such award for all of our losses. ..the loss of peace, and of mental and physical health. .. These of course can never be compensated for. It is not what we want.</p> <p>The notice delivered to each resident shows a basic outline of the area concerned, part of which is Mandale Meadow at the top of the 'plan'. The notice gives the impression that the entire area will be Country Park, along with which would come 1700 houses, not forgetting the so called Relief Road, to 'relieve' congestion on the A19 and serve the 1700 new homes. The plan actually shows the road through Mandale Meadow which would discharge traffic onto Mandale Road, at a junction adjacent to residents homes where the current car park is now situated, actually at the foot of their gardens.</p>
John David Duffy				<p>I am no expert to really consider this section, but from what I can see it looks to cover all the relevant areas to pull this type of document.</p> <p>If I am correct the level of surveys the facebook groups are asking for will be provided when the developers look to submit planning applications. i.e. air and noise surveys etc.</p> <p>I have no further comments and will look to ensure I read future survey information.</p>
Judith Harbron				No development

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Judith Maunder	Friends of Mandale, Greenstuff, resident very local to Mandale Meadow			<p>p15,2.6 site access. The road for the new estate can be a loop road, it does not need to exit at Mandale meadow and the residential roundabout on Mandale road which already suffers from heavy traffic congestion during rush hour so is not a suitable access/exit point for the new estate.</p> <p>2.8 and 2.9 ALL the trees along the Blue Bell Beck should be retained a) for aesthetic purposes, b) because they give out oxygen and their complex root systems contain fungi which aids the growth of other flora thereby benefiting the fauna, c) chopping them down releases all their stored carbon, d) because it will take another 30 to 50 years for saplings to become viable storers of carbon and emit oxygen, e) because mature and semi mature trees take up gallons of water per day per tree. Axing them will increase flooding in the Blue Bell Beck Valley</p> <p>p19, 2.5 flood risk. Building on farm land means less land for rain water to soak away and it runs downhill on the gravel path and towards Mandale meadow which is frequently so water logged and boggy with lying water building a road will cause more flooding and SuDs have proved to be ineffective and death traps for wildlife when they dry up. Diverting any of the water into the beck is not viable as it will cause serious flood issues.</p> <p>p19, 2.8 although the existing Mandale woodland is described as relatively young it has taken 30 to 40 years to achieve the semi mature status it has. This cannot be replicated by introducing 30cm saplings, many of which will die and most will be subjected to vandalism as has already been witnessed. The woodland on Mandale needs to remain intact. It is a rich habitat and provides the unique requirements of the rare bee orchid as well as two banks on which the Red listed dingy Skipper and Brown Argus can be found as well as a range of other orchid species. Birds of prey rely on the rich habitat Mandale provides and buzzards, kestrels, sparrowhawks, red kites and goshawks have all been seen on Mandale. The fact that a wildlife survey has not yet been carried out means MBC are quite happy to commit ecocide as they would rather kill wildlife and rare habitats in order to procure the maximum amount of council tax.</p> <p>2.9 EASEMENT the current woodland on Mandale and Farley copse do not ease traffic noise pollution at all. It is a constant nuisance to residents as it is, building a road past people's back gardens will increase the traffic noise pollution so considerably that it will be intolerable.</p> <p>p20, Mandale meadow is already richly biodiverse so it doesn't have the 'potential' to form a wide range of habitats, it already has them and if officers bothered to visit the site and accept a knowledgeable local to guide them round they would see that what they have said is utter poppycock. MBC, the Mayor and Deputy Mayor need to understand you can't create a biodiverse habitat overnight - it takes decades.</p> <p>p21, the use of desktop software clearly doesn't provide any knowledge of the flora and fauna in the area. Any local resident could tell you about the flocks of lapwings they see flying over Mandale meadow. There are bats, tree sparrows, water voles, hedgehogs, deer, a wide range of dragons and damsels hunting in the Mandale trees and a long list of Red and orange listed endangered species which you are happy to wipe out. The greenwashing makes me sick, it does not draw attention away from you committing ecocide.</p> <p>p23, 2.15 The map shows drainage flowing towards Mandale, this will be considerably worsened and the idea of digging SuDs on a pristine meadow is appalling.</p>
Julia Povey				We can't control what happens on privately owned land but there should be NO ROAD THROUGH MANDALE MEADOW/WOODS.
Julie Riley				NO ROAD THROUGH MANDALE MEADOW
Kabaser Ajaib	Retailing	Kabaser Ajaib	Retailing	No through road
Keith Russell				No road through Mandale Meadow
Keith Young				No road through Mandale, you can do what you like on the private land

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Lauren Teate				<p>Page 16, Site analysis - Site Views: 2.4 - The area of land seen from the carpark is NOT flat, mown grass but a vibrant wildflower meadow. This picture was deliberately taken after the meadow was harvested and this description is a downright lie.</p> <p>Page 18, Chapter 2.8. The trees there at least 25/30 years old and are not "relatively new". It will take another 25/30 years to replace the trees that are felled to make way for the busy road. Planting trees does not mitigate the felling of decades old trees. There are also a lot more than "the occasional" mature tree. This is hardly an accurate depiction of the site.</p> <p>Page 19 -2.9 - "The most significant issues regarding air quality is anticipated to be traffic noise from the surrounding roads. The A19 & A174 both have high volumes of traffic." Air quality and noise pollution are completely different problems. Is this a mistake? Air quality directly effects our physical health. Is nothing being done to prevent further air pollution? Bearing in mind that this estate is designed to attract young families with young children and is currently already surrounded by very busy roads. The noise pollution from the A19 and A174 is already extremely high in the estate, not to mention the existing estates and not enough is being done to mitigate this. Have any studies been on the area to measure air quality and noise pollution? I believe Middlesbrough has a significant problem with air quality that has simply been ignored by the council. https://www.itv.com/news/tyne-tees/2019-12-05/middlesbrough-s-air-pollution-worst-in-the-north-east-and-equivalent-to-smoking-90-cigarettes-a-year</p> <p>2.11 page 20 - Fig 4- Why have the wildflower meadows not been mentioned in this section? These are more significant than most of the other habitats that have been described.</p> <p>"Fig 5: Biodiversity. Fig 5 helps to highlight the relatively low site biodiversity and reflects the sites use as intensive arable farmland. Bluebell Beck however has the potential to deliver significant environmental benefits in the form a of a wide range of habitats including woodland, wetlands, grassland and riparian habitats and has a high priority." - Bluebell beck and Mandale meadow have an extremely high level of biodiversity and this site is significant according to the Butterfly Conservation Society for rare species of butterflies and population density.</p> <p>Page 21 - "Fig 8: Species Notable species identified within MAGIC include Lapwing and Tree Sparrow to the north of the site as well as Grassland assemblages, Farmland Birds and potentially Redshank. Bat species are expected within Bluebell Beck and are likely to use this area as well as linear hedge features for foraging." - Why has nothing been mentioned of the butterflies, especially the Dingy Skipper that live and breed on this site? Dingy Skippers are red listed species and the proposed road will flatten their only breeding site in the area. The Butterfly Conservation Society are aware of this site. There is also no mention of the water voles that use Bluebell Beck are one of the species very sensitive to pollution. The Teesside Wildlife Trust are aware of this population of water voles, which have already been significantly effected by the temporary road that was put in for the building of the new Acklam Grange School site.</p>
Leah Davey				<p>This housing allocation is not necessary and should stop.</p> <p>Any 'country park elements' should remain within the site already under construction and NOT damage the area on Mandale Meadow, Newfield Meadow or the connecting green spaces by the addition of landscaping features, footpaths, cycle paths, play equipment or 'desire lines'.</p>
Liam Harris				Please see sheet 1

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Linda Barker				<p>This housing allocation is not necessary and should stop.</p> <p>Any 'country park elements' should remain within the site already under construction and NOT damage the area on Mandale Meadow, Newfield Meadow or the connecting green spaces by the addition of landscaping features, footpaths, cycle paths, play equipment or 'desire lines'.</p> <p>This site allocation will cause SIGNIFICANT detrimental impacts to the existing wildlife site at this location.</p>
Lynn Blagg				<p>Leave site as it is to enable residents and others to use it in its natural state for exercise and pleasure and avoid destruction of wildlife habitats and ageing flora and fauna and avoid excess traffic in the area.</p>
M. E. Bailey				<p>*p15, 2.3 Farmer Pearson should not be forced into selling the farm he loves through compulsory purchase, it's just plain wrong and the views over his agricultural land enhance the green wedge which we have.</p> <p>*p16, 2.4. it is deliberately misleading to refer to Mandale meadow as "a mown field". It is a meadow and the photo (view 1) was taken at hay harvesting.</p> <p>*p18, 2.5. Flood risk is raised at the lower reaches of the beck even more as agricultural land is built on.</p> <p>*p18, . 2.6 For the reasons mentioned in the response to the Introduction there is no necessity for a road to go through Mandale meadow and exit onto the A1130, a loop road for the estate is entirely sufficient due to changes in lifestyle since the covid pandemic..</p> <p>*p18, 2.7, some of the undocumented PROWs through Mandale meadow are highly prized by residents precisely because they are narrow, not surfaced and bring you closer to nature with good views of a variety of insects, one is so enchanting it is named 'the fairy path' by residents. This path cannot be seen on aerial maps. There is a gravel path from Mandale to Newfield which adequately serves the local community. Any path building on the existing open public space will degrade the site.</p> <p>*p19, 2.8. The ribbon of trees starting from Mandale may only be semi mature but it has taken 30 to 40 years for them to achieve that growth. Felling any of them is unacceptable for a number of reasons, a) you can't replace semi mature trees with small whips (many of which will die) as they cannot soak up water, absorb carbon, emit oxygen or provide easement for noise and air pollution from the A19.</p> <p>*19,2.9 The trees which provide easement on the Northern sections, i.e. Farley Copse and Mandale woodland don't ease the traffic noise as it is and residents already find it too intrusive. Building a road through Mandale meadow will massively increase the noise pollution and even more so if you fell the trees which provide a small degree of easement. A road even closer to residents properties will make life intolerable for them.</p> <p>*p19, 2.10 Mandale meadow should be awarded Conservation status because of the endangered species on it.</p> <p style="text-align: center;">p3</p> <p>*p20, 2.11, Fig 2, It isn't true that most of the existing woodland would remain if a road is ploughed through Mandale meadow as much of it would be lost.</p> <p style="padding-left: 40px;">Fig 5, it is totally untrue to say there is low biodiversity on the northern section as Mandale meadow is the most richly biodiverse are of the whole of the Bluebell beck valley as can be seen from the greenstuff response to the 2018 consultation.</p> <p style="padding-left: 40px;">Fig 8, this proves how inadequate a desktop survey is. It is only showing tree sparrow and lapwing on Mandale meadow. Please see the wildlife maps and keys on Greenstuff file section.</p>
Malcolm Chilvers				<p>Like many people I have neither the time nor inclination to be able to read and absorb the information contained in all these chapters unlike yourselves who are being paid to do it.</p>

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Margaret Elliot				<p>I object to the site having access from the north due to the following reasons.</p> <ol style="list-style-type: none"> 1. Traffic congestion. It is already very difficult to emerge from Heythrop Drive on to Mandale Road during peak periods due to severe congestion. When returning to the Acklam area from the north on the A19, the slip road also gets very congested to the point it slows / stops traffic on its running lanes. Much better infrastructure is already desperately needed and simply won't cope with the immense increase of traffic that will come if a spine road is built. 2. Air pollution 3. Noise pollution. 4. Adverse effects on wildlife / increased roadkill.
Margaret Jackson				<p>Page 2 The grass may be mown in the photograph. This is done once a year when the meadow grass is mown for hay. It is an ancient natural meadow, not farmland which has been sown with grass-seed for a crop. These meadow lands are becoming rare but it is now recognised that they provide a habitat for a wide range of wild-life. This is not compatible with have a road built through the middle. Having read the description of the current wild space it obviously downplays the diversity of wildlife to be found and implies that this will be improved by the development. This gives a misleading impression for anyone who hasn't actually visited the site.</p>
Matthew Shippey				<p>Don't want this. I walk in mandale meadow. Middlesbrough is lacking in green space and you want to build a road through to facilitate new housing. Middlesbrough Council you are a disgrace.</p>
Michael Starford				<p>2.9 (air quality) natural screening of roads absorbs some dust and fuel pollution. Though in the lifetime of a new road we expect non-polluting vehicles to be introduced as part of a government green policy.</p>
Michelle Hedger				<p>The loop road option. No road through Mandale Meadow. Use the loop road option. If a road needs to be there make it a narrower road of 20mph. My children cannot play near the huge 30mph road planned. They can play now in the meadow. They will be hit by a car in this 'country park'.</p>
Mike Conyard				<p>Do not build on this land. This is much needed green space. There are no parks in this area and nowhere for people to walk, kids to play and people to live their lives. I find it disgusting that you propose to build on this beautiful piece of land. Also, who wants to live right next to the A19? These house will be worth literally nothing.. The land is also a buffer zone from the A19 to the residents of Heythrop Drive and beyond.</p>
Miller Homes and Avant Homes	Miller Homes and Avant Homes	Mr Neil Morton	Savills	<p>We have reviewed the site analysis section and this appears to be largely factual and predominantly based upon desk-top analysis. As part of a planning application for the remaining southern ownership land, our clients has instructed a full suite of technical and environmental assessments which will in turn inform the application proposals. This will provide additional layers of detail in terms of the understanding of the site and its constraints and opportunities to inform the design process. Clearly this process could have the potential to result in different design solutions and principles to those set out in the masterplan, however these matters, should they arise, can be examined as part of the consideration of the planning application process.</p>

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Nichola Davey				This housing allocation is not necessary and should stop. Any 'country park elements' should remain within the site already under construction and NOT damage the area on Mandale Meadow, Newfield Meadow or the connecting green spaces by the addition of landscaping features, footpaths, cycle paths, play equipment or 'desire lines'.
Nicola Smith				We DO NOT WANT IT!!!! WHY can't you build on areas where you have flatted hundreds of houses but not rebuilt? Keith Road or Marton Burn Road??? This is the only green space we have in Acklam and hundereds of residents rely on this being walking distance. Pre covid the traffic on mandale Road, Acklam Rd and levick Crescent is bad enough at peak times without further traffic. You have Acklam Grange Secondary school round the corner from the meadow with 1200+ students that also use these roads. This is already a very busy junction without diverting more traffic to it. What about adults and children's mental health?? Has the past year taught you nothing?? I think having a open greenspace with all the wildlife and plenty of room to exercise and get much needed fresh air and been an absolute saviour to hundereds of local residents. This project has not considered anything but money. Looking at the plans there will be very little green left and it will be surrounding a road and housing estate. Absolutely nothing like we have now. This is NOT ACCEPTABLE FROM ANYONE I KNOW WHO LIVES IN ACKLAM 😞😞😞😞😞😞😞😞
Peter Davey				This housing allocation is not necessary and should stop. Any 'country park elements' should remain within the site already under construction and NOT damage the area on Mandale Meadow, Newfield Meadow or the connecting green spaces by the addition of landscaping features, footpaths, cycle paths, play equipment or 'desire lines'.
Poppy Bowyer				How was the number of houses being developed decided? What was the aim and objectives of the build that meant this amount of houses was decided upon? The areas chosen for housing and recreational buildings is old arable farm land, this is not good for building on, has this been considered as a possible limitation here? What is being put in place for future flood mitigation or subsidence? And is there any worries concerning costs of erosion, compaction or long term damage to the landscape and habitats? In an already highly populated area, how will the number of new houses, people and cars, impact the traffic issues already faced here? What ideas are behind the retentions and new planting schemes? Is there a formal plan? What plans will be put into place to ensure as little disturbance as possible befalls the population of wildlife already found on the site i.e. the Lapwings, Redshanks and Bat species and habitats etc? Some of these species need large open spaces to nest and hunt, which means with so much space taken up these species may be lost from this site. Has this been a consideration at all?
Rob Atkin				No road through Mandale.

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Robert Teate				<p>2.15 Topography and Flooding & Surface Water. I understand that the flood zone was changed from non development to development in recent years, when the temp haul road was built to gain access to the rebuilding of Acklam Grange school the Mandale Meadow had a huge plastic pipe buried underground to aid the removal of surface water. The pipe was never removed.</p> <p>In view of climate change we should not be building huge estates but rewilding areas such as Stainsby area.</p> <p>fig 5: Biodiversity. You name two or three bird species such as Tree Sparrow and Lapwing but the planned building will impact on hundreds of birds and mammals, building a so called highly managed country park will no more ease pressure on the wild life than the farm land it in planned to be built on. There are many rare and endangered species in the Blue Bell Beck area and adding more bricks and concrete will have a very negative effect.</p>
Scott Elliot				<p>Site access. I have been objecting to the spine road since 2014 on the following grounds.</p> <ol style="list-style-type: none"> 1. Traffic congestion. Traffic already backs up as far as Acklam Road on both Mandale Road and Levick Crescent at certain times of the day. During these times it is extremely difficult to emerge from either end of Heythrope Drive. Also, when returning to the Acklam area from A19 north, the slip road gets backed up on to the running lanes of the A19. This will only get worse if the spine road is built, effectively closing at least one lane of the A19 during busy periods. Much better infrastructure needs to be in place first. 2. Air pollution. 3. Noise pollution 4. Adverse effects on wildlife.
Sean Marshall				Grate location
Sofia Torre				How do you sleep at night when you see the views that you will be destroying those green areas especially for those who have lived for years you will destroy their views and put up buildings that will destroy this beautiful area, how greedy and thought less you are
Sonia Bowler				<p>The issue with a potential flood zone along the course of Blue Bell Beck, particularly near the A19 mentioned on Page 18 and the issue with the 2 large sewer pipes underneath the site restricting building mentioned on page 19 suggest to me that a more preferential layout would be a loop road that avoids that area, does not cross Mandale Meadow, does not interfere with the existing green space owned by Middlesbrough Council and does not come out at the already badly congested small roundabout on the A1130!</p> <p>No road through Mandale Meadow!</p>
Stephanie Atkin				<p>No road through Mandale Meadow.</p> <p>I disagree with the analysis of this site as a 'field of mown grass'. Who's purposes are served by describing a wildflower meadow in such a way? I wonder...</p>
Stephen Connors				Thoughts previously mentioned.

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Steven Brown				<p>2.7 Rights of Way & Connections</p> <p>There is one Public Right of Way to the South of the site. The route is along the site boundary next to the A19 and A174. The PROW crosses the A19 and A174 on single track road bridges. The crossing point of the A19 and A174 are important connections to retain as there are very limited crossing points on these roads for pedestrians, cycles or horses.</p> <p>Within the council owned Public Open Space land there are a number of footpaths that are well used by the local community, although they are not formally recognised as PROWs. Some of these routes are permanent features with tarmac paths, other are more informal routes across the public open spaces that have been created by people using the space</p> <p>How will this affect the residents of Stainsby Hall Farm. The residents of this area have paid for the road upkeep I personally have lived here since the onset of Stainsby Hall Farm development by Mandale properties and have never had with anti social disturbance. I believe that with this development the situation will change. What are the plans for access to and from the development with respect to the occupants of Stainsby Hall Farm and their privacy.</p> <p>2.13 Views from Public Rights of Way</p> <p>There is a high point to the south of the site, on the bridge that the footpath crosses, that provides good views across the site; To the North across Stockton and Middlesbrough and distant views East toward the Cleveland Hills and Roseberry Topping. Before the Miller properties where built I had the views as stated but have now been taken away by the development</p>
stewart jones				Mandale has not been fully analysed in fact no land assessment has taken place and no SA carried out apart from that super

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Thomas Maunder				<p>*p15, 2.3 Farmer Pearson should not be forced into selling the farm he loves through compulsory purchase, it's just plain wrong and the views over his agricultural land enhance the green wedge which we have.</p> <p>*p16, 2.4. It is deliberately misleading to refer to Mandale Meadow as "a mown field". It is a meadow and the photo (view 1) was taken at hay harvesting.</p> <p>*p18, 2.5. Flood risk is raised at the lower reaches of the beck even more as agricultural land is built on.</p> <p>*p18, . 2.6 For the reasons mentioned in the response to the Introduction there is no necessity for a road to go through Mandale meadow and exit onto the A1130, a loop road for the estate is entirely sufficient due to changes in lifestyle since the covid pandemic..</p> <p>*p18, 2.7, some of the undocumented PROWs through Mandale meadow are highly prized by residents precisely because they are narrow, not surfaced and bring you closer to nature with good views of a variety of insects, one is so enchanting it is named 'the fairy path' by residents. This path cannot be seen on aerial maps. There is a gravel path from Mandale to Newfield which adequately serves the local community. Any path building on the existing open public space will degrade the site.</p> <p>*p19, 2.8. The ribbon of trees starting from Mandale may only be semi mature but it has taken 30 to 40 years for them to achieve that growth. Felling any of them is unacceptable for a number of reasons, a) you can't replace semi mature trees with small whips (many of which will die) as they cannot soak up water, absorb carbon, emit oxygen or provide easement for noise and air pollution from the A19.</p> <p>*19,2.9 The trees which provide easement on the Northern sections, i.e. Farley Copse and Mandale woodland don't ease the traffic noise as it is and residents already find it too intrusive. Building a road through Mandale meadow will massively increase the noise pollution and even more so if you fell the trees</p> <p>which provide a small degree of easement. A road even closer to residents properties will make life intolerable for them.</p> <p>*p19, 2.10 Mandale meadow should be awarded Conservation status because of the endangered species on it.</p> <p>*p20, 2.11, Fig 2, It isn't true that most of the existing woodland would remain if a road is ploughed through Mandale meadow as much of it would be lost.</p> <p>Fig 5, it is totally untrue to say there is low biodiversity on the northern section as Mandale meadow is the most richly biodiverse are of the whole of the Bluebell beck valley as can be seen from the greenstuff response to the 2018 consultation.</p> <p>Fig 8, this proves how inadequate a desktop survey is. It is only showing tree sparrow and lapwing on Mandale meadow. Please see the wildlife maps and keys on Greenstuff file section.</p>
Trevor King				We can't control what happens on privately owned land but we want NO ROAD THROUGH MANDALE MEADOW/WOODS.
Val Young				No road through the meadow
Dave Blackburn				I said to the wife, look at these beautiful fields in the paperwork, why do they think they can do better than this. We were talking about compensation, and she said 50k would not make me give up that field. We have lived here is 89 and all our family have used it. Children and dog spilling out of the back gate and having hours of fun.
Gill Sullivan				There are some stunning pics of Mandale and Farmers Fields. Beautiful. But my question is why would you want to destroy that natural beauty. Mr Blackburn (Resident) complains that his and our rights to roam freely as he has done since 1989 will not curtailed by MBC plans. The plans make much of the new residents views but does not help the current resident. As theirs will be blighted by a large road carrying around 15k vehicles if the developers get their way.

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				<p>It would appear to your current residents that their rights haven't the same weight as your new ones. so MBC please don't forget that you have a duty of care to your current population. A legal obligation too. Always act in the best interest of individuals and others. Not act or fail to act in a way that results in harm. If this road is pushed through mandale field, you will be doing direct harm both to residents and the ECO system of the beck. To be felt for years to come.</p> <p>It will give a message loud and clear to developers come to MBC and don't worry about green spaces or covenants. Just take what you want build what you want we don't take residents wants and needs into account. Only yours and your money carry any weight. Your residents gave you a clear message in 2018, 900 responses to no housing on the field and no spine road on open green space.</p>

Question 4 – Please provide any comments you may have on Chapter 3.0 Design Development

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Name	Organisation (where applicable)	Agent	Organisation	Please provide any comments you may have on Chapter 3.0 Design Development
Alexander Henegan				It is surprising that junction capacity studies have not been carried out considering the volume of vehicles that will be using the junctions. What are referred to as “roundabout situations” from development areas to the Primary Road will be extremely busy. It is most encouraging that 53% of the total site will be retained as green space.
Aiden Woodward				I think the layout of the primary and secondary roads are sensible, and like that some green space is being kept between clusters of housing. The central hub is in a good location, I think it should be considered however that needs for at least the shopping part of it is likely needed before all houses are done. The timeline is vague, but there are currently minimal shops in a convenient walking distance from those further into the existing development.
Aji Varghese		Aji Varghese		Good
Alan Liddle	Stainton and Thornton Parish Council			Once again, there appears to be no connectivity to existing footpaths and cycle-ways to the south of the development.
Amanda Stevenson				3.1 states that the design of the primary road has emerged following three consecutive rounds of community consultation - it does not say what the outcomes of community consultations actually were, are people in favour of or opposed to the new road? 3.1 also states that the design of the road is subject to junction capacity studies yet to be undertaken and will need to be developed in conjunction with detailed applications - what does this mean? Is there a possibility that the road could be built on even more of Mandale Meadow than has been indicated in the Masterplan.
Amy Lord				Section 3.1 states a junction capacity study has not been carried out. This would seem a massive oversight. I regularly commute from Trimdon Avenue along Mandale Road onto the A19. It will often take 20 minutes to complete this section of the journey at peak times, with Mandale roundabout being a particular bottleneck. Adding another major junction there will have a knock on effect throughout Acklam, causing traffic to back up, increasing journey times and pollution. I would hope that any study will be carried out to consider normal traffic volumes and not the reduced volumes we have seen during the pandemic. Inevitably, the new spine road will see large amounts of stationery traffic as people wait to get out onto the roundabout, leading to pollution in the meadow and surrounding homes.
Andrea Spooner				Design development - currently natural habitat but trying to sell us a commercialised plan which will not be maintained similar to the Trimdon avenue play park which is virtually unusable due to lack of maintenance and surveillance. this is exactly what will happen here, youths from Whinney banks and surrounding areas will use it as a hangout with anti social behaviour to follow leaving it as an estate that nobody will want to live in, a country park that becomes another go area in the town and the local habitat destroyed. Plans look very idyllic but get real and learn from the past instead of blundering into yet another disaster. Is there going to be security? Know your area, listen to your residents and value their opinions instead of getting. Don't get me wrong I would love a country park on my doorstep but not one that is basically a housing project and commercialised. Build a proper country park with gates that close on night with park rangers, create jobs and memories not more cheap housing.

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angela barber				<p>AS a resident of this area, I fear my house value will lower so close to this new road.</p> <p>There will be No where to walk my dog</p> <p>There would be Little or no space for children to play safety in area</p> <p>An area to exercise my self and enjoy picnics with the family</p> <p>Local wild life will loose there home and endangered species plants and animals will loose there environment</p> <p>Why should be suffer ill health because of further pollution</p> <p>We pay council tax for green space- will our council tax lower-expect not</p> <p>Increased noise during the day and night- lack of sleep and depression result</p> <p>All you want to do is provide new access road to the estate- why cannot they come around down downside and Acklam road instead as always has happened. I feel the new road will increase traffic into Middlesbrough through our area.</p> <p>I do not want or need the road as I do not drive- I doubt it will be included on the bus route, so it does me no good.</p> <p>In fact everything about it is BAD</p>
Angela O'Byrne				Hands off our meadow!
Anthony O'Malley				It seems a shame that the current planting plan (especially the tree lined roads) appears to omit the current "phase 1" development, and will only start at the future building locations. It would better 'tie' the area together if this was implemented from the start of the current beck crossing where adequate space has been left for it.
Barbara Keville				<p>3.1, 3.4, 3.5, and 3.6.</p> <p>Building roads whether primary or secondary and making formal cycle tracks and footpaths is only going to destroy the eco system that already exists.</p>
Barry pinkney				Country park no
Barry, Ann and Craig Pearson	Stainsby Hill Farm	Michael Mealing	Michael Mealing Planning	<p>A particular constraint that should have been included in the evolution of the Masterplan is the provision of an independent access for farm vehicles and machinery from the Stainsby Hill Farm farmstead to the A19 bridge to the south. This must be provided to facilitate access from the farm complex to my clients' land located to the west of the A19 and to the south of the Stainsby Medieval Village and Open Field System Scheduled Ancient Monument.</p> <p>The need to accommodate a route for such access is included in Policy 21(m) of the Housing Local Plan – "maintain an access route for farm vehicles from the farmstead at Stainsby Hill Farm to farmland south of Stainsby Grange Equestrian Centre."</p> <p>Provision was made in the earlier Masterplan but seems to have been omitted from the current draft of the Stainsby Country Park and Masterplan. This omission should be remedied by reserving land between the proposed Country Park and the A19 for the necessary track. This will naturally have implications for the land-take of the Country Park.</p>

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Barry, Ann and Craig Pearson		Michael Mealing	Michael Mealing (Planning)	<p>A particular constraint that should have been included in the evolution of the Masterplan is the provision of an independent access for farm vehicles and machinery from the Stainsby Hill Farm farmstead to the A19 bridge to the south. This must be provided to facilitate access from the farm complex to my clients' land located to the west of the A19 and to the south of the Stainsby Medieval Village and Open Field System Scheduled Ancient Monument.</p> <p>The need to accommodate a route for such access is included in Policy 21(m) of the Housing Local Plan – “maintain an access route for farm vehicles from the farmstead at Stainsby Hill Farm to farmland south of Stainsby Grange Equestrian Centre.”</p> <p>Provision was made in the earlier Masterplan but seems to have been omitted from the current draft of the Stainsby Country Park and Masterplan. The necessary track will naturally have implications for the land-take of the Country Park.</p>
Brad Raistrick				Through road and bus route are the worst plans for the estate! Taxi / general cut through traffic is going to ruin a nice quiet estate!
Carol Newmarch				I object to the whole plan
Carolyn Dodds				Will Planning and Development Department scrutinise each section of the development to ensure the developers keep to plan.
Catherine Howell (Boyle)				<p>3.1 The road is considered essential to provide adequate transport capacities to service the allocated development site. If the site is not developed, the road is not required. If the site is developed, the most appropriate option is to loop the road through the housing development. Creating a spine road will divert massive amounts of traffic across an existing green space to avoid congestion on the A19/A174.</p> <p>3.5 Strategic cycle route - This will not drive local connectivity as the cycle network beyond the development is not safe, well planned or well maintained.</p> <p>3.8 Again, 53% green space is equivalent to a loss of 47% current green space.</p>
Charlotte McAdam				3.6 - page 30 - the number of paths proposed on Mandale Meadow is unnecessary and would make the area much less green, and take away the country element of this "country park". There are already well-worn paths in the Meadow, and so adding permanent, fixed concrete paths is entirely unnecessary.
Chris King				<p>The design of the road is subject to junction capacity studies yet to be undertaken and will need to be developed in conjunction with detailed applications.'</p> <p>May I respectfully suggest this such as study is vital to the whole development. Clearly we will need to await post-Covid levels of traffic before such a study is undertaken.</p>
Christine King				We can't control what happens on privately owned land but we want NO ROAD THROUGH MANDALE MEADOW/WOODS.

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Christopher Bieda				<p>In the current version of the plan, specific mention is given to the fact that the main through road will not have properties facing the road without parking or green boundary in between. As per the below Photo 6770, you can clearly see that this is not accurate and that a number of houses face directly onto the road.</p> <p>In light of the above, and also as mentioned in the current plan, On street parking would not feature. As per photo 6771 you can clearly see this is false and clearly needs to be considered.</p> <p>In light of the above the width of the road would be limited due to vehicles parked at the housing.</p> <p>The current plan states the location of the proposed Roundabouts that will help control vehicle speeds. My concern is that in relation to the the Low Lane roundabout, the distance is too far to the next proposed roundabout (over 1.5miles) and this will encourage road users to speed at dangerous levels in a highly residential area.</p>
christopher Eddon				<p>no more houses please, in your 2014 housing plan it says that the majority of people who work in the boro commute from further afield so you might want to address that question first. are they just travelling through the boro to get to the surrounding industrial estates for work. why dont you do an employee survey and ask everyone where they are coming from and take it from there. i would like to see a mini version of a centre parks centre built there for the communty as we have no swimming baths in walking distance from where i live and this could include eco friendly offices, cafes and a community hub serving the surrounding park land and include a footpath to mandale park and ride not a road</p>
Claire Boyd				<p>We can't control what happens on privately owned land but there should be NO ROAD THROUGH MANDALE MEADOW/WOODS.</p>
Claire Di Cicco				<p>No to a road.</p>
CLAIRE KANE				<p>The pictures are misleading it will be a black tarmac road for people to speed up and down destroying the habitats and wildlife - we already have foxes owls and bats around the area that are walking and flying around our 40 + year old estate because they have had their habitats destroyed by the new houses on Jack Simon Way estate</p>
Claire Wilks				<p>No road as mentioned in above points: habitat and wildlife species loss especially through the MBC owned area of 'green open spaces and woodland'. A large area will be built over to access further into the fields. The roads and paths will will only destroy the little remaining woodland that exists.</p> <p>3.6 Paths take away the natural element of the area. They create a constraint and less opportunity for discovery and freedom.</p>

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Colin and Morag Bailey				<p>*p26, 3.1. the design for the primary road system is not fixed, the spine road can be cancelled and the new estate have a loop road only. No weight can be attributed to the new LP in the planning process in accordance with the guidance set out in the NPPF. The current LP is being based on now outdated LPs which take no account of the changes in lifestyles since the covid pandemic altered life forever. The planning is still at a very early stage and can be altered. With the government's commitments to tackling climate emergency and even more recent murmurings about protecting nature I very much doubt that the council would face any legal challenge if they dropped the plan for a road through Mandale meadow, or, indeed, if they did not pursue the purchase of farmer Pearson's land. This would mean fewer houses but the council is planning in excess of 1000 more houses than needed and even the extra amount of housing could be built on landscaped brown field sites which are on the council's register.</p> <p>*p27, 3.2. SuDs and water channels need careful consideration. A large lake planted with bog plants and marginals at it's edges will reduce the need for numerous SuDs which may be less effective although the idea of some smaller SuDs with pond dipping platforms is attractive.</p> <p>*p29, 3.4. There will be traffic congestion at various points if such a large number of houses are built but it will be particularly bad on the A1130 if a road exits at the Mandale Road roundabout which is already a considerable pinch point.</p> <p>*p30, 3.5 and 3.6. and 3.7 A dedicated, separate cycle path, rather than a mixed use one is a good idea as those with hearing problems, or the many cyclists who fail to ring their bell, are a danger to pedestrians. People with mobility issues may find it difficult to move out of the way in time to avoid being seriously hurt. I think the option of one single, dedicated cycle track through Mandale is a good idea but not a 4.5m wide one. 1.8 m is more than wide enough and most cycle commuters will be cycling in the same direction mornings and early evenings. The series of multiple paths shown through the existing open public space at BB Valley should not happen, they will destroy the feeling of being in the countryside - the very thing local residents love about the Bluebell Beck Valley. No road through Mandale, just one single, narrow dedicated cycle track.</p> <p>*p 32, 3.8. The green areas around new housing clusters is an excellent idea but leave the existing open public land alone. Nor are so many houses needed in the new development. Much more of the land between housing clusters should be green to protect nature and give a real sense of being in the country rather than a sterile country park. No development should happen on Farmer Pearson's land.</p> <p>Public consultation 1: p17, 'Officers were able to tackle misinformation that the Stainton Way Western Extension would be a dual carriageway.' Greenstuff admin asked the Chief Planning Officer at a meeting held with him that he could NOT guarantee the road would not become a dual carriageway because it would be dependent on how much house building went on in Middlesbrough South and East Cleveland. Certainly such a mixed use path next to the projected spine road raises the possibility the road could be changed to a dual carriageway at some point.</p> <p>'Concept of country park overall positively received' this only related to green spaces being provided on the proposed new housing estate. While a percentage of people would welcome a cycle path on the Bluebell Beck Valley the concept of the road, buildings and general destruction of habitat on our existing open public land was not welcome. Only a loop road on the new housing estate is acceptable. NO road through Mandale meadow.</p> <p>Other negatives from the public consultation included the fact that only 5% of the housing will be designated as affordable. This is unacceptable in a deprived post industrial town. Mandale Meadow, gifted to the people of the area to enjoy for leisure "in perpetuity" should not have had it's covenant changed by MBC and certainly not without the knowledge and consent of the public it was intended for.</p> <p>* 53% of the 179 respondents did not contribute towards what facilities would be welcome within the Local Centre and stated "no housing, road or Country Park". So the majority view is being ignored again. There also over 900 written submissions to the council stating no building of any kind on Mandale meadow was acceptable. Council officers must accept they were elected to carry out the wishes of the people.</p> <p>*public consultation event no 2. 'Large percentage of people opposed a road through existing public open space and connecting at Mandale roundabout' In 2021 this view is held even more strongly as life has changed because of the Covid pandemic and is unlikely to look the same in coming years. A road through Mandale will also bring a return of the drug runners on off road bikes which residents have fought so hard over many years to stop. Only after Travellers arriving on the meadow the day before a planned free</p>

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				<p>event for families was something done - a metal fence was erected after the Travellers were seen knocking down a section of the wooden fence to gain entry.</p> <p>p10. A loop road through the new estate will be perfectly adequate, an exit point on the A1130 is not needed. The quantity of housing needs to be reduced as the LP provides 1,342 dwellings over expected needs and most of these can be delivered on brown field sites.</p> <p>p10. Flood risk will be increased by too much building on farm land which currently acts as a soakaway for rainfall.</p> <p>p11, site access - A road is not needed at Mandale to exit onto the A1130, there should be a loop road only as was promised to the first purchasers of homes at Brookfield.</p> <p>p12. PROW and informal paths. The informal paths around Mandale meadow are greatly prized by locals because of their wildness and closeness to nature giving the opportunity to see flora and fauna up close. One such prized, ungravelled path, so enchanting, is known as 'the fairy path'. It cannot be seen on aerial maps.</p> <p>p13, trees and hedges. While it is laudable and desirable to plant more hedges and trees it is ridiculous to axe the existing woodland on Mandale meadow as doing so releases all their stored carbon, as is the case if the meadow land is churned up during construction. Furthermore, the trees on Mandale have taken 30 to 40 years to reach their current height and are now capable of absorbing gallons of water each per day as well as producing oxygen. Again, a chainsaw massacre of the Mandale woodland goes entirely against the Council's declaration to become carbon neutral and it isn't possible to replace semi mature trees with 30cm whips as they won't be capable of storing carbon, emit oxygen, act as an easement for traffic air and noise pollution or soak up gallons of water.</p> <p>p14. The existing woodland at Farley Copse (the semi mature woodland which runs across the back of the gardens on Farley Road) does not provide easement of traffic noise pollution and adding another road so close to residents back gardens will increase both noise and air pollution. If the Mandale woodland is axed down that would also create an increase in traffic air and noise pollution making residents lives intolerable.</p> <p>p14 sewerage system. has it been calculated whether the system could cope with 1670+ more houses?</p> <p>p15, summary. Mandale meadow should be designated a Conservation Area or even an SSSI because of the endangered wildlife which can be found there. Doing a full and proper wildlife survey after the plans are passed is too late.</p>
Darren Middleton				<p>Also because the 100 houses on the meadow have been denied, it's as if everything is all rosy!!!</p> <p>Alot of questions but it all seems to point towards that the country park is a smoke screen to get the development pushed through?</p>

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Dave McGuire	Sport England			<p>I refer to the above document and your recent consultation with Sport England. Thank you for seeking our views on this matter.</p> <p>Sport England is a recommended consultee on major residential developments (300 or more units) and we this opportunity to seek two distinct outcomes: Firstly we seek to ensure that the local sporting infrastructure keeps pace with local housing growth by highlighting what demand the new development would generate.</p> <p>Secondly we recognise the form of new development can have a direct impact on resident's physical activity levels, and we seek amendments to try and encourage residents to be more active. This is called Active Design.</p> <p>In respect of the first matter, we have a number of planning tools to help local authorities estimate the demand for sports facilities that might arise from new developments. The Playing Pitch Calculator uses known team generation rates in the local authority area for football, cricket, rugby and hockey and, applying them to the assumed population level of the proposed development identifies how many pitches would be needed in respect of each sport on site, or what it would cost to provide those pitches off-site.</p> <p>Similarly the Sports Facility Calculator uses known use parameters for swimming pools, and sports halls and, applying them to the assumed population level of the proposed development identifies how many pools, and sports halls would be needed on site, or what it would cost to provide those facilities off-site.</p> <p>To give a worked example we have assumed the average occupancy rate for the new development to be 2.4 people per dwelling resulting in an assumed population of 3600 and run each report on this basis (both attached). It can be seen that the population of the development is not big enough to warrant on site provision as 37sq.m of waterspace is not a practical proposition. The model suggests a sports hall of 1 badminton court size, and if the main hall of the proposed school was this size (and capable of community use) this demand could be addressed within the development itself.</p> <p>The playing pitch calculator suggests the provision of particular pitch types – some are workable units such as mini and youth football pitches, and as with the sports hall example above, this could be met by provision within the proposed school if the pitches were constructed with sufficient capacity and they were accessible for managed community use. The demand generated for other pitch types is fractions of a pitch which suggests that provision would be better made by enhancing the capacity of that pitch type in the locality.</p> <p>We would suggest that you will find these figures a useful benchmark to be compared to the local sports facility network. If your data shows that the local pools, halls, AGPs, and grass pitches have capacity to absorb the demand that the new development will generate then the need for additional provision or capacity enhancements falls away. If however the development will place strains on local sports infrastructure that it does not have the capacity to absorb then we would suggest that the respective reports give an indication as to what form additional provision or capacity enhancement might take and what planning contributions might be sought from the developer to achieve this.</p> <p>Turning to the second matter, Sport England has identified ten principles which we consider are important elements in ensuring that development encourages people to be more active, and they are set out in our guidance document 'Active Design' which can be found at the following location on our website; https://sportengland-production-files.s3.eu-west-2.amazonaws.com/s3fs-public/spe003-active-design-published-october-2015-high-quality-for-web-2.pdf?uCz_r6UyApzAZlaiEVaNt69DAaOCmklQ</p> <p>If you wish to embed the relevant Active Design principles into the masterplan there is a checklist that can be used to review the draft masterplan against. It can be found at; https://sportengland-production-files.s3.eu-west-2.amazonaws.com/s3fs-public/active-design-checklist-oct-2015.pdf?az73PYXRmKYaXMfLu8BCxgXSByeiAQ1d</p>

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				I would be happy to provide further advice and guidance on the matters raised in this response, but trust you will our comments your fullest consideration.
Dennis Mccabe				This is a great and fresh way of building new homes for the town and it has plenty of green space. A lot better than any other housing development in the area
Dr Enoch Akowuah				The strategic cycling route should be extended so that its contiguous with the current development on the site and should link with the new development on strait lane and with Stainton village
Edna Reddy				""The design of the road is subject to junction capacity studies yet to be undertaken and will need to be developed in conjunction with detailed applications." This suggests the road through the meadow has not yet been properly assessed. Are we expected to accept whatever might be decided in future, after the consultation process has ended?
Emily quintt				No road necessary. Do you want people and animals to get run over?
Francine Kenny				Yes to Country Park Yes to houses yes to road

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Geoffrey Payne				Page 26 3.1 I note that congestion studies need to be undertaken, these must be carried out post lockdowns and during school term time. Option 1 on the 2085-34 ▪ Stainsby Link Road ▪ Initial Option Assessment Report 25 January 2021 ▪ Version 1.1 ▪ Issue seems the best route to prevent adding to the congestion already faced on Levick Crescent, Mandale Road and the A19. Levick Crescent and Mandale Road are also school routes for pedestrians with no controlled road crossings.
Gill Mollard				This housing allocation is not necessary and should stop. Any 'country park elements' should remain within the site already under construction and NOT damage the area on Mandale Meadow, Newfield Meadow or the connecting green spaces by the addition of landscaping features, footpaths, cycle paths, play equipment or 'desire lines'.
Glen Instone				Pg 29: Secondary road links extremely close to my property with obvious noise and emission pollution. What will be done to make this carbon neutral? Pg 30: Strategic cycle path and walkways adjacent to my property - great! I am pretty sure this will have several effects: 1) Reduced privacy 2) Reduced property value. The cycle and walkways then stop at the boundary. Am sure people will continue to walk past the Stainsby Hall properties.
Graham Hadfield				See above – various points invalidate the assumptions on which the Design Development is based.
Graham Loughborough				The country park design is an excellent idea, for the estate and wider region. Creating much needed family space.
Helen McGregor				While 53% of the current area is proposed to be retained as 'green', the road will nevertheless cut through ecosensitive woods and meadows. That is why I am asking you to consider a loop road which does not interfere with this.
Ian Etherington				Happy with the layout, but woul like to see more footway access to the existing este over the beck. Also there should be a cycle lane following the main relief road.
Ian Marron				In section 3.1 Remove the reference to a road to Mandale roundabout. Stating the primary road would end at the boundary with the Middlesbrough council boundary at Foxwood and a second link road from the proposed roundabout shown in the figure on page 27 adjacent Stainsby Hall farm or upgrading the secondary road shown on the diagram on page 29. Upgrade the figure in page 31 to show the alternative South link road.
Ian Rogers				Page 30 - The image shows the spine road, for up to 12,000 cars per day, cutting through mature meadow, destroying the habitat of protected butterflies and plants. It also shows many pathways cutting through this habitat, but who wants to spend their leisure time by a busy road? Unlike the A19 which is out of site, this road will be in full view, ruining this wonderful open space, which is part of the reason that many of us chose to live here.
Jacqueline Young				What experience and qualifications have the designers got to be able to provide this data. Please include this information on the MBC site
jamie barber				frankly, none of this should go ahead. you're planning to destroy natural green space and animal habitats that are at a premium in Middlesbrough.

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janice slater				<p>design development 3.1 primary road and sewer easement as an existing sewer easement exists within the site of the Bluebell Beck running almost North & South. This establishes an area that is undevelopable.</p> <p>A large road crossing these pipes is a development is it not??</p> <p>Should not these comments relate also to this easement of sewer pipes which will now be containing the extra sewage from the new builds.</p> <p>I request that satisfactory strategies are in place to ensure that these pipes are not load bearing nor are they in such a position that should a problem occur that they are not accessible. I would be grateful for details of such strategies and welcome any reports that ensure safety of this problem area prior to the acceptance of this plan.</p>
Jean Franklin				<p>Page 26 'Design of the road is subject to junction capacity yet to be undertaken' Already the area is gridlocked at peak times, Acklam road, Mandale Road the Middlesbrough side and Mandale road from Thornaby to the A19, the A19 and A174. Adding in An extra 12000 cars from the Stainton Way western extension is going to cause a complete stoppage with the accompanying pollution and noise.</p> <p>Page 30 Network of paths Whilst I applaud making all areas accessible to everyone, creating 3.5m compacted gravel paths throughout the areas of the current green belt will encourage cycling creating danger for pedestrians and children. There is already an ongoing issue with off road motor cycling and made paths has the potential to increase this problem. It also increases access for anti social behaviour such as fly tipping.</p>
Jennifer A Alderson				<p>Are MBC going to keep existing boundaries in place? My property borders the site to the North and would like to maintain my privacy if paths of the park are going to run alongside my property.</p>
Jenny McCullagh				<p>This road design is making me emotional to look at. It is going to make the lives miserable of the thousands of people who live along here. At every stage of this planning more and more houses in this area get put up for sale. Not because it's a lovely idea, but because they can't wait to get out before you turn their gardens into another Ingleby Barwick.</p>
Jill Hadfield				<p>See above – various points invalidate the assumptions on which the Design Development is based.</p>

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Joanne Mitchell				<p>The A19 has already been widened at some cost in terms of physical and mental health and the general well being of residents. Issues exist which require resolution regarding noise and air pollution and ought not to be made worse still by this imposition.</p> <p>The road would destroy the meadow.</p> <p>These sad plans they have to ruin one of the most beautiful parts of Middlesbrough not to mention the habitat of wildlife that will disappear without the meadow. Greenspaces are very precious and need to be kept as they are. Years of conservation and preservation work has been invested. People moved close to the meadow being given to understand it is protected, a greenbelt where no building or any development may ever take place. It may never have been a designated greenbelt but it was the subject of a Deed of Covenant, the land having been gifted to the people of Middlesbrough in perpetuity. A road is being planned through the meadow to service a housing estate.</p> <p>At the end of the day destroying a pristine meadow which is rich in biodiversity and has never been 'treated' with pesticides and herbicides, is nothing short of ecocide and vandalism and makes a laughing stock of the council's One Planet Living and green strategies.</p> <p>There was always another option, the 'developers' could create an estate loop road similar to the one at Trimdon.</p> <p>The council has stated they won't even pay local residents a penny in compensation for taking our public amenity from us and subjecting us to increased air and noise pollution, damaging our health.</p> <p>the disruption, destruction, the irreparable damage, and the blatant disregard for people who have lived here and who have been involved during many years in the care and preservation, and who will have their lives destroyed, just beggars belief. Residents already suffer the constant excessive traffic noise...and the pollution inflicted upon them. To think that there are those with the power who lack the empathy and the understanding, or thought and consideration for people who use the meadow and those who live close by is unbelievable.</p> <p>The residents in the Ayresome ward would no longer be able to access Mandale meadow from Mandale Road. It will be impossible for people in the Ayresome ward, including those of us who live a 2 minute walk from the current entrance to Mandale meadow is because it would involve walking straight across two lanes of the proposed roundabout. The road on OUR meadow stopping us accessing OUR meadow is the issue. All this nonsense about "you are getting extra greenspace" is an insult when you are planning something which prevents us from accessing OUR green space.</p> <p>The remarkable thing about this is some of the language used in the document to describe the proposed plan: "To create a diverse ecosystem" etc when one already exists that will be destroyed by the building of so many houses. The plan to create this "diverse ecosystem" on land which has been polluted by herbicides, pesticides and pollution from the building process while destroying a pristine meadow which is genuinely a diverse ecosystem containing Red and Orange endangered species marked on this map of Mandale meadow. They know this because the map and the legend were included in the Greenstuff response to the 2018 LP.</p> <p>The area is home to rare species flora and fauna, which have taken decades to fully establish, all of this would be destroyed.</p> <p>The faux manufactured country park, cannot, and will never, as if by magic, replace this.</p> <p>Mandale Meadow is well used and loved by many, an accessible place in which we are able to walk and relax.</p> <p>Residents currently suffer excessive noise and air pollution levels 24/7 owing to the proximity of the A19...how can it possibly be considered in any way acceptable to bring yet another road even closer to homes.</p> <p>When the A19 was widened residents received compensation for the loss in value of their homes..but despite the proposed road being routed even closer we are told we would not receive any such award for all of our losses. ..the loss of peace, and of mental and physical health. .. These of course can never be compensated for. It is not what we want.</p> <p>The notice delivered to each resident shows a basic outline of the area concerned, part of which is Mandale Meadow at the top of the 'plan'. The notice gives the impression that the entire area will be Country Park, along with which would come 1700 houses, not forgetting the so called Relief Road, to 'relieve' congestion on the A19 and serve the 1700 new homes. The plan actually shows the road through Mandale Meadow which would discharge traffic onto Mandale Road, at a junction adjacent to residents homes where the current car park is now situated, actually at the foot of their gardens.</p>

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John and Janet Roebuck				<p>Strategic Cycle Route, p29 Masterplan</p> <p>Based on the information provided, the Strategic Cycle route does not start at the entrance to Jack Simon Way. As such, cyclists will not be segregated from traffic, entering into Jack Simon Way, for around ¼ mile or more.</p> <ul style="list-style-type: none"> • Why hasn't the strategic cycle route commenced from the start of Jack Simon Way? • What is being proposed for cyclists on the initial access into Jack Simon Way? • It is described as a commuting superhighway into Middlesbrough? What is the cycling route through Stainsby connected to, to access Middlesbrough?
John David Duffy				<p>I see a lot of comments about the road cutting through a Country Park. From what I can see only a small percentage of the road will interact with the country park and I have also seen that the document considers boundary treatment.</p> <p>It sometimes looks like a small space when looking at this on a map. To me, the scheme looks to create further open space for people to enjoy.</p> <p>We have to acknowledge that housing will be built here. What we need to do is make sure the green space is the best it can be and I think the document looks to provide this.</p> <p>Who doesn't want better footpaths so more people can enjoy the area. Who doesn't want 53% of the total site is to be retained as green space. Who doesn't want to support the strategic cycle route.</p> <p>Without this document, we wont have these things. If we do it will not be to the standard Middlesbrough needs to compensate for losses elsewhere in the town.</p> <p>Small losses at mandale, but we are gaining much much more.</p>
Jonathan Hills				<p>How will the creation of the park affect the residents currently living in the boundary?</p> <p>Will they still be dependent on private landscaping companies or green area maintenance?</p>
Judith Harbron				No development
Judith Maunder	Friends of Mandale, Greenstuff, resident very local to Mandale Meadow			<p>p29, 3.4.1 Secondary road links. secondary road links will not alleviate traffic congestion during rush hour, they are more likely to lead to stationary traffic. a loop road through the estate is a much better plan.</p> <p>p26, 3.1. a road is not needed through Mandale meadow. an estate loop road is the best plan.</p> <p>p30, map. the sheer number of wide paths all over Mandale meadow is an outrage, it is as if the Mayor, deputy mayor and planning officers are attempting to inflict as much damage as possible on the meadow.</p> <p>p32, 3.8 increasing green space around the new development is laudable BUT leave Mandale and the rest of the existing public green space at BBB alone.</p>
Julia Povey				We can't control what happens on privately owned land but there should be NO ROAD THROUGH MANDALE MEADOW/WOODS.
Julie Riley				NO ROAD THROUGH MANDALE MEADOW
Kabaser Ajaib	Retailing	Kabaser Ajaib	Retailing	No through road
Keith Russell				No road through Mandale Meadow
Keith Young				No road through Mandale, you can do what you like on the private land

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Kirk Trainor				Contest further housing development
Lauren Teate				<p>Page 26. Chapter 3.1 - "This entrance arrangement will be carefully considered to minimise any impact on to the Green Open space and creating a gateway to the site." What has been considered exactly? This is a big impact on the greenspace that is already in such short supply and completely destroys the dingy skipper breeding sites.</p> <p>Page 30. Chapter 3.6 - Network of paths. How exactly is this in keeping with the natural landscape of the area? The paths are cutting right across the wildflower meadows and will all but demolish this significant habitat. The reason this "greenspace" is held in such regard by the local population is because it has no paths and it feels like a slice of wilderness. Dozens and dozens of pathways will turn this beauty spot into another Albert Park with absolutely no biodiversity whatsoever. This is just as bad as developing the site. Concrete paths cutting through and obliterating the wildflowers and the meadows is absolutely appalling. I notice there will even be paths right through the scrubland and wooded area to the back of Farley Drive and thus right past the gardens here that enjoy the privacy that the woodland and scrub provides. Where is nature in this plan exactly? How can you possibly increase biodiversity by paving over the significant habitats that make up this area? Have any conservation or wildlife organisations been contacted for their input on how to increase biodiversity? That is what was promised in the introduction section - that this development of the Countrypark will increase biodiversity and preserve the habitats that already exist. By all means put in a path that follows the already trodden paths that skirts around the meadowland if that is what is needed, however, this current design is nothing short of ecocide and will be challenged as such.</p> <p>Page 32. Chapter 3.8 - This part claims that the site will be 53% greenspace.. how much of this will actually be useable for people and wildlife? What exactly will be on these greenspaces? Greenspace is an extremely loose term and could be applied to almost anywhere that has greenery for use of aesthetic or recreational purposes. These places could simply indicate grass verges alongside roads or trees along the streets.</p>
Leah Davey				<p>This housing allocation is not necessary and should stop.</p> <p>Any 'country park elements' should remain within the site already under construction and NOT damage the area on Mandale Meadow, Newfield Meadow or the connecting green spaces by the addition of landscaping features, footpaths, cycle paths, play equipment or 'desire lines'.</p>
Linda Barker				<p>This housing allocation is not necessary and should stop.</p> <p>Any 'country park elements' should remain within the site already under construction and NOT damage the area on Mandale Meadow, Newfield Meadow or the connecting green spaces by the addition of landscaping features, footpaths, cycle paths, play equipment or 'desire lines'.</p> <p>This site allocation will cause SIGNIFICANT detrimental impacts to the existing wildlife site at this location.</p>
Lynn Blagg				<p>Leave site as it is to enable residents and others to use it in its natural state for exercise and pleasure and avoid destruction of wildlife habitats and ageing flora and fauna and avoid excess traffic in the area.</p>

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M. E. Bailey				<p>*p26, 3.1. the design for the primary road system is not fixed, the spine road can be cancelled and the new estate have a loop road only. No weight can be attributed to the new LP in the planning process in accordance with the guidance set out in the NPPF. The current LP is being based on now outdated LPs which take no account of the changes in lifestyles since the covid pandemic altered life forever. The planning is still at a very early stage and can be altered. With the government's commitments to tackling climate emergency and even more recent murmurings about protecting nature I very much doubt that the council would face any legal challenge if they dropped the plan for a road through Mandale meadow, or, indeed, if they did not pursue the purchase of farmer Pearson's land. This would mean fewer houses but the council is planning in excess of 1000 more houses than needed and even the extra amount of housing could be built on landscaped brown field sites which are on the council's register.</p> <p>*p27, 3.2. SuDs and water channels need careful consideration. A large lake planted with bog plants and marginals at it's edges will reduce the need for numerous SuDs which may be less effective although the idea of some smaller SuDs with pond dipping platforms is attractive.</p> <p>*p29, 3.4. There will be traffic congestion at various points if such a large number of houses are built but it will be particularly bad on the A1130 if a road exits at the Mandale Road roundabout which is already a considerable pinch point.</p> <p>*p30, 3.5 and 3.6. and 3.7 A dedicated, separate cycle path, rather than a mixed use one is a good idea as those with hearing problems, or the many cyclists who fail to ring their bell, are a danger to pedestrians. People with mobility issues may find it difficult to move out of the way in time to avoid being seriously hurt. I think the option of one single, dedicated cycle track through Mandale is a good idea but not a 4.5m wide one. 1.8 m is more than wide enough and most cycle commuters will be cycling in the same direction mornings and early evenings. The series of multiple paths shown through the existing open public space at BB Valley should not happen, they will destroy the feeling of being in the countryside - the very thing local residents love about the Bluebell Beck Valley. No road through Mandale, just one single, narrow dedicated cycle track.</p> <p>*p 32, 3.8. The green areas around new housing clusters is an excellent idea but leave the existing open public land alone. Nor are so many houses needed in the new development. Much more of the land between housing clusters should be green to protect nature and give a real sense of being in the country rather than a sterile country park. No development should happen on Farmer Pearson's land.</p> <p>Chapter 3 Design Development</p> <p>Public consultation 1: p17, 'Officers were able to tackle misinformation that the Stainton Way Western Extension would be a dual carriageway.' Greenstuff admin asked the Chief Planning Officer at a meeting held with him that he could NOT guarantee the road would not become a dual carriageway because it would be dependent on how much house building went on in Middlesbrough South and East Cleveland. Certainly such a mixed use path next to the projected spine road raises the possibility the road could be changed to a dual carriageway at some point.</p> <p>'Concept of country park overall positively received' this only related to green spaces being provided on the proposed new housing estate. While a percentage of people would welcome a cycle path on the Bluebell Beck Valley the concept of the road, buildings and general destruction of habitat on our existing open public land was not welcome. Only a loop road on the new housing estate is acceptable. NO road through Mandale meadow.</p> <p>Other negatives from the public consultation included the fact that only 5% of the housing will be designated as affordable. This is unacceptable in a deprived post industrial town. Mandale Meadow, gifted to the people of the area to enjoy for leisure "in perpetuity" should not have had it's covenant changed by MBC and certainly not without the knowledge and consent of the public it was intended for.</p> <p>* 53% of the 179 respondents did not contribute towards what facilities would be welcome within the Local Centre and stated "no housing, road or Country Park". So the majority view is being ignored again. There also over 900 written submissions to the council stating no building of any kind on Mandale meadow was acceptable. Council officers must accept they were elected to carry out the wishes of the people.</p> <p>*public consultation event no 2. 'Large percentage of people opposed a road through existing public open space and connecting at Mandale roundabout' In 2021 this view is held even more strongly as life has changed because of the Covid pandemic and is unlikely</p>

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				<p>to look the same in coming years. A road through Mandale will also bring a return of the drug runners on off road bikes which residents have fought so hard over many years to stop. Only after Travellers arriving on the meadow the day before a planned free event for families was something done - a metal fence was erected after the Travellers were seen knocking down a section of the wooden fence to gain entry.</p> <p>p10. A loop road through the new estate will be perfectly adequate, an exit point on the A1130 is not needed. The quantity of housing needs to be reduced as the LP provides 1,342 dwellings over expected needs and most of these can be delivered on brown field sites.</p> <p>p10. Flood risk will be increased by too much building on farm land which currently acts as a soakaway for rainfall.</p> <p>p11, site access - A road is not needed at Mandale to exit onto the A1130, there should be a loop road only as was promised to the first purchasers of homes at Brookfield.</p> <p>p12. PROW and informal paths. The informal paths around Mandale meadow are greatly prized by locals because of their wildness and closeness to nature giving the opportunity to see flora and fauna up close. One such prized, ungravelled path, so enchanting, is known as 'the fairy path'. It cannot be seen on aerial maps.</p> <p>p13, trees and hedges. While it is laudable and desirable to plant more hedges and trees it is ridiculous to axe the existing woodland on Mandale meadow as doing so releases all their stored carbon, as is the case if the meadow land is churned up during construction. Furthermore, the trees on Mandale have taken 30 to 40 years to reach their current height and are now capable of absorbing gallons of water each per day as well as producing oxygen. Again, a chainsaw massacre of the Mandale woodland goes entirely against the Council's declaration to become carbon neutral and it isn't possible to replace semi mature trees with 30cm whips as they won't be capable of storing carbon, emit oxygen, act as an easement for traffic air and noise pollution or soak up gallons of water.</p> <p>p14. The existing woodland at Farley Copse (the semi mature woodland which runs across the back of the gardens on Farley Road) does not provide easement of traffic noise pollution and adding another road so close to residents back gardens will increase both noise and air pollution. If the Mandale woodland is axed down that would also create an increase in traffic air and noise pollution making residents lives intolerable.</p> <p>p14 sewerage system. has it been calculated whether the system could cope with 1670+ more houses?</p> <p>p15, summary. Mandale meadow should be designated a Conservation Area or even an SSSI because of the endangered wildlife which can be found there. Doing a full and proper wildlife survey after the plans are passed is too late.</p>
Malcolm Chilvers				Like many people I have neither the time nor inclination to be able to read and absorb the information contained in all these chapters unlike yourselves who are being paid to do it.
Margaret Elliot				Access Road. Same comments as Chapter 2.
Margaret Jackson				Page 2 The plan shows where the proposed road will exit the meadow, in the area of the current car park. It has been noted elsewhere that car parking will be retained for users of the meadow but this is not shown on the plan. From my knowledge of the area I can envisage this requiring some of the woodland and hedgerows to be destroyed to make way for this.

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Martin Allen				<p>3.1 Primary Road - The relocated car park at the Northern entrance, whilst fitting into the space left from the shape of the new access road, does require everyone (and their dog) who uses it to cross a potentially busy road in order to go for a walk in the new country park which doesn't look sensible – though you will have a better idea of how busy it will be as to how viable it will be. I'm not sure whether it is technically possible to swap those two positions so that the access road is to the north and the carpark to the south from where people can exit directly into the park, but if you do then the road is less intrusive physically on the meadow and takes up less space within the country park & immediately adjacent meadow. I think 4.17 Indicative Northern Gateway Study was supposed to address this but all those "Chicanes or gateway features" just looked like sticking plaster for poor car-park location and really a country park should have minimum of sub-urban features of this sort. Calling a mound 'naturalistic' doesn't stop it looking like a pile of earth plonked on an otherwise flat landscape to which the mound has no logical relationship and I would be cautious about the amount of hedgerows planted due to the cost of maintenance which the council have said they will adopt. In summary it would be safer for visitors having the carpark on the south side of the access road and in the open (i.e. surrounded by fencing, rather than mounding or shrubs) which would mean changing slightly the position of the primary road</p> <p>I see in the FAQ document that you are expecting the access road to be as busy as Stainton Way and so I will take this opportunity to mention that I am hoping Middlesbrough Council will be designating part of Stainton Way as an LWS for its grassland soon, as it meets the criteria and then they will managing the vegetation better in respect of that designation.</p> <p>3.8 Green Space calculation</p> <p>I was really uncomfortable with the figure that 53% of the total site is to be retained as green space when almost half of that green space is already green space on public land and its current use is unrelated to whether or how the privately owned Stainsby sites are developed. I couldn't quickly see the hectareage of the publicly owned part to redo the calculation relating only to the privately owned site. It felt a bit of a marketing con to me especially given that the publicly-owned area is already used quite happily by local residents without it being a called a public park. I can see the concept for including it into the overall vision such that money would be available for works to be undertaken on the public side of the site or that historically you were going to build houses on that side, but there is nothing that would be of particular advantage to the existing public site and as currently proposed much that will damage the habitats and wildlife present, so I don't see why it is included. Perhaps a clearer breakdown of areas would be a more honest way to put the point across.</p>
Matthew Shippey				Don't want this. I walk in mandale meadow. Middlesbrough is lacking in green space and you want to build a road through to facilitate new housing. Middlesbrough Council you are a disgrace.
Michael Miller	Natural England			please see general comments under chapter 8 comments.
Michael Starford				3.5 the combined effect of the main cycleway and roadway at the Mandale road entrance is a large swath off the meadow.
Michelle Hedger				<p>The loop road option.</p> <p>No road through Mandale Meadow. Use the loop road option. If a road needs to be there make it a narrower road of 20mph. My children cannot play near the huge 30mph road planned. They can play now in the meadow. They will be hit by a car in this 'country park'.</p> <p>Please don't put all the footpaths in. We like to follow the natural winding dirt tracks. There is only lip service paid to these. We don't want artificial paths. It's beautiful, muddy and filled with nature.</p>

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Mike Conyard				Do not build on this land. This is much needed green space. There are no parks in this area and nowhere for people to walk, kids to play and people to live their lives. I find it disgusting that you propose to build on this beautiful piece of land. Also, who wants to live right next to the A19? These house will be worth literally nothing.. The land is also a buffer zone from the A19 to the residents of Heythrop Drive and beyond.
Miller Homes and Avant Homes	Miller Homes and Avant Homes	Mr Neil Morton	Savills	<p>3.3 Development Quantum Within Residential Context – insofar as the remaining southern ownership land is concerned, the locations of the Residential Development Zones and Central Hub Zone are broadly supported, although the final quantum will be for the planning application process to determine. Our clients welcome the inclusion of the Residential Development Zone north of the SWWE and south of the proposed playing fields and this should be retained in the final masterplan.</p> <p>3.8 Green Space Calculation – the draft masterplan states that “an equivalent area to cover 53% of the total site area is to be retained as green space”. The Council’s ambition in this regard is noted and it is acknowledged that HLP Policy H21(h) requires that the development incorporates a country park, although no scale is prescribed other than its general disposition across the allocated site. The masterplan’s Green Space Calculation (%) is a site-wide calculation (as opposed to an application specific calculation) and our clients are broadly satisfied with the green space quantum shown as it relates to their land interest although the final provision will be subject to the planning application process.</p>
Miss Taylor	Northumbrian Water			<p>We note section 3.1 relates to primary roads and sewer easements on the site. We feel it is worth noting that a strategic water main is also present within the redline boundary of the wider site. The strategic water main may fall outside of the proposed development parcels but we will need to work with the developer(s) to establish the exact location of our assets and ensure any necessary diversion, relocation or protection measures required prior to the commencement of the development. The easement of this main must remain in public open space. No private gardens or driveways can be placed within the easement width, this includes private shared driveways. This is to provide Northumbrian Water with unrestricted access in accordance with Section 159 of the Water Industry Act 1991, to undertake any repairs, alterations and maintenance to the water mains. For further information please contact Niki Mather on 0191 419 6603 or alternatively via niki.mather@nwl.co.uk.</p> <p>In addition, the developer should contact Northumbrian Water to agree allowable discharge rates and points into the public sewer network. This can be done by submitting a pre planning enquiry directly to us. Full details and guidance can be found at https://www.nwl.co.uk/developers/predevelopment-enquiries.aspx or telephone 0191 419 6559.</p>

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Mrs D M Hornby				<p>Footpaths</p> <p>One multi-purpose footpath, suitably surfaced, NOT tarmac'd around the eastern edge of Mandale/Acklam meadow is acceptable. Maximum width 7ft6ins. Less is more!</p> <p>Resurfacing of the path known locally as the 'White Path' along the northern and western edges is also acceptable, again, suitably surfaced, NOT tarmac'd. The road is also stated to have a cycleway/footpath.</p> <p>This field is a known nesting site for Green Plover (Peewit) and Skylark, already on endangered lists. Surfaced footpaths from west to east across the remaining open space will cause unacceptable disturbance to these birds and others. The grass should not be mown during nesting and fledging seasons for the same reasons, and not mown until wildflowers have seeded and dropped their seeds.</p> <p>An enclosed footpath should be created along the side of the Acklam Grange school field, giving access to Lodore Grove and thence the shops on Acklam Road. Also a foot access to the existing gate in the western edge of the school field for pupils from the new estates to walk to school, rather than causing more traffic through existing housing.</p>
Nichola Davey				<p>This housing allocation is not necessary and should stop.</p> <p>Any 'country park elements' should remain within the site already under construction and NOT damage the area on Mandale Meadow, Newfield Meadow or the connecting green spaces by the addition of landscaping features, footpaths, cycle paths, play equipment or 'desire lines'.</p>
Nicola Smith				<p>We DO NOT WANT IT!!!!</p> <p>WHY can't you build on areas where you have flatted hundreds of houses but not rebuilt? Keith Road or Marton Burn Road??? This is the only green space we have in Acklam and hundereds of residents rely on this being walking distance. Pre covid the traffic on mandale Road, Acklam Rd and levick Cresent is bad enough at peak times without further traffic. You have Acklam Grange Secondary school round the corner from the meadow with 1200+ students that also use these roads. This is already a very busy junction without diverting more traffic to it. What about adults and children's mental health?? Has the past year taught you nothing?? I think having a open greenspace with all the wildlife and plenty of room to exercise and get much needed fresh air and been an absolute saviour to hundereds of local residents. This project has not considered anything but money. Looking at the plans there will be very little green left and it will be surrounding a road and housing estate. Absolutely nothing like we have now. This is NOT ACCEPTABLE FROM ANYONE I KNOW WHO LIVES IN ACKLAM 😞😞😞😞😞😞😞😞😞</p>
Nicolas Mitchell				<p>While it sounds very generous in 3.00 Design Development to include 53% of the total site as green space, the area covered includes Middlesbrough Council land which is already common land and much valued for that. I think the Council must ensure that the semi-wild common land aspect is retained and that the development is NOT turned into a planned urban park. In otherwise DON'T do too much to change the nature of the place.</p>
Peter Davey				<p>This housing allocation is not necessary and should stop.</p> <p>Any 'country park elements' should remain within the site already under construction and NOT damage the area on Mandale Meadow, Newfield Meadow or the connecting green spaces by the addition of landscaping features, footpaths, cycle paths, play equipment or 'desire lines'.</p>

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Poppy Bowyer				<p>Do you think the vehicle access and through road will be sufficient for the number of houses (1800ish) within the development?</p> <p>What was deciding factor of 1500 houses?</p> <p>Can 1500 homes be environmentally friendly long term?</p> <p>Will the needs of the clientele have changed by the time the project is finished?</p> <p>Do you think that many houses will take away from the integrity and delicacy of the design or vision?</p> <p>Do you think that many will take away from the sustainability of the project?</p> <p>Are the LAP areas going to translate as somewhere to play?</p> <p>How do you intend to entice and encourage use of these areas?</p> <p>Will all access and access points be wheelchair and handicap friendly?</p> <p>Will all signs incorporate braille?</p>
Rob Atkin				No road through Mandale.
Robert Teate				Do not agree with this delopment at all.
Ronald Dennison				A large open area such as that at Stainsby would be welcome for exercise and for nature and it is good to see that links to the exisiting rights of way to the south and the west next to the A174 and A19 will be included.
Scott Elliot				<p>Primary road.</p> <p>Same comments as Chapter 2.</p>
Sean Marshall				Excellent design but measures to keep it in good condition needs to be taken. Proper park security
Sofia Torre				<p>"The location and design of the primary road has emerged following three consecutive rounds of community consultation" I laugh at consultation you had already made your mind up, we were faced with developers and behind their skirts was the town and planning not able to face the community how pathetic. Questionnaires that were loaded questions and biased, no where to give true feelings. What is incredulous is that again perform a consulation, hiding behind covid to not show your faces and to do it on line so you can't see by faces what you are doing is destroying our only green area shame on you all, shame on you all.</p> <p>You talk of easement , but where will be the easement when you will be blocked joining the bottom end of Mandale road? Traffic lights, larger roundabout or non at all, you are going to create the biggest bottleneck but you won't be breathing in the fumes no becuase you all probably live somewhere else as is always the case</p>
Sonia Bowler				<p>That roundabout is already congested with traffic!</p> <p>Sorry you haven't convinced me.</p> <p>No road through Mandale Meadow!</p>

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Stacey baillie		Stacey.baillie@icloud.com		Unable to comment as there are no details
Stephanie Atkin				Do whatever you will on private land but NO road through public greenspace.
Stephen Connors				Thoughts previously mentioned
Steven Brown				<p>3.6 Network of Paths The final layer to be applied to the design development will be a network of paths, connecting and tie together all aspects of the masterplan layers. The path structure, as its developed, will navigate through all the Landscaping conditions creating a network of hierarchy throughout the site. Each development area should be able to access all areas of the site through this integrated path structure. At this stage all paths are shown as principles for indicative purposes What will be the connection with regards to Stainsby Hall Residents</p> <p>3.0 DESIGN DEVELOPMENT 3.8 Green Space Calculation The adjacent Diagram 3.7.1 indicates the strength of the landscaping proposal by displaying the extent of green space in comparison to development area, explored further in section 4.0. The diagram highlights a primarily landscape setting for development clusters with connecting infrastructure. An equivalent area to over 53% of the total site is to be retained as green space.</p> <p>The diagram indicates a false representation of green space Private land on the South of the site is included in the colour diagram where as all other areas are grey. This gives a false indication of more green space. This should be grey and the red line adjusted to the correct site plan</p>
Stewart Jones				<p>Can you look at incorporating a scheme like this in the CP This is from Wetherby were they created an adventure cycle track for children</p> <p>SEE EMAIL 05/03/21. FILE CANNOT BE ATTACHED</p>
stewart jones				Essential low density is only approved, Delighted that the residential dwellings were removed from Mandale meadow

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Thomas Maunder				<p>*p26, 3.1. the design for the primary road system is not fixed, the spine road can be cancelled and the new estate have a loop road only. No weight can be attributed to the new LP in the planning process in accordance with the guidance set out in the NPPF. The current LP is being based on now outdated LPs which take no account of the changes in lifestyles since the covid pandemic altered life forever. The planning is still at a very early stage and can be altered. With the government's commitments to tackling climate emergency and even more recent murmurings about protecting nature I very much doubt that the council would face any legal challenge if they dropped the plan for a road through Mandale meadow, or, indeed, if they did not pursue the purchase of farmer Pearson's land. This would mean fewer houses but the council is planning in excess of 1000 more houses than needed and even the extra amount of housing could be built on landscaped brown field sites which are on the council's register.</p> <p>*p27, 3.2. SuDs and water channels need careful consideration. A large lake planted with bog plants and marginals at it's edges will reduce the need for numerous SuDs which may be less effective although the idea of some smaller SuDs with pond dipping platforms is attractive.</p> <p>*p29, 3.4. There will be traffic congestion at various points if such a large number of houses are built but it will be particularly bad on the A1130 if a road exits at the Mandale Road roundabout which is already a considerable pinch point.</p> <p>*p30, 3.5 and 3.6. and 3.7 A dedicated, separate cycle path, rather than a mixed use one is a good idea as those with hearing problems, or the many cyclists who fail to ring their bell, are a danger to pedestrians. People with mobility issues may find it difficult to move out of the way in time to avoid being seriously hurt. I think the option of one single, dedicated cycle track through Mandale is a good idea but not a 4.5m wide one. 1.8 m is more than wide enough and most cycle commuters will be cycling in the same direction mornings and early evenings. The series of multiple paths shown through the existing open public space at BB Valley should not happen, they will destroy the feeling of being in the countryside - the very thing local residents love about the Bluebell Beck Valley. No road through Mandale, just one single, narrow dedicated cycle track.</p> <p>*p 32, 3.8. The green areas around new housing clusters is an excellent idea but leave the existing open public land alone. Nor are so many houses needed in the new development. Much more of the land between housing clusters should be green to protect nature and give a real sense of being in the country rather than a sterile country park. No development should happen on Farmer Pearson's land.</p>
Tim Allen				Would it not be possible to allow walkers / cyclists to use the 2 smaller current bridges that cross over the a19 to link up with paths into thornaby?
Trevor King				We can't control what happens on privately owned land but we want NO ROAD THROUGH MANDALE MEADOW/WOODS.
Val Young				No road through the meadow
Dave Blackburn				You mention views in your plan, fr the new residents on famers fields. But what about mine. Your field picture were so lovely I didn't recognise them at first. Why would you want to spoil them. I have enjoyed the view of mandale for many years but I also want it leaving for the younger residents. This road will be ugly and routine my lovely outlook. Unacceptable.
Doris Hammond				Stop spending money you don't have to. And leave what is left o f the green space as green and a field. Please do not touch my trees and paths.
Gill Sullivan				You state biodiversity has a high priority for MBC, that cannot be true otherwise MBC would not be considering their current action. The nearest wide space green belt is in Durham and Tyneside. Surely MBC are not suggesting that their residents need to travel to Durham when we have our own wild space just down the road. Cllrs tell us it is inevitable, just accept it, coz it will happen. When happened to democracy in this town. I am sure the residents elected the mayor and deputy mayor and Cllrs, you and your staff have been elected to serve the residents not developers. If you put this road through you will make a mock of your green planet living. MBC has been made aware that there is currently a world wide climate emergency and their response is

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				building thousands of houses which will generate thousand more cars with pollution and carve up a field, not needed, as their is a perfectly good alternative. In the process mess up the unique biodiversity of blue belle beck and its meadows. What is MBC thinking? Your ethos of green plant living is nothing but words and if the road goes through a farce.

Question 5 – Please provide any comments you may have on Chapter 4.0 Land Principle

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Alexander Henegan				A great deal of thought has gone into landscaping and creating a range of amenities for those using the park. Attention to biodiversity is also strong.
Aiden Woodward				I love the country park and the idea of having smaller play areas interspersed throughout the development. The country park is key to the whole thing and needs to look as nice as the pictures shown, and be well maintained after completion.
Aji Varghese		Aji Varghese		Good
Alan Liddle	Stainton and Thornton Parish Council			Play areas and green spaces are an important part of the development as a whole and should be maximised where possible.
Alan Turley				<p>Environmental Impact: The Masterplan document states “The character of the development will be defined by the landscape setting and the importance of nature. It will enhance and extend existing habitats” What evidence have the council got that this is achievable ? The land currently utilised as farm land serves to provide a natural habitat for a variety of species including deer and foxes. Both species are naturally suspicious of humans and avoid contact wherever possible. To suggest that they will happily co exist alongside this development is clearly inaccurate. They will be displaced from the last remaining natural habitat. Local reports already indicate that deer have been displaced and sighted in urban areas in the neighbouring Borough. Deer alongside a road with a high volume of traffic is a recipe for disaster for both them and motorists. The existing hedgerows that provide and have provided a home for other animals for over a hundred years will be wiped out as they have already been on the existing phases.</p> <p>The building of houses in place of farm land will never alter the carbon footprint of the town for the better. The current fields provide much needed natural drainage which will be lost should the development go ahead. It’s also safe to assume that each house built will have at least one car and will be drawing resources such as gas and electricity. How does any of that equate to meeting the councils reduced carbon targets.</p> <p>Existing facilities: The council intends to spend tax payers money to develop and maintain a country park. The current developments already have access to a vast amount of green spaces. The area from Gayton Sands along the green belt is surrounded by trees leading out onto an open field complete with kickabout pitch. Beyond that you have Fox’s wood leading out onto more green space and another pitch. From there you can walk as far as Mandale in one direction and Low Lane in the other with only having to cross one road. It’s well serviced by a path and has an existing nature trail. To improve access to this area for phase two of the existing development would simply take the construction of a further path from the new estate onto the existing green area. This could be achieved at a fraction of the cost to taxpayers.</p> <p>Historical Impact: The existing land has been farmed for over a hundred years and is the last remaining example of the agricultural heritage of the Acklam area. The council has already permitted building on other previously farm land in the town stretching from the borders of Hambleton to Thornaby. To allow this development to take place will rob future generations of the opportunity to see the land being worked and the nature that co exists within the land.</p>

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Alan Turley				Existing facilities: The council intends to spend tax payers money to develop and maintain a country park. The current developments already have access to a vast amount of green spaces. The area from Gayton Sands along the green belt is surrounded by trees leading out onto an open field complete with kickabout pitch. Beyond that you have Fox's wood leading out onto more green space and another pitch. From there you can walk as far as Mandale in one direction and Low Lane in the other with only having to cross one road. It's well serviced by a path and has an existing nature trail. To improve access to this area for phase two of the existing development would simply take the construction of a further path from the new estate onto the existing green area. This could be achieved at a fraction of the cost to taxpayers.
Amanda Stevenson				4.3 I cannot see how building a road through the Mandale Medow will strengthen or expand the existing habitats. The road will be a rat run that will result in increased pollution levels, reduced green space and have a negative impact on pedestrians and wildlife.
Amy Lord				4.4 Play areas. The new play area on Trimdon Avenue has been a consistent source of problems with anti-social behaviour since it was installed. How will any similar areas in this area be monitored? 4.5 Additional paths in the meadows not impacted by the road seem unnecessary. One of the attractions of this land is its natural state, which will be reduced by the additional of gravel paths. This will also attract dirt and quad bikes, which are a constant problem in the area.
angela barber				AS a resident of this area, I fear my house value will lower so close to this new road. There will be No where to walk my dog There would be Little or no space for children to play safety in area An area to exercise my self and enjoy picnics with the family Local wild life will loose there home and endangered species plants and animals will loose there environment Why should be suffer ill health because of further pollution We pay council tax for green space- will our council tax lower-expect not Increased noise during the day and night- lack of sleep and depression result All you want to do is provide new access road to the estate- why cannot they come around down downside and Acklam road instead as always has happened. I feel the new road will increase traffic into Middlesbrough through our area. I do not want or need the road as I do not drive- I doubt it will be included on the bus route, so it does me no good. In fact everything about it is BAD
Angela O'Byrne				Hands off our meadow!
Barbara Keville				Formalising footpaths, woodland, dog walks recreation areas is only going to sterilise the existing areas. I cannot see any benefit in formal orchards or allotments when people these days want small formal gardens that require no work.

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Barry Jobson	South Tees Green Party			<p>However, should the scheme go ahead, we have noted a number of positives and negatives in the proposals, and also have additional comments, which we have noted below.</p> <p>Positives</p> <ul style="list-style-type: none"> · Commuter cycle route · Additional ponds and swales incorporated into the site, to compliment those existing, and the proposed SUDS. This will provide extra wetland habitat, and potential for water vole expansion from Bluebell Beck. · Previously proposed housing on eastern side of Bluebell Beck have now been removed · New woodland [including carr], grassland, and wetland habitats – including woodland understory [often overlooked] · New foot/cycle paths · Pedestrian-friendly crossing points · Formal and informal play areas · 20mph speed limits on tertiary roads, and 10mph in 'home zones' welcomed. <p>Negatives</p> <ul style="list-style-type: none"> · Loss of open space · Loss of open views from existing housing and green spaces · Loss of trees and grassland habitats created in the 1980s and 1990s that are only now maturing. · Loss of marsh habitat [location of original 1980s Mandale Pond – filled in in 1990s when A19 was widened] · Increased traffic noise for existing houses off Mandale Road. Additional Comments · Wet areas on Mandale Meadow could be opportunities for additional wetland creation. Historical maps show a pond along the eastern boundary that was lost to development. New ponds could make-up for this loss. · Secondary roads should also be 20mph, with traffic calming planned in, to prevent these become rat runs when traffic builds up at peak times. · New tree planting within the country park should reflect locally native species, as not all British native trees are locally native. · Seeds for grassland, woodland understory, and wetlands should be sourced from local biodiverse sites within the Tees Valley. Scattering green hay, harvested from these local sites, will contribute to their management, and ensure that plants are adapted to local conditions. · Grassland seeds should be sown on sub-soil, as this will ensure species rich grassland is not lost to more vigorous species that enjoy nutrient-rich soil. This will also ensure that grassland is slow growing, and as such will save the council money by requiring mowing less-often than grassland sown on top-soil. · Scrub should include species that were found nearby historically, but are no longer present, such as gorse and broom, which are referenced in field names on tithe maps from the 1800s.
Barry pinkney				Country park no

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Ben Stephenson	Persimmon Homes Teesside			<p>Section 4.8 Play and Sports Provision</p> <p>Whilst we understand the aspiration of providing a Local Area for Play (LAP) within one minute walk time of each property we believe that the sheer quantity of LAPS around the site may lead to a number of issues.</p> <p>The 20 LAPS identified on the plan in Section 4.8 of the report are in addition to three LEAPs, a NEAP, managed sports pitches with associated pavilion and a multi-use games area / tennis court. This seems excessive for a site of this size. As well as being a serious and costly maintenance liability in the long term, these areas could also lead to conflict between surrounding residents and users, particularly if they are used as gathering points for antisocial behaviour.</p> <p>The development is to be supported by a clear, safe network of footpaths and links. We therefore believe that these should be utilised and the focus in terms of play provision be on the quality of areas provided rather than quantity, ensuring that LEAPs are located at convenient locations on the public footpath network and appeal to as big of an age range as possible. We believe that this more concentrated approach will reduce the potential for conflict on the estate once constructed.</p> <p>Section 4.17 Indicative North Gateway Study</p> <p>We note that the image associated with this section of the report identifies an almost continuous line of avenue tree planting along the Stainton Way Western Extension. Given the location of this tree planting between the highway and the strategic cycle route we would expect this area to form part of the adopted highway.</p> <p>We have found from our experience elsewhere throughout the region that the adoption costs associated with street trees adjacent to the highway can be substantial. Whilst the aim of incorporating substantial avenue tree planting is therefore admirable, it is important that the costs associated with such an approach are considered alongside the other measures suggested by the document to ensure that the overall scheme remains a viable proposition. This is particularly important as the document suggests the verge along all primary, secondary and tertiary routes will incorporate tree planting.</p> <ul style="list-style-type: none"> · Are the Local Highways Authority prepared to adopt the trees? · Have the costs of adopting street planting been identified and considered as part of the viability review of this document?
Bethany Thatcher				I am very pleased with the proposed design of the landscape. It will be lovely to have such a nice area within walking distance
Carol Newmarch				I object to the whole plan

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Catherine Howell (Boyle)				<p>4.1 Country Park - the 'country park' overlays a well used and well loved green space that serves local residents well. Most access on foot; few use vehicles. Residents have particularly made use of the space lately for physical and mental wellbeing. The proposed country park development will be bisected by a major road, which will be used by traffic to avoid congestion to estimated numbers of 12,000 vehicles per day. Additional traffic will be driven to the area from outside as it will be marketed as a visitor attraction (see Wynyard, Flatts Lane, Billingham Beck etc). This will massively increase pollution and noise and spoil the area for local people. There will be substantial and irreplaceable damage to the area's conservation value, especially during the construction phase.</p> <p>"Green Commercial and Community Hub" - what is 'green?' What does this mean in practice?</p> <p>"Food growing and allotments" - there is no shortage of space within Middlesbrough for food growing and small allotment waiting lists. MBC are currently pursuing a policy of allotment self management, so how would this be managed, who would be responsible and how would this be accommodated in future budgets? Public orchards require an annual management cycle. Again, who would be responsible for this? How would food be made accessible and how would this be budgeted given the constraints acknowledged by MBC at the current time?</p> <p>p36 "Urban Pollinators Project" - what is this?</p> <p>4.5 "Commuting superhighway" - how is the safety of pedestrians and cyclists to be maintained on a road being used by 12,000 vehicles per day? The pollution and noise will make it unappealing and dangerous for those using sustainable transport. How will this link with the existing poor quality network beyond the proposed development?</p> <p>p41 Allotments designated in the area that most frequently breaches air quality limits in the town, where noise and fumes will render allotment activities unpleasant at best? How will the management of these allotments be incorporated within Middlesbrough's current allotment strategy? Are staff adequately resourced to be able to do this? Is there an anticipated demand for allotments from residents that will have houses with gardens?</p> <p>Community growing/orchards - is there a demand for these? How will they be managed and who will manage them?</p> <p>4.1.2 Who will be responsible for the long term upkeep and maintenance of 'public art' and will this be costed into MBC budgets for the future?</p> <p>4.1.4 "The purpose of a country park is..." This space is already enjoyed by local people in this way without a road through the middle and increased traffic flow.</p> <p>p46 Commercial and community hub with spaces for eating, activities etc. How is this now affected by Covid restrictions, anticipated to last for several years? "Volunteering" Again, who will manage this and has it been costed into budgets?</p> <p>p53 "Led," not "lead." Graphics annotations "wildlife," "wetland" misspelt. This does not inspire confidence in the proposal!</p>
Charlotte McAdam				<p>4.5 - page 39 - you have a proposed access point to the path network at the gate for Acklam Grange School. This gate is very rarely opened currently. However, as a house that backs onto the school field, the idea that people would be encouraged to use this as an access point would make us feel significantly less secure.</p> <p>4.14 - page 46 - you identify that a significant outcome of public consultation on the Country Park is that new roads should be kept away from the Country Park, and you state that these issues have been addressed in the new masterplan. However, you are continuing to have an access point coming through Mandale Meadow and onto Mandale roundabout, and this does not address this issue at all.</p>

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Chris King				<p>'The strategy at Stainsby is simple. Everyone will be able to access paths and routes that take them where they want to be, whether this is to a neighbourhood play area, the commercial centre, the country park, or further afield and into Middlesbrough or the open countryside. The adjacent diagram shows an indicative proposal, subject to detailed design. The hierarchy is such that a strategic segregated pedestrian and cycle route at 4.5m wide will be attached to the main roads providing a commuting 'superhighway' into Middlesbrough and surrounding areas.'</p> <p>The idea of a safe superhighway into Middlesbrough and the surrounding areas is highly desirable. At present, however, conditions on the Mandale Roundabout can be challenging and indeed dangerous at peak times. The introduction of yet more traffic into an already congested highway is hardly safe. Indeed, long lengthy queues of traffic using the Mandale interchange often form on the south bound A19 at times of the evening commute.</p> <p>'Indicative Northern Gateway Study The Northern Gateway to the site is one of the most significant moments in the Stainsby masterplan, creating a distinctive landscaped focal entrance arrangement. This moment includes a pedestrian focused crossing point with natural traffic calming features to emphasis the transition into the Country park.'</p> <p>It would be very interesting to see the design for the entrance onto Mandale Rd? I presume by Northern Gateway you mean the southern entrance?</p>
Christine King				We can't control what happens on privately owned land but we want NO ROAD THROUGH MANDALE MEADOW/WOODS.
Christopher Bieda				In light of the above I would like to establish who will be responsible for all green areas, pavements, roads and lighting services within the above contested areas. surely with this being a major through road, it should be adopted by the council and be subject to full council responsibility. (PLEASE CAN YOU ADVISE)
Claire Boyd				We can't control what happens on privately owned land but there should be NO ROAD THROUGH MANDALE MEADOW/WOODS.
Claire Di Cicco				No to a road.
CLAIRE KANE				The pictures are misleading it will be a black tarmac road for people to speed up and down destroying the habitats and wildlife - we already have foxes owls and bats around the area that are walking and flying around our 40 + year old estate because they have had their habitats destroyed by the new houses on Jack Simon Way estate

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Claire Wilks				<p>4.1 Paths are not needed for recreation in the area. People can do that anyway without spoiling the natural landscape that there is there. These plans showing paths and cycle paths of 3.5-4.5m will take away the natural beauty of the landscape and fields, creating a maze of gravel. There are play and sports provision areas already in the local community, Acklam Grange School, Acklam Green Centre and the Acorn Centre as well as Stewart Park</p> <p>The road speed limit will not prevent people speeding to use the road as a cut through to miss out traffic in the A174. This will be dangerous for those using the area especially children cycling and playing.</p>
Colin and Morag Bailey				<p>*p34, 4.1. The landscape is far more important than the built environment in this proposal. We have Climate Emergency and the rapid decline of wildlife to be concerned about. All building construction pumps carbon into the atmosphere and MBC has signed up to becoming carbon neutral. More than 53% of the land needs to be green and allow nature to grow and flourish and wildlife needs some 'wild' areas in order to do that. The recent sad sight of a baby deer running frantically around the streets of Thornaby searching for its mother tells us we have already destroyed far too much of the natural environment with endless house building. Middlesbrough needs to set an example and put nature before profit. 'Ensuring the Country Park permeates the development in terms of its character throughout the site'. I strongly disagree with this statement. The existing public open space can be left as it is and the new development can blend seamlessly into the natural landscape already on the Bluebell beck valley and mandale.</p> <p>There is no need for play parks or other man made structures on the existing open public space which local residents love as it is. If people can't find their own routes across the existing open public space perhaps a country park setting is not right for them. There is no sense of adventure or exploration in an area with endless paths. These plans for multiple paths cause destruction and carbon release. Most, if not all, of the paths are too wide to allow for a sense of quiet or solitude or a oneness with nature. Very narrow natural paths caused by people exploring, such as 'the fairy path' on Mandale, are the best kind of paths for solitude and quiet. A multitude of man made paths is very constricting and unnatural.</p> <p>*p35, 4.2. SuDs need to be more carefully thought out than they have been on Mandale as the Mandale SuDs floods in winter and dries up in warmer weather, leaving wildlife such as tadpoles and invertebrates in the flooded areas to die in isolated mud puddles which soon turn to hard, dry clay. The Mandale SuDs should be enlarged and made deeper in the central area to prevent the unnecessary death of wildlife.</p> <p>*p36,4. 'facilitating the movement of wildlife throughout the site' The green corridors need to be much wider than they are on the plan to allow wildlife movement throughout the site.</p> <p>'The woodland type' only native deciduous trees are mentioned but there needs to be some native evergreens included in the planting for the easement of traffic noise pollution such as English yew, holly etc. Hornbeam should also be planted as it attracts a moth whose only habitat is hornbeam.</p> <p>' In other places new hedgerows will be planted to break the monotony of the long sewer easements' please do not apply this to the existing open public greenspaces, we love our well established and natural meadows which have occurred without man's interference! The large meadow on Mandale is much loved and attracts a vast range of insects which are vital pollinators and provide food for the small mammals, birds, damsels and dragonflies. You can't improve a natural landscape.</p> <p>'with naturalistic earth mounding using the construction spoil'. DO NOT put construction spoil on Mandale meadows, it will despoil the environment. Dumping it on Mandale will destroy the existing flora and fauna there. Use it on your new development. Instead of relying on Tees Valley for ideas you should consult with Greenstuff admin as they know the land and its flora and fauna best.</p> <p>*P37, 4.3. The landscaping plans, apart from paths being too wide and numerous, are fine except on the existing open public green space which should be left as it is. Public orchards are a lovely idea but would be better planted in deprived areas as a source of free fruit.</p> <p>*p38, 4.4. Recreational play for children is better served by natural greenspace than man made structures as it develops the imagination, peer friendships and co-operation, conversation and creativity. Today's children have far too much organized activity in terms of electronic gadgets, they need natural green spaces in which to develop physical skills and learn their body's limits.</p> <p>*p39, a 4.5 metre path through Mandale meadow and the beck valley is far too wide and destructive. 2.5m and 3.5m paths are ridiculously numerous and too wide, they need reducing in size and number and kept to the new development with the exception of one narrow, dedicated cycle path on the existing open public space. I'm not sure why 'potential entry points' are shown on the map as places like Mosswood Crescent already have access to the existing open public space.</p>

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				<p>*p40, 4.6 as above, reduce the width and number of paths and only one, narrow, dedicated cycle path through the existing open public green space. Informal paths should not be gravelled or their edges close mown as that causes the loss of informality and the feeling of being close to nature. 'Desire line' paths do not need to be mown, they occur naturally through the footfall of wildlife and humans.</p> <p>*p41. DO NOT dump excavation spoil on Mandale meadow, calling the spoil 'landforms' does not detract from the fact you are dumping waste on a meadow and killing off a large area of meadow. Use your building spoil where you remove it, i.e. on the new housing estate. Residents do not want your rubbish dumping on them and scarring the landscape. NO ROAD through MANDALE.</p> <p>*p42, 4.8 please keep LAPs and LEAPs in the new housing development and not on the existing semi wild, public open green space. There is ample opportunity to play and stay healthy in the existing green space, local children play all sorts of imaginative games and stay fit and healthy on our natural green space. Man made paly areas tend to get trashed by teenagers.</p> <p>*p43, 4.9 the first LAP pictured on this page is too small for a play area, it needs to be at least 5x as large. The NEAP shown at Houston is a good example of how to use excavated spoil on the new estate, and with encouragement, young children can use such equipment as an acted retelling of a story such as The Billy Goats Gruff as well as being fun for slightly older children. The LEAP shown in Rome is not adventurous enough. The mounds could be higher for sledging down in the winter for example. again, keep LAPS, LEAPs and NEAPs on the new estate.</p> <p>*p44, 4.12. Public art and signage is not needed on the existing open public greenspace although some more informative signs next to the gravel path would be a good idea as the old signage is worn out and fails to show much of the wildlife we have in the area. Interestingly, MBC are fully aware that we have water voles in and around the beck as the signage says so. Art doesn't lend itself to natural spaces as it is obviously a man made structure.</p> <p>*p45, Hedgerows are welcome habitats and provide both shelter and food for wildlife, the denser planted hedges are best to provide more shelter and safety for wildlife. 4.13 please leave the existing open public green space exactly as it is other than some hedging by the Mandale metal fence which should stay in place to prevent entrance to Travellers and off road drug runners bikes.</p> <p>*p46, 4.14. The problem with the 'vision' of the country park is that it is too manicured and formal, which is the opposite of what MBC claim is their intention. Leave Mandale and the BB Valley as they are - richly biodiverse, semi wild natural green spaces. Where the council see a beautiful country park all we see is another town park such as Albert park, too manicured, too manmade to be a beautiful rural retreat.</p> <p>*p47, 4.15, retaining the existing open public semi wild green space as it is, is much more important to residents than green flag status.</p> <p>*p48, 4.16. Leaving the existing open public green space as it is with NO ROAD through Mandale, just a loop road through the estate, means the manicured 'country park' will flow into the semi wild existing area and make a pleasing change to the heavily manufactured park.</p> <p>*p50, 4.17, there is no need for 'an indicative gateway' at the northeren end, i.e. Mandale meadow as it is well known and well used. Just don't build the road through the meadow and save a lot of money. Residents don't need or want an increase in air and noise pollution. The combination of road, 4.5m path and all the planting and dumping of construction spoil on Mandale is not wanted. The 'naturalistic earth mounds' created from excavated spoil will not grow healthy trees and shrubs. Central reservations will not be needed if there is no Stainton Way Western Extension. No road through Mandale, just a loop road through the new development.</p> <p>*p51, it isn't clear where the SuDs etc in the map on p51 are going to be, hopefully not on Mandale meadow. All the grasses and plants in Mandale meadow are vital for wildlife. Councillors may just see long grass on a map, it isn't what residents and wildlife see.</p> <p>*p52, the 4.5 m multi use path is far too wide for a rural country park look.</p>
Darren Middleton				<p>Also it states a 100 acres of new green space will be available? For scale the 2 football pitches are approximately 4 acres. Can't see another 96 acres of new green public space being created. The majority of the green space on the pic is existing. Also the 20,000 trees that are to be planted and the creation of wetlands. How many established trees and existing wetlands are to be destroyed? The road that will run up through Mandale meadow will destroy existing nature reserves.</p>
Dennis McCabe				I personal like the landscaping that the site will bring
Emily quintt				No road necessary. Do you want people and animals to get run over?
Francine Kenny				Yes to Country Park Yes to houses yes to road

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Gill Mollard				This housing allocation is not necessary and should stop. Any 'country park elements' should remain within the site already under construction and NOT damage the area on Mandale Meadow, Newfield Meadow or the connecting green spaces by the addition of landscaping features, footpaths, cycle paths, play equipment or 'desire lines'.
Glen Instone				Pg 34: New Structural Woodland Habitat - this sounds good but what about existing habitat. We have deers and foxes who have this as their habitat today. There are also great crested newts in the pond that is on the boundary line. Have you considered this? The last time I checked and was trying to get planning in a neighbouring county then this was an issue. Pg50: A number of native hedgerows will be removed accoringing to your plans.
Graham Hadfield				Page 35 - As mentioned above, real life experience (as opposed to desktop modelling) demonstrates the inability of SUDS to drain the land, which suffers from waterlogging and flooding all the way through into the Acklam Grange School grounds. Page 41 – All wonderful in theory but real life isn't like that. Spending cuts mean that park maintenance is one of the first services to suffer. That is demonstrated in real life by the lack of maintenance of the “country park” along the A19 north of Mandale Road. The copses are a mixture of a neglected tangle of brambles &c which trap litter & attract fly tipping and the homes of drug/drinking dens with burned out furniture and abandoned shopping trolleys.
Helen McGregor				Man-made play areas and paths should be restricted to the housing area with the existing meadow and woods left for discovery of natural habitats.
Hilary Morley		Hilary Morley	Resident	Refer to chapter 1.0
Ian Etherington				Happy with this as long as it is delivered as promised.
Ian Rogers				4.3 Habitats and Biodiversity - The plans will destroy mature meadow that cannot be replicated. There are plans to alter Blue Bell Beck, but while manufacturing a 'more diverse' habitat you risk destroying that of many protected species! Has anyone actually studied what exists already or just done a desktop assessment?
Jacqueline Jones				It must be low density and light pollution kept to a minimum in the woodland areas
Jacqueline Young				How do you propose growing a man made country park
jamie barber				frankly, none of this should go ahead. you're planning to destroy natural green space and animal habitats that are at a premium in Middlesbrough.
Jean Franklin				Page 34 'Quiet areas' There will not be the possibility of quiet areas if a 7.5m 30mph carrying 120000 vehicles a day goes through the development.
Jenny McCullagh				How many years will it be before there is any wildlife in this area again? Once it's been demolished there will be nothing but houses for years. And it's very misleading to fill these plans with photographs of existing forests. Forests that have been there hundreds of years, not a few baby trees planted in an attempt to disguise the financially motivated plans.

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Jill Hadfield				<p>Page 35 - As mentioned above, real life experience (as opposed to desktop modelling) demonstrates the inability of SUDS to drain the land, which suffers from waterlogging and flooding all the way through into the Acklam Grange School grounds.</p> <p>Page 41 – All wonderful in theory but real life isn't like that. Spending cuts mean that park maintenance is one of the first services to suffer. That is demonstrated in real life by the lack of maintenance of the “country park” along the A19 north of Mandale Road. The copses are a mixture of a neglected tangle of brambles &c which trap litter & attract fly tipping and the homes of drug/drinking dens with burned out furniture and abandoned shopping trolleys.</p>
Joan Hornby	Member of the Friends of Blue Bell Beck (fobbb)			<p>I was unable to find Rodwell et al but did find Rodwell and Patterson 1994 Creating new native woodlands in Britain, I also found N Brown, Oxford Forestry Institute 1997, Redefining native woodlands.</p> <p>There seems to be some contention between these 2 studies “ the term native is misleading as it creates an arbitrary and ecologically unsound distinction “. How I understood this “ how do we know what would be native as humans and climate have had a huge impact over thousands of years “.</p> <p>With this in mind I think that the species of trees and shrubs that are already successful and flourishing in the conditions of Blue Bell Beck should be preferred.</p> <p>The oak, quercus robur, and hawthorn should have the majority of numbers, a higher ratio, over other species named in Rodwell. The oak and hawthorn are the very best for wildlife species numbers. Holly, rowan, crab apple, cherry and hazel in lower numbers will provide extra wildlife and human interest.</p> <p>These species are mostly recommended in Rodwell, it's the proportion/ratio of numbers that are important.</p> <p>There is no need to plant elder it will find its own way</p> <p>The existence of Ash dieback disease along Blue Bell Beck probably means that any ash whips will die.</p> <p>There is a scheme to propagate elm saplings from mature trees that have survived Dutch elm disease, also currently present in Blue Bell Beck, perhaps you could source some of these special elms and give them pride of place near the new visitor centre. Kudos for MBC.</p> <p>I would argue against “encourage dynamic process... wetland habitat to dry grassland “ this will promote the loss of habitat as most nature conservationists will agree, wetland has to be managed to maintain a diverse habitat. Otherwise the loss of ponds and wetland will mean a decrease in diversity.</p> <p>Random hedgerows across the meadows will create problems for the tractor/mower drivers. Existing wildflowers may be destroyed in the process of planting. Existing hedgerows and margins could be enhanced by appropriate shrubs. Planting new hedgerows alongside the new road is also an option.</p> <p>The council should take over the annual hay making as soon as possible as the FoBBB were unable to manage a cut in 2020 due to Covid-19 and poor weather. The FoBBB are unable to meet at the moment also due to Covid-19.</p>

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Joan Hornby				<p>The Mandale Meadow car park should have at least 20 parking spaces considering the pre COVID-19 use, sited on the meadow side of the road will mean dogs and people will not have to cross the road. Barriers will be needed on both sides of the road to deter unauthorised vehicular access.</p> <p>The areas on the lower Mandate meadow, in between groups of trees planted about 40 years ago, will be close to the road as it crosses the beck, please barrier these areas before construction begins to prevent the accidental straying of plant vehicles as these areas have species of orchids and other wild flowers growing there.</p>
Joan Pollak				<p>I feel that there has been little thought given to hedges in the plan. There are some quite ancient hedges in the landscape which are former field boundaries. As they are particularly beneficial to wildlife, providing valuable habitats and food for a variety of species, as well as improving air quality, it is essential to retain as many of these as possible and improve and extend the variety of native species.</p>
Joanne Mitchell				<p>The A19 has already been widened at some cost in terms of physical and mental health and the general well being of residents. Issues exist which require resolution regarding noise and air pollution and ought not to be made worse still by this imposition.</p> <p>The road would destroy the meadow.</p> <p>These sad plans they have to ruin one of the most beautiful parts of Middlesbrough not to mention the habitat of wildlife that will disappear without the meadow. Greenspaces are very precious and need to be kept as they are. Years of conservation and preservation work has been invested. People moved close to the meadow being given to understand it is protected, a greenbelt where no building or any development may ever take place. It may never have been a designated greenbelt but it was the subject of a Deed of Covenant, the land having been gifted to the people of Middlesbrough in perpetuity. A road is being planned through the meadow to service a housing estate.</p> <p>At the end of the day destroying a pristine meadow which is rich in biodiversity and has never been 'treated' with pesticides and herbicides, is nothing short of ecocide and vandalism and makes a laughing stock of the council's One Planet Living and green strategies.</p> <p>There was always another option, the 'developers' could create an estate loop road similar to the one at Trimdon.</p> <p>The council has stated they won't even pay local residents a penny in compensation for taking our public amenity from us and subjecting us to increased air and noise pollution, damaging our health.</p> <p>the disruption, destruction, the irreparable damage, and the blatant disregard for people who have lived here and who have been involved during many years in the care and preservation, and who will have their lives destroyed, just beggars belief. Residents already suffer the constant excessive traffic noise...and the pollution inflicted upon them. To think that there are those with the power who lack the empathy and the understanding, or thought and consideration for people who use the meadow and those who live close by is unbelievable.</p> <p>The residents in the Ayresome ward would no longer be able to access Mandale meadow from Mandale Road. It will be impossible for people in the Ayresome ward, including those of us who live a 2 minute walk from the current entrance to Mandale meadow is because it would involve walking straight across two lanes of the proposed roundabout. The road on OUR meadow stopping us accessing OUR meadow is the issue. All this nonsense about "you are getting extra greenspace" is an insult when you are planning something which prevents us from accessing OUR green space.</p> <p>The remarkable thing about this is some of the language used in the document to describe the proposed plan: "To create a diverse ecosystem" etc when one already exists that will be destroyed by the building of so many houses. The plan to create this "diverse ecosystem" on land which has been polluted by herbicides, pesticides and pollution from the building process while destroying a pristine meadow which is genuinely a diverse ecosystem containing Red and Orange endangered species marked on this map of Mandale meadow. They know this because the map and the legend were included in the Greenstuff response to the 2018 LP.</p> <p>The area is home to rare species flora and fauna, which have taken decades to fully establish, all of this would be destroyed.</p> <p>The faux manufactured country park, cannot, and will never, as if by magic, replace this.</p> <p>Mandale Meadow is well used and loved by many, an accessible place in which we are able to walk and relax.</p> <p>Residents currently suffer excessive noise and air pollution levels 24/7 owing to the proximity of the A19...how can it possibly be considered in any way acceptable to bring yet another road even closer to homes.</p> <p>When the A19 was widened residents received compensation for the loss in value of their homes..but despite the proposed road being routed even closer we are told we would not receive any such award for all of our losses. ..the loss of peace, and of mental and physical health. .. These of course can never be compensated for. It is not what we want.</p>

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				The notice delivered to each resident shows a basic outline of the area concerned, part of which is Mandale Meadow at the top of the 'plan'. The notice gives the impression that the entire area will be Country Park, along with which would come 1700 houses, not forgetting the so called Relief Road, to 'relieve' congestion on the A19 and serve the 1700 new homes. The plan actually shows the road through Mandale Meadow which would discharge traffic onto Mandale Road, at a junction adjacent to residents homes where the current car park is now situated, actually at the foot of their gardens.
John and Janet Roebuck				Amenity, Movement and Play p38 / LAP LEAP and NEAP Why has the Council failed to provide amenity provision in the initial phases of the development in relation to the 500 dwellings already developed to date, in light of their existing design and green infrastructure policies?
John campbell				Enhance existing meadow with more greenery to reduce carbon

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John David Duffy				<p>I recent visited Hardwick Country Park. Wouldn't it be nice to have the meadow extended and have this type of space right on our doorstep.</p> <p>I think everyone has been thought about throughout this section. Cycle routes, dog walkers, kids, fitness. You name it its there.</p> <p>We lose some trees from the development of the road, but this will be minor to the promise of</p> <ul style="list-style-type: none"> • 24000 new treess • 1.6km of new species rich/native hedgerow • 800 ornamental street trees • 300 fruit trees to be planted as part of public orchards <p>It is a vision for the many not the few. It is currently used yes, but 100s if not 1000 will use the new space. Please don't allow the few to stop what will really be something Middlesbrough can be proud of. Just like Marton have Stewart Park, we at acklam can trump them with Stainsby Country Park.</p>
Judith Harbron				No development
Judith Maunder	Friends of Mandale, Greenstuff, resident very local to Mandale Meadow			<p>p34, 4.1 the plans for landscaping the new housing estate are laudable except it looks like Albert Park rather than natural countryside. The paths are all very wide and too numerous to leave enough space for wildlife. You should not destroy the rich biodiversity which exists on Mandale and the rest of the open public space to satisfy potential residents of the potential new houses. leave the BBB valley as a semi wild natural green space.</p> <p>Quiet areas??? how with an estimate of another 4000 residents using the land plus everyone from south Middlesbrough seeing it is a potential dog walking/picnic area? it is already too heavily used for any quiet moments such as could be had before Covid lockdowns. People come from afar afield as Northallerton, Thornaby, Stockton, Hartlepool, Redcar etc. I know this because I carried out a survey. Th enew park will be hideously overused just as Stewarts park and Albert Park are. It won't be a natural green space at all.</p> <p>Some large trees need planting on the new estate as well as saplings or it will look like a concrete desert, which it will be really.</p> <p>p36.4.3, close mown grass is not good for wildlife and cannot be called 'diverse'.</p> <p>earth mounding of excavated spoil on Mandale meadow is an utter outrage. LEAVE IT ALONE. there is rich biodiversity on Mandale meadow. NO sports pitches on Mandale or the BBB valley. Keep them on the new estate if that is what people want.</p> <p>p39,4.5 a 4.5m (14'9") shared path next to a busy road is an extremely bad idea. A) it is dangerous for children and B) it is a serious health risk for all ages. It is also an unnecessarily wide path destroying even more of Mandale meadow.</p> <p>p40, 4.6 multi use paths are a bad idea. Close mowing besides the paths is a bad idea. No opportunity to watch wildlife as there is on the 'Fairy Path' or gravel path on Mandale - until some one decides it's a good idea to mow wide sections of the herbage next to the gravel path thereby wiping out countless butterfly and moth eggs as well as destroying the feeling of being in the countryside. at 1.8 to 2.4 metres the paths are too wide to enjoy nature. 1.8 is more than wide enough to accomodate wheelchairs and prams and some paths should be just 1m wide with no close mowing for people to enjoy nature close up.</p> <p>p35, map. it would be better to creat one very large pond/lake on the new estate rather than litter Mandale meadow with them.</p> <p>4.4 Recreational amenity creates something other than a 'country' park but trim trails etc in the new estate only is not a bad idea...although building a road through Mandale will damage the health of eAyresome ward residents, not improve it.</p> <p>p41, 4.7 putting walking/running tracks next to a heavily polluted road is an absurd idea and will further destroy Mandale meadow's rich biodiversity. Putting excavated spoil on top of Mandale meadow is an absolute NO from me.</p> <p>p42,4.8 LAPs and LEAPs may need proper patrols given what already happens to the play parks.</p> <p>p44, 4.12 we don't need or want public art and signage on Mandale meadow. We want it leaving as the richly biodiverse meadow which it is.</p> <p>p45, I am in favour of thicker native hedging, thin stand alones will not be helpful to wildlife so I would like them to be as thick as possible.</p> <p>p46, green flag status. I can understand the need for some people to get national acknowledgement and status but his doesn't have to be acheived by creating air and noise pollution for Ayresome ward residents whose wishes are being completely ignored, nor does it require bringing serious traffic congestion to Ayresome ward residents. Bringing public transport into our green space, Mandale meadow, will have serious consequences for residents. HEALTHY, SAFE and SECURE it will not be for Mandale. No other 'country park' has a huge road running through it.</p>

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				<p>p48, I hope those aren't roundabouts on the Mandale meadow??? the legend for the map is not clear on my pc.</p> <p>p50, 4.17 it is difficult to decipher the map. It looks like an extra roundabout inside the meadow</p> <p>There is nothing left of the meadow with all the structures being included and it makes the meadow and 'country park' accessible to Travellers and drug running off road bikers again. The only 'significant' thing about the 'northern gateway' is all the damage it will cause and all the nuisance to residents.</p> <p>p51, nothing left of the richly biodiverse meadow in that ghastly plan.</p>
Julia Povey				We can't control what happens on privately owned land but there should be NO ROAD THROUGH MANDALE MEADOW/WOODS.
Julie Riley				NO ROAD THROUGH MANDALE MEADOW
Kabaser Ajaib	Retailing	Kabaser Ajaib	Retailing	No through road
Keith Russell				No road through Mandale Meadow
Keith Young				No road through Mandale, you can do what you like on the private land
Laura McGregor				<p>The proposed landscaping of the current Maldale Meadow will damage it's current role in providing for wildlife and educating children in natural landscaping. Perfectly mown lawns and play areas are not educational.</p> <p>Native tree species should be planted in rows down streets to prevent streets from looking bare. As per the top image in section 7.13.</p>

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Lauren Teate				<p>Page 44. Chapter 4.1 - "Trim Trail & Dog Walking, running and walking routes, including areas that promote physical activity in a green environment and access for all." Will the roads be fenced off to provide a safe place for dogs to be walked? How will you stop dog/wildlife conflict? Will there be secure fields in the new estate for people to allow their dogs to be walked off lead safely and prevent conflict between recreation users of the park and dogs? Will there be bins provided for disposal of dog waste? These are things that MUST be considered. The spine road poses a risk to dogs, children and wildlife. There must be safe crossings put in place and the road must be secure so that dogs, wildlife and children cannot run onto it.</p> <p>Page 35. Chapter 4.3 - If this was all put into place, this would certainly benefit wildlife and nature, however, it cannot become like Albert Park. There must be wild areas left untouched and the pathways need to be reduced to allow space for wildlife to thrive undisturbed. I would suggest areas that are completely sealed off from the public so that children and dogs cannot trample or disturb wildlife. There is a herd of roe deer that currently use the farm fields as a means to escape from dogs and people. With this being developed and the proposed spine road cutting off their escape routes, there will need to be somewhere for wildlife to be safe from disturbance. This would also benefit ground nesting birds such as Grey Partridge, Skylarks and Curlew that already use the agricultural land to breed.</p> <p>Page 37 - Chapter 4.3. The scrubland/tree planting seems to encroach onto the existing wildflower meadows. I disagree that this should happen. These are already species rich, however scrubland/woodland would certainly be beneficial around the edge of the existing meadows. I do not think there needs to be more cut grass areas than there already is on this site. Ornamental and "parkland" trees are not needed along the streets. There are many native trees that are beautiful to look at and equally beneficial to wildlife, such as Rowan and Hazel, Horse Chestnut and Hawthorn. 300 fruit trees does not seem like very much. Certainly not enough to create an "orchard". Why not replace the ornamental trees with fruit trees, which benefit everybody including the wildlife.</p> <p>Page 39 - Chapter 4.5 - Why do all of the dozens of paths need to be so wide? Again, this shows that nature and wildlife is placed last. The meadowlands are delicate places and house numerous species of flora and fauna. Tarmac paths at 3.5m wide, cutting through and around the meadows is not benefiting anything but people. This is ridiculous and completely at odds with chapter 4.3. This shows that it will become another Albert Park with nothing but squirrels, pigeons, crows and ducks living there. Bluebell Beck is special because it isn't paved and because nature has been left alone to thrive. Leave these paths out and make it accessible around the edges of the meadow. Put the wildlife FIRST and the people second. This site cannot sustain hundreds/thousands of visitors every day, which seems to be the intention of this "countrypark" so far. The local residents do not want to have to deal with the noise and mess of these visitors. In chapter 4.3 there was mention of "quite places to watch wildlife", however I see absolutely no place where this can be done. The entire place is being paved and the "feel" of being in the wilderness completely obliterated.</p> <p>Page 40. Chapter 4.6 - The 3.5m tarmac paths will also be mown on either side, making these paths even wider and destroying yet more habitat instead of leaving the edges to re-wild as has already happened along the Bluebell Beck Nature reserve area. Then there are gravel paths, mown on either side and lastly mown paths. Where exactly is nature in amongst these pathways? There is no space left for the meadowland apart from in tiny sections, which will be trampled by children, bikes and dogs. There is no mention of preventing off-road motorcycles, which will gain access to the site using these very wide, smooth pathways. Again, this is not in keeping with preserving existing habitats, as mentioned previously.</p> <p>Page 42/43. Chapter 4.8/ 4.9. The LEAP and NEAP overlap and spill over to Bluebell Beck. This should certainly be somewhere for children to visit but there should not be play equipment installed on this site at all. The paths will be taking up far too much space as it is without more space being destroyed to make way for children play equipment. Again there is no mitigation for the disturbance of wildlife nor conflict between dog walkers and people. If there is nowhere quiet for dogs to be walked without children, people, cyclists etc, there will be significant problems. Many people in the surrounding areas own dogs and use this area to walk them. There are also many dog walking businesses using this area and there needs to be a place for these dogs to walk off lead without the worry of conflict with children and cyclists. There is nowhere else in Middlesbrough that this can be achieved, hence why it is so popular with dog walkers. These proposals will cause huge problems for businesses and may even result in dangerous conflicts. This site is not large enough to accommodate all that is being proposed. Nature and wildlife is being shoved to one side in order to pander to the developers notion of "living within a park". There should be no facilities built on Bluebell Beck side. The facilities and play areas must be kept to the new estate side only.</p> <p>Page 44. Chapter 4.12. There needs to be an address to the light pollution that will be given off in these "well lit" areas in the new estate. There are several ways that this can be done, from motion sensor lights to lamposts that only shine downwards. Light pollution is already and huge problem from the A19 and Teesside Park in the area.</p> <p>Page 45. Chapter 4.13. Hedges and fences must be maintained properly. Hedge trimming along the wilder areas must be kept to an absolute minimum to allow flowers berries to grow and birds to forage.</p>

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				Roadside verges need to be planted with native wildflowers and not for aesthetic reasons. Plants such as clover, daisies, dandelion, plantain, vetch, cornflower and poppies are extremely valuable to pollinators. Children/youths have a tendency to burn anything that is wooden and this needs to be kept in mind when placing fences. Already Bluebell Beck has had the bridges burnt down and replaced with metal bridges.
Leah Davey				This housing allocation is not necessary and should stop. Any 'country park elements' should remain within the site already under construction and NOT damage the area on Mandale Meadow, Newfield Meadow or the connecting green spaces by the addition of landscaping features, footpaths, cycle paths, play equipment or 'desire lines'.
Liam Harris				Please see sheet 1
Linda Barker				This housing allocation is not necessary and should stop. Any 'country park elements' should remain within the site already under construction and NOT damage the area on Mandale Meadow, Newfield Meadow or the connecting green spaces by the addition of landscaping features, footpaths, cycle paths, play equipment or 'desire lines'. This site allocation will cause SIGNIFICANT detrimental impacts to the existing wildlife site at this location.
Lynn Blagg				Leave site as it is to enable residents and others to use it in its natural state for exercise and pleasure and avoid destruction of wildlife habitats and ageing flora and fauna and avoid excess traffic in the area.

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M. E. Bailey				<p>*p34, 4.1. The landscape is far more important than the built environment in this proposal. We have Climate Emergency and the rapid decline of wildlife to be concerned about. All building construction pumps carbon into the atmosphere and MBC has signed up to becoming carbon neutral. More than 53% of the land needs to be green and allow nature to grow and flourish and wildlife needs some 'wild' areas in order to do that. The recent sad sight of a baby deer running frantically around the streets of Thornaby searching for its mother tells us we have already destroyed far too much of the natural environment with endless house building. Middlesbrough needs to set an example and put nature before profit. 'Ensuring the Country Park permeates the development in terms of its character throughout the site'. I strongly disagree with this statement. The existing public open space can be left as it is and the new development can blend seamlessly into the natural landscape already on the Bluebell beck valley and mandale.</p> <p>There is no need for play parks or other man made structures on the existing open public space which local residents love as it is. If people can't find their own routes across the existing open public space perhaps a country park setting is not right for them. There is no sense of adventure or exploration in an area with endless paths. These plans for multiple paths cause destruction and carbon release. Most, if not all, of the paths are too wide to allow for a sense of quiet or solitude or a oneness with nature. Very narrow natural paths caused by people exploring, such as 'the fairy path' on Mandale, are the best kind of paths for solitude and quiet. A multitude of man made paths is very constricting and unnatural.</p> <p>*p35, 4.2. SuDs need to be more carefully thought out than they have been on Mandale as the Mandale SuDs floods in winter and dries up in warmer weather, leaving wildlife such as tadpoles and invertabrates in the flooded areas to die in isolated mud puddles which soon turn to hard, dry clay. The Mandale SuDs should be enlarged and made deeper in the central area to prevent the unnecessary death of wildlife.</p> <p>*p36,4. 'facilitating the movement of wildlife throughout the site' The green corridors need to be much wider than they are on the plan to allow wildlife movement throughout the site.</p> <p>'The woodland type' only native deciduous trees are mentioned but there needs to be some native evergreens included in the planting for the easement of traffic noise pollution such as English yew, holly etc. Hornbeam should also be planted as it attracts a moth whose only habitat is hornbeam.</p> <p>' In other places new hedgerows will be planted to break the monotony of the long sewer easements' please do not apply this to the existing open public greenspaces, we love our well established and natural meadows which have occurred without man's interference! The large meadow on Mandale is much loved and attracts a vast range of insects which are vital pollinators and provide food for the small mammals, birds, damsels and dragonflies. You can't improve a natural landscape.</p> <p>'with naturalistic earth mounding using the construction spoil'. DO NOT put construction spoil on Mandale meadows, it will despoil the environment. Dumping it on Mandale will destroy the existing flora and fauna there. Use it on your new development. Instead of relying on Tees Valley for ideas you should consult with Greenstuff admin as they know the land and its flora and fauna best.</p> <p>*P37, 4.3. The landscaping plans, apart from paths being too wide and numerous, are fine except on the existing open public green space which should be left as it is. Public orchards are a lovely idea but would be better planted in deprived areas as a source of free fruit.</p> <p>*p38, 4.4. Recreational play for children is better served by natural greenspace than man made structures as it develops the imagination, peer friendships and co-operation, conversation and creativity. Today's children have far too much organized activity in terms of electronic gadgets, they need natural green spaces in which to develop physical skills and learn their body's limits.</p> <p>*p39, a 4.5 metre path through Mandale meadow and the beck valley is far too wide and destructive. 2.5m and 3.5m paths are ridiculously numerous and too wide, they need reducing in size and number and kept to the new development with the exception of one narrow, dedicated cycle path on the existing open public space. I'm not sure why 'potential entry points' are shown on the map as places like Mosswood Crescent already have access to the existing open public space.</p> <p>*p40, 4.6 as above, reduce the width and number of paths and only one, narrow, dedicated cycle path through the existing open public green space. Informal paths should not be gravelled or their edges close mown as that causes the loss of informality and the feeling of being close to nature. 'Desire line' paths do not need to be mown, they occur naturally through the footfall of wildlife and humans.</p> <p>*p41. DO NOT dump excavation spoil on Mandale meadow, calling the spoil 'landforms' does not detract from the fact you are dumping waste on a meadow and killing off a large area of meadow. Use your building spoil where you remove it, i.e. on the new housing estate. Residents do not want your rubbish dumping on them and scarring the landscape. NO ROAD through MANDALE.</p> <p>*p42, 4.8 please keep LAPs and LEAPs in the new housing development and not on the existing semi wild, public open green space. There is ample opportunity to play and stay healthy in the existing green space, local children play all sorts of imaginative games and stay fit and healthy on our natural green space. Man made paly areas tend to get trashed by teenagers.</p>

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				<p>*p43, 4.9 the first LAP pictured on this page is too small for a play area, it needs to be at least 5x as large. The NEAP shown at Houston is a good example of how to use excavated spoil on the new estate, and with encouragement, young children can use such equipment as an acted retelling of a story such as The Billy Goats Gruff as well as being fun for slightly older children. The LEAP shown in Rome is not adventurous enough. The mounds could be higher for sledging down in the winter for example. again, keep LAPS, LEAPs and NEAPs on the new estate.</p> <p>*p44, 4.12. Public art and signage is not needed on the existing open public greenspace although some more informative signs next to the gravel path would be a good idea as the old signage is worn out and fails to show much of the wildlife we have in the area. Interestingly, MBC are fully aware that we have water voles in and around the beck as the signage says so. Art doesn't lend itself to natural spaces as it is obviously a man made structure.</p> <p>*p45, Hedgerows are welcome habitats and provide both shelter and food for wildlife, the denser planted hedges are best to provide more shelter and safety for wildlife. 4.13 please leave the existing open public green space exactly as it is other than some hedging by the Mandale metal fence which should stay in place to prevent entrance to Travellers and off road drug runners bikes.</p> <p>*p46, 4.14. The problem with the 'vision' of the country park is that it is too manicured and formal, which is the opposite of what MBC claim is their intention. Leave Mandale and the BB Valley as they are - richly biodiverse, semi wild natural green spaces. Where the council see a beautiful country park all we see is another town park such as Albert park, too manicured, too manmade to be a beautiful rural retreat.</p> <p>*p47, 4.15, retaining the existing open public semi wild green space as it is, is much more important to residents than green flag status.</p> <p>*p48, 4.16. Leaving the existing open public green space as it is with NO ROAD through Mandale, just a loop road through the estate, means the manicured 'country park' will flow into the semi wild existing area and make a pleasing change to the heavily manufactured park.</p> <p>*p50, 4.17, there is no need for 'an indicative gateway' at the northeren end, i.e. Mandale meadow as it is well known and well used. Just don't build the road through the meadow and save a lot of money. Residents don't need or want an increase in air and noise pollution. The combination of road, 4.5m path and all the planting and dumping of construction spoil on Mandale is not wanted. The 'naturalistic earth mounds' created from excavated spoil will not grow healthy trees and shrubs. Central reservations will not be needed if there is no Stainton Way Western Extension. No road through Mandale, just a loop road through the new development.</p> <p>*p51, it isn't clear where the SuDs etc in the map on p51 are going to be, hopefully not on Mandale meadow. All the grasses and plants in Mandale meadow are vital for wildlife. Councillors may just see long grass on a map, it isn't what residents and wildlife see.</p> <p>*p52, the 4.5 m multi use path is far too wide for a rural country park look.</p>
Malcolm Chilvers				Like many people I have neither the time nor inclination to be able to read and absorb the information contained in all these chapters unlike yourselves who are being paid to do it.
Malcolm Watson				It is interesting that under the heading "Country Park Essential Criteria Design Check" in Section 4.14 there is no mention of the two bullet points which it seems to me are the key i.e. the ability to draw "a clear boundary - marked on a map" and "no more than 5% of the area built on". Intentional omissions like that seriously reduce the credibility of the document please also see comments in attached file

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Malcolm Watson				<p>Is there a map showing the boundaries of the country park ? There must be, because it has to be "defined by a clear boundary – marked on a map, whether it's open or fenced in" (ref Essential Criteria for a Country Park). It cannot be a map of the whole development because, although paragraph 1.3.2 says that "the new Country Park will comprise over 53%of the total development area", 3.0 says "An equivalent area to over 53% of the total site is to be retained as green space" i.r. they are equating "green space" with "country park". These two statements are incompatible as a country park has to have "no more than 5% of the area built upon" (ref Appendix 2 - Accredited Country Park Criteria).</p> <p>Separately, we have been told that "the Country Park would contain... 100 acres of new green public space" (Andy Preston, 31st Jan). By drawing a polygon on the area using Google Earth I can see that the area of the existing publicly-accessible land from Trimdon Avenue to Mandale Road, and up through Foxes Wood towards Brookfield is very close to 100 acres. So does the "new" in "100 acres of new green public space" merely relate to re-designating the existing public area as "Country Park" ?</p>
Martin Allen				<p>Section 4.2 SuDs. Excellent to see this included as an integral part of the development, however I've yet to see a best practice example of SuDs in M'bro but important that we keep trying. The SuDs in the adjacent Kingsbrook Wood Estate was so poorly designed that I had to complain twice about it as the level of silt entering the beck was so grim: Because it was so poorly engineered the problem was made substantially better using bales of straw which says a lot for the ability of the designers/engineers or perhaps the landscapers misunderstood the instructions.</p> <p>Also, the upright metal bits are truly awful – it really should be possible to design SuDs without something as intrusive into the visual landscape. My concern here in the Stainsby Country Park is that even if the overall design is clear and exciting, the developers will still manage to make it look crap and no I don't know how to stop them and evidently neither do the council judging by the above photo, but I'm flagging the issue up. I would also add in the photo above the landforming looks really municipal and artificial; if the aim is for a Country Park then you need to find a way of preventing the developer from doing what Kingsbrook Wood Estate have done.</p> <p>On terminology, a stream is continuously flowing and in gardens a rill is usually formal and stone or concrete lined; what is proposed in the Country Park design is a ditch that will fill and only flow when rain falls on the proposed new housing area. Call a ditch a ditch and build it like one; having areas of the ditch to fill with water was a system we have used at Wynyard Woodland Park simply by half blocking existing ditches at intervals and I have found it slows the flow well.</p> <p>Where the SuDs is eventually discharged into the becks I would have liked to see a more diffuse flow – perhaps through a marsh, or in the manner of the wet flushes we get on the beck valleys (I think mentioned in the Maltby Beck LWS), planted with say marsh marigolds/watermint/iris/meadowsweet rather than the direct entry via a piece of concrete surround that I imagine is likely to be undertaken.</p> <p>(See photos)</p> <p>Or even use a landscaping method that mimics a rural drainage entry into the beck (like the way a farmer would put a ditch to drain into the beck) – anything would be better than the concrete at Kingsbrook Wood Estate (above) which just urbanises & cheapens the Country Vision you are attempting.</p> <p>I was puzzled by the phrase "SuDS help to deal with surface water on site, prior to discharge into the local sewer system of Blue Bell Beck, helping to prevent flooding and helping to improve water quality." which reads that the beck is a sewer system to me but perhaps something else is meant.</p> <p>It is possible that in the past there was a large pond (medieval fishpond? Image from LIDAR on the National Library of Scotland maps) comparable in size and in a similar spot to the proposed large SuDs pond to the far north of the site. The Norfolk Pond Project https://ghostponds.wordpress.com/ have had very good results simply digging out ghost ponds and letting them revegetate from seeds in the soil seed bank and that might be an option in this case.</p>

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				<p>Some of the ponds are shown in the imagery of the Masterplan as full and with dipping stations. I didn't see any notes about the soil type where those ponds are (maps I can find suggests clay which is good) but do note that the two ponds previously dug near to the Bluebell Beck are rarely full with water and frequently dry in the summer.</p> <p>And I think that one of the fields previously had an area dug for sand which has since been refilled and I think is shown on your aerial photos as a dark circle below but can't find my ref. for it.</p> <p>If you want to see some biodiversity SuDs ponds adjacent to water-vole habitat, albeit for beck overflow flood alleviation rather than sub-urban drainage, then we created some in 2018 at Six Fields in Hartburn which is open access.</p> <p>Do also bear in mind that any pond that has public access will be virtually useless for pondlife because ponds become turbid with silt when dogs swim in them & they always do...unless they are out of sight or inaccessible via a ditch or too far from the path.</p> <p>4.3 Habitats and Biodiversity</p> <p>There is far too much tree and hedge planting adjacent to the becks – I cannot see any habitat benefit to it and do wonder whether it is included as tree planting is publicly popular and the mayor has targets in terms of tree-planting numbers he wants to meet.</p> <p>At the north east of the site on Bluebell Beck it impinges on the important habitat for water-voles of which Middlesbrough beck valleys are a stronghold in the Tees Valley. Some works were carried out in the area for benefit of water-voles in 2013 https://www.teeswildlife.org/what-we-do/past-projects/living-waterways/1961-2/ (scroll down) and you might want to think of a way to adjust your plan so it complements the works that occurred in 2013.</p> <p>To the north it is wrong to plant trees/shrubs on the Bluebell Beck Meadows, part of the Bluebell Beck complex LWS, and I think there must have been some sort of error or mis-understanding for such a poor design decision given the quality of the rest of the plan.</p> <p>Bluebell Beck Meadows are the only piece of National Priority Habitat Lowland Meadow that has been surveyed as MG5 in Middlesbrough (details of the letter I wrote to the council in 2012 regarding this are given at the end of this document) and given Natural England estimate there is only 6000 ha of this habitat in England (compared to over 200,000 ha of ancient woodland) then every effort should be made to conserve it and extend it however small it might be currently. Additional tree planting around any of the meadows will shade and diminish what exists there currently. Utterly bewildered how this part of the plan could have passed through Middlesbrough's planning department and the design agency without anyone questioning it, but I know planting trees on species-rich grassland is still occasionally occurring nationally as it has been highlighted on social media, particularly that Forestry England and the Woodland Trust in one case should have known better. Anyway, the proposed plan goes against 1.4 Planning Policy Context and Existing Site part k "enhances the Local Wildlife Site in the north of the site..." by demonstrably damaging both the grassland habitat and the habitat for the water-vole so you will have to comprehensively rethink that part.</p> <p>The MAGIC site is unfortunately out of date in some respects, grassland being one, and the Tees Valley Local Nature Partnership has written to Natural England to explain this. I have recently written to Defra regarding the Lowland Meadow layer in the Tees Valley & blogged here https://theintermingledpot.wordpress.com/2021/02/16/losing-lowland-meadows/.</p> <p>The design as suggested in the Masterplan gives no access to many grassland areas for machinery to maintain (cut and remove hay) the meadows here. In addition, the blocking of sight lines with hedgerow/young trees and the enclosed small glades will make women and older people feel unsafe walking through the area – especially given the speed at which an attacker could leave the area via the access road and the current problems with evening activities at the existing car park.</p> <p>I would not recommend any tree planting on any of the land owned by Middlesbrough Council other than the occasional specimen tree or an avenue to line the access road. There is no habitat gain from it, only detriment to existing more important habitat and priority species. The plan as proposed would make it almost impossible to maintain the lowland meadow habitat successfully due to insufficient access and the whole area would feel unsafe due to poor visibility from paths.</p> <p>Alder carr is a continuously damp or wet habitat and would require lowering of the soil levels so either Bluebell Beck flows across a wider floodplain or to be planted at least somewhere that acted as a filter of the SuDs. We are possibly more of willow carr area with there being so much crack willow on the becks, but perhaps with a leaky dam across the beck so it floods the lower levels when rainfall is high. Any increase in wet woodland in the Tees Valley is a good thing but I just can't see how the woodland type mentioned fitted into the landscape proposed.</p> <p>The issue of safety applies to the planting on the private land part of the masterplan too, especially alongside the beck. My personal preference would be to provide a park-like setting with semi-mature standard trees placed around an open landscape of species-rich grassland which can be closely mown or left longer to flower as desired. In Middlesbrough residents tend to dislike shrubs (thinking they collect the litter or attract rats)</p>

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				<p>and where trees are planted prefer them open underneath/visible through the clumps, as at Fairy Dell Park (photo below showing meadow, mown grass, new open woodland and play equipment at Fairy Dell) or more as dotted trees on a grassland landscape as at Albert Park.</p> <p>I personally would prefer some broom or gorse bushes dotted about amongst the species-rich grassland as a reference to the historic shrubs (yes I know my previous comment was against shrubs) that grew in the area as noted in the Lordship Map field names Broom, Broom Hill and Acklam Whin (whin being another name for gorse) but I appreciate that they are both easily flammable shrubs and so may not be desirable. They would however be an important habitat to add to the site in small clusters set within the open areas of species-rich grassland.</p> <p>The amount of species-rich grassland to be created is to be greatly applauded and far more unusual and important than the tree planting mentioned – I can't think I have seen another design where that vegetation is proposed across a site in such an integral manner. Combined with the similarly comprehensive SuDs it ought to be seen as pioneering and inspirational for new developments and retrofitting public space in old developments. Obviously, I would prefer the seed for it to be from local sources (within the Tees Valley) as this helps manage Middlesbrough's (and the Tees Valley's) existing native grasslands (from which the seed is taken as green hay) and promotes a local floral identity as well as being best practice in conservation. I would prefer the plan to mention that the species-rich grassland must be created on subsoil as this gives the best chance of successful wildflower growth, keeps the vegetation slow growing, and importantly (I think) the management costs low. Perhaps it could be written into the plan that developers must consult/work with the Local Nature Partnership?</p> <p>A good example of this type of open species-rich grassland with patches of scattered scrub (hawthorn and dog-rose) can be seen a Green Vale LNR in Stockton where currently it is cut every five years but I hope to get it cut (at least in part) most years so we can use the seed for making new grassland in Stockton Council's public areas to give biodiversity gains and associated cost savings.</p> <p>And for mown road verges a good example at the Hemlington roundabout.</p> <p>The woodland planting along the boundary with the A19 is all very sensible and practical – could even be brought adjacent to the road edge where that is possible to make a change from always having grassland at the edge of the road.</p> <p>4.5 paths – there do seem to be a lot of paths shown and I wonder where there is quiet space for the wildlife – perhaps people don't need to access every pond or every stretch of beck.</p> <p>4.7 Informal Amenity - I don't think wildlife hides are a good idea – perhaps use the idea of a wildlife screen (as opposed to an enclosed hide) which can be seen by the lake in Fairy Dell (sorry I don't have a photo).</p> <p>I liked the rationale of landforms combining sound pollution dispersement with a bike track – we used to have something similar for bikes by Marton West Beck in the 70's. I didn't understand the landform placement elsewhere around the site unless the designer felt there ought not to be any blank space in the design – they can be difficult to manage when covered in grassland so you really need to be sure they will have a social or biodiversity benefit.</p> <p>Bins need to be placed where there is quick access for emptying otherwise it adds to running costs – preferably don't have bins and ask people to take their rubbish home – given most people will live in the adjacent housing this is not a big ask.</p> <p>See comment in the SuDs section on issues of full ponds and positioning of dipping platforms.</p> <p>4.12 Signage etc. – from that map there is so much dotted around that it doesn't feel like countryside – is that the intention? It's not something I personally like and it may be better to design a phone app that tells you things as you walk around the site – i.e. wildflower walk, tree walk, history walk, etc. such that there are fewer physical objects where future maintenance/repairs/dealing with vandalism has to be thought of and as people will mainly live locally they won't want to read the same sign post every time they go for a walk nor will they need a map.</p> <p>The comment on well-lit routes is important point and brings up the issue of light pollution that the new development will cause – ways to minimise the effect of light pollution on moths bats etc should be designed into the scheme.</p> <p>In 4.15 Green Flag there is a mention of ancient trees. Whilst not yet of ancient status there are some large (for Middlesbrough) Ash trees along Saphwood Beck which I measured twenty odd years ago but unfortunately have been unable to find the records or go out and re-measure. I'm flagging it up here so that someone is aware & also if there is more public access in the area then the effect of any ash dieback should be on someone's health and safety agenda.</p>

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				Given the council is offering to take over the management there absolutely should be an underlying design rationale throughout to minimise maintenance costs and maximise biodiversity benefits of any public spaces. Especially so given the legacy costs of poor landscape design choices in the past for Middlesbrough and the lack of money we have as an area generally. Generally some of the design ideas shown I felt were on the fussy/urban side for a rural park and may be expensive to maintain. The expense of maintenance is so important.
Matthew Shippey				Don't want this. I walk in mandale meadow. Middlesbrough is lacking in green space and you want to build a road through to facilitate new housing. Middlesbrough Council you are a disgrace.
Michael Miller	Natural England			please see general comments under Chapter 8 comments.
Michael Starford				Concern that the country park landscape should appear natural and established rather than contrived for first appearance.
Michelle Hedger				No road through Mandale Meadow. Use the loop road option. If a road needs to be there make it a narrower road of 20mph. My children cannot play near the huge 30mph road planned. They can play now in the meadow. They will be hit by a car in this 'country park'. Please don't put all the footpaths in. We like to follow the natural winding dirt tracks. There is only lip service paid to these. We don't want artificial paths. It's beautiful, muddy and filled with nature.

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Mike Conyard				Do not build on this land. This is much needed green space. There are no parks in this area and nowhere for people to walk, kids to play and people to live their lives. I find it disgusting that you propose to build on this beautiful piece of land. Also, who wants to live right next to the A19? These house will be worth literally nothing.. The land is also a buffer zone from the A19 to the residents of Heythrop Drive and beyond.
Miller Homes and Avant Homes	Miller Homes and Avant Homes	Mr Neil Morton	Savills	<p>4.1 Landscape Strategy & Masterplan – the masterplan notes that public consultation has shown concern over the loss public open space and the open countryside, however it should be acknowledged that the site is now allocated for development and located within the limits to development. As the masterplan goes onto acknowledge, the majority of the site is private agricultural land with no public access and relatively low levels of biodiversity.</p> <p>As a general comment, we note that many of the green fingers shown by the masterplan between and around Residential Zones are flanked by the proposed secondary road network meaning that residents would need to cross roads to access many of the green spaces. Further consideration should be given to whether this represents the most appropriate arrangement in terms of the accessibility, usability and safety for all of the Green Zones.</p> <p>The masterplan then moves on to set out more detailed expectations in terms of landscaping provision within the allocated site. It is at this point where we are concerned that the masterplan becomes overly detailed and prescriptive, and goes beyond its remit as design guidance. For example, the 4.16 Summary of Landscape</p> <p>Strategy plan at p.48 illustrates the significant level of detail set by the masterplan which in our view goes beyond the remit of design guidance. Furthermore, we consider that the masterplan seeks to introduce some elements which exceed what is required by the development plan. Again, whilst our clients are fully committed to delivering a well-designed community at the site and the ambition of the masterplan in this regard is acknowledged, it is not the role of the masterplan to set the site requirements bar at a level which is not justified. For example, 4.8 Play and Sports Provision sets out the proposed approach towards provision across the allocated site. The plan at p.42 appears to suggest an overly onerous approach to provision with the plan showing 1 NEAP, 3 LEAPs and 20 LAPs as well as MUGA and playing pitches/pavilion. P.43 goes onto set out detail on the characteristics of the play provision. Clearly the detail of these matters will need to be examined and determined at the planning application stage.</p> <p>4.12 Signage, Wayfinding and Public Arts and 4.13 – Boundary Conditions to the Country Park – our general comment applies that the level of detail prescribed by the masterplan goes too far.</p> <p>4.14 Country Park and 4.15 Achieving Green Flag Status for Stainsby – we acknowledge the Council’s ambition to achieve Green Flag status for the country park and our clients are committed to delivering a high quality, well designed community and doing all that they can to assist the Council in achieving this. However, care must be taken that this ambition does not translate into the masterplan setting requirements on the site which are not justified in planning terms.</p>
Mrs D M Hornby				<p>Wildlife</p> <p>It is known that deer, fox and other mammals, bats, owls, tree sparrows and many others, many insects, plants can be found on and in the present meadows and woodlands, several of which are on red and orange endangered species lists. We do not have the right to condemn them to disruption, starvation and extinction.</p>
Nichola Davey				<p>This housing allocation is not necessary and should stop.</p> <p>Any 'country park elements' should remain within the site already under construction and NOT damage the area on Mandale Meadow, Newfield Meadow or the connecting green spaces by the addition of landscaping features, footpaths, cycle paths, play equipment or 'desire lines'.</p>

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Nicola Smith				<p>We DO NOT WANT IT!!!!</p> <p>WHY can't you build on areas where you have flatted hundreds of houses but not rebuilt? Keith Road or Marton Burn Road??? This is the only green space we have in Acklam and hundereds of residents rely on this being walking distance. Pre covid the traffic on mandale Road, Acklam Rd and levick Cresent is bad enough at peak times without further traffic. You have Acklam Grange Secondary school round the corner from the meadow with 1200+ students that also use these roads. This is already a very busy junction without diverting more traffic to it. What about adults and children's mental health?? Has the past year taught you nothing?? I think having a open greenspace with all the wildlife and plenty of room to exercise and get much needed fresh air and been an absolute saviour to hundereds of local residents. This project has not considered anything but money. Looking at the plans there will be very little green left and it will be surrounding a road and housing estate. Absolutely nothing like we have now. This is NOT ACCEPTABLE FROM ANYONE I KNOW WHO LIVES IN ACKLAM 😞😞😞😞😞😞😞😞</p>
Nicolas Mitchell				<p>This where I think extreme care is required with the landscape and the term "country park". It doesn't need too many concrete walkways and cycle paths or it will look like another new town with some green lawns. More tree planting will help, especially for trees lost by the proposed road.</p>
Peter Davey				<p>This housing allocation is not necessary and should stop.</p> <p>Any 'country park elements' should remain within the site already under construction and NOT damage the area on Mandale Meadow, Newfield Meadow or the connecting green spaces by the addition of landscaping features, footpaths, cycle paths, play equipment or 'desire lines'.</p>

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Poppy Bowyer				<p>How do you plan to create a native woodland? They can take around 80 years to establish from young standards.</p> <p>Is there someone with the qualifications to ensure the right species, size and age are selected, planted properly and are fit for purpose?</p> <p>Who is designing and managing the execution of the planting scheme to ensure instant impact and longevity?</p> <p>What plants will be incorporated the various habitats to ensure optimum meadowland/wetland creation?</p> <p>Is there someone to ensure that these habitats are created to the standard promised?</p> <p>How often will, the crucial habits, the glades and meadows be mown?</p> <p>Will there be any planting within the waterways?</p> <p>What species will be used for the hedgerows?</p> <p>Is there a plan for management of the orchards?</p> <p>Will the local community receive training in fruit tree pruning to be able to get the most from their orchard?</p> <p>Construction spoil is notorious for looking awful is there a plan to tidy it up with biodegradable mulch matting or sprayed seeds?</p> <p>Is there any danger to foundations or from erosion with construction spoil? Is it something future buyers may have to consider?</p> <p>An 'unobtrusive road' is very hard to achieve in a built up area. Roads and high footfall in general tend to have an extremely negative effect on improving or sustaining wildlife and habitat. Is this something that has been thought about long term?</p> <p>Will management of the estate be an additional cost to buyers?</p> <p>This design idea seems very innovative. What makes you think it will work here?</p>
Rob Atkin				No road through Mandale.
Ronald Dennison				Looking 20 or 30 years ahead, a 140 hectare Country Park appears to be a very exciting and very much needed venture. As the population of South Middlesbrough has increased, it has become apparent that the existing open spaces with public access are too small - both individually and collectively.

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Simon Grundy	Stockton on Tees Borough Council			<p>Additional clarity could be provided on potential public rights of way improvements linking into this strategic area. Section 4 (p.39) of the document identifies a linkage into the site via a footbridge over the A19, which links to Allensway / Thornaby Town Centre and beyond. The link provides an opportunity to enhance sustainable transport opportunities for residents in the estate.</p> <p>In the opinion of officers of this Council, this footpath should be safeguarded and could potentially be upgraded in the future, should funds be available. However, it is unclear whether the updated masterplan (p.13) will safeguard the route, and whether opportunities to enhance the route have been integrated into the framework or the associated infrastructure delivery plan. Clarification on this matter would assist this Council to understand any potential implications for the public rights of way network in Stockton on Tees Borough.</p>
Sofia Torre				4.1 great idea, but don't build houses or a spine road do it for the surrounding community
Sonia Bowler				<p>Page 34 That green area is not large enough nor far enough from the A19 or A1130 to allow for quiet areas and solitude!</p> <p>The landscape buffering between the proposed exit road onto A1130 and the existing housing looks insufficient particularly for the final houses on Heythrop Drive before the exit onto A1130. It looks like they have the entrance to a cycle path at the bottom of their back garden!</p> <p>No road through Mandale Meadow!</p>
Stephanie Atkin				<p>Do what you will with the private land but leave Mandale Meadow as it is. No road through public greenspace. Respect the covenant and the vision of people from the past who knew the importance of this place - it has only grown not waned over time.</p> <p>Mature makes it's own paths - no need for a tarmac network carving up the greenspace. We already have parkland that can be accessed in this way. A wild greenspace requires a different set of principles altogether.</p>
Stephen Connors				Thoughts previously mentioned

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Steve Gorman				<p>I would like to ask with regards to landscaping within the area, are any plans going to be made to the existing Foxes Woods tree areas? Can we expect any improvements in this area as part of the plan? The woods is a lovely area to use ,but it is not an all year round place to visit, due to the lack of proper pathways within the area. The green space field before this that backs onto the gardens from Hesleden avenue is also in need of improvements as well! The area has poor drainage and is hard to use in the wet winter months, this could easily be rectified with better drainage that falls to the beck. Is there any planned improvement to this area to be made as well?</p> <p>Finally I want to raise a concern about the trees planned, Looking at the master plan its saying 18,000 to 25,000 trees to be planted ,which on paper sounds amazing . I do want to stress though that the first part of the new houses off Low lane have not got a tree between them. What assurances will the council give that this will happen? Personally I think adding to the existing Foxes woods and extending it with a bigger woodland area would be ideal. with a proper pathway system sympathetically made within the area would add fantastic value to this historic area and make the overall plan even better.</p>
Steven Brown				<p>4.0 LANDSCAPE 4.3 Habitats & Biodiversity The adjacent diagram illustrates the potential extends of Habitats and Biodiversity created throughout the site through different methodologies.</p> <p>South of the site Again the diagram used indicates private land in the Species rich grassland created in glades & swathes</p> <p>4.0 LANDSCAPE 4.13 Boundary Conditions to the Country park The purpose of the boundary is to make the public open space safe for children and animals and create a barrier to urban areas and roads. Strong boundary treatment helps to define the public open space and create a sense of arrival into the country park. A range of boundaries can be used depending on whether screening or surveillance is required and whether a formal and urban character is required or a less formal and rural character is appropriate</p> <p>What about existing housing and the boundary fence ? Local land seller looks like hes obtained a green space around his property looks like I have ended up with a five foot wooden fence Told Green space on original plans was just an artist impression</p> <p>4.0 LANDSCAPE 4.16 Summary of Landscape Strategy</p> <p>Again the diagram used indicates private land as grassland in colour</p>

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Steven Henderson				As well as native trees it would be good to ensure a proportion providing edible fruit, such as walnut, cob nut apple, cherry pear etc.
stewart jones				Super,although I would prefer it to be maintained as a Nature reserve,ponds essential and a mix of evergreens for wildlife in the winter
The Countryside Charity North Yorkshire CPRENY	CPRENY			Paying specific attention to the design principles set out in the Masterplan document, CPRENY, welcomes the Council's attention to the inclusion of biodiversity and provision of pedestrian and cycle routes across the whole site. The document proposed that 53% of the total site be retained as green space which is encouraged. The inclusion of a specific landscape strategy as central to the masterplan is considered good planning. The provision of a country park and facilities, green connectivity and green fingers across the site, the creation of SuDs, recreational and circular routes, public art within the development, trim trails and dog walking areas, formal sport pitches and play areas, new woodland habitat, a green commercial and community hub, food growing areas including public orchards and quiet areas for solitude and wildlife-watching are all assets to be welcomed and encouraged. The summary of landscape strategy plan (pg 48) shows proposed locations for all potential facilities and 'areas', however, CPRENY are aware that this is not a planning application and has not been put forward by developers as a specific proposal so are concerned that much of this could be considered aspirational and unviable. It is hoped that MC would seek to ensure that these concepts are delivered in actuality across the whole site which is not entirely within Council control through the use of legal agreements etc.

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Thomas Maunder				<p>*p34, 4.1. The landscape is far more important than the built environment in this proposal. We have Climate Emergency and the rapid decline of wildlife to be concerned about. All building construction pumps carbon into the atmosphere and MBC has signed up to becoming carbon neutral. More than 53% of the land needs to be green and allow nature to grow and flourish and wildlife needs some 'wild' areas in order to do that. The recent sad sight of a baby deer running frantically around the streets of Thornaby</p> <p>searching for it's mother tells us we have already destroyed far too much of the natural environment with endless house building. Middlesbrough needs to set an example and put nature before profit.</p> <p>'Ensuring the Country Park permeates the development in terms of its character throughout the site'. I strongly disagree with this statement. The existing public open space can be left as it is and the new development can blend seamlessly into the natural landscape already on the Bluebell beck valley and mandale.</p> <p>There is no need for play parks or other man made structures on the existing open public space which local residents love as it is. If people can't find their own routes across the existing open public space perhaps</p> <p>a country park setting is not right for them. There is no sense of adventure or exploration in an area with endless paths. These plans for multiple paths cause destruction and carbon release. Most, if not all, of the paths are too wide to allow for a sense of quiet or solitude or a oneness with nature. Very narrow natural paths caused by people exploring, such as 'the fairy path' on Mandale, are the best kind of paths for solitude and quiet. A multitude of man made paths is very constricting and unnatural.</p> <p>*p35, 4.2. SuDs need to be more carefully thought out than they have been on Mandale as the Mandale SuDs floods in winter and dries up in warmer weather, leaving wildlife such as tadpoles and invertebrates in the flooded areas to die in isolated mud puddles which soon turn to hard, dry clay. The Mandale SuDs should be enlarged and made deeper in the central area to prevent the unnecessary death of wildlife.</p> <p>*p36,4. 'facilitating the movement of wildlife throughout the site' The green corridors need to be much wider than they are on the plan to allow wildlife movement throughout the site.</p> <p>'The woodland type' only native deciduous trees are mentioned but there needs to be some native evergreens included in the planting for the easement of traffic noise pollution such as English yew, holly etc. Hornbeam should also be planted as it attracts a moth whose only habitat is hornbeam.</p> <p>' In other places new hedgerows will be planted to break the monotony of the long sewer easements' please do not apply this to the existing open public greenspaces, we love our well established and natural meadows which have occurred without man's interference! The large meadow on Mandale is much loved and attracts a vast range of insects which are vital pollinators and provide food for the small mammals, birds, damsels and dragonflies. You can't improve a natural landscape.</p> <p>'with naturalistic earth mounding using the construction spoil'. DO NOT put construction spoil on Mandale meadows, it will despoil the environment. Dumping it on Mandale will destroy the existing flora and fauna there. Use it on your new development. Instead of relying on Tees Valley for ideas you should consult with Greenstuff admin as they know the land and it's flora and fauna best.</p> <p>*P37, 4.3. The landscaping plans, apart from paths being too wide and numerous, are fine except on the existing open public green space which should be left as it is. Public orchards are a lovely idea but would be better planted in deprived areas as a source of free fruit.</p> <p>*p38, 4.4. Recreational play for children is better served by natural greenspace than man made structures as it develops the imagination, peer friendships and co-operation, conversation and creativity. Today's children have far too much organized activity in terms of electronic gadgets, they need natural green spaces in which to develop physical skills and learn their body's limits.</p> <p>*p39, a 4.5 metre path through Mandale meadow and the beck valley is far too wide and destructive. 2.5m and 3.5m paths are ridiculously</p>

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				<p>numerous and too wide, they need reducing in size and number and kept to the new development with the exception of one narrow, dedicated cycle path on the existing open public space. I'm not sure why 'potential entry points' are shown on the map as places like Mosswood Crescent already have access to the existing open public space.</p> <p>*p40, 4.6 as above, reduce the width and number of paths and only one, narrow, dedicated cycle path through the existing open public green space. Informal paths should not be gravelled or their edges close mown as that causes the loss of informality and the feeling of being close to nature. 'Desire line' paths do not need to be mown, they occur naturally through the footfall of wildlife and humans.</p> <p>*p41. DO NOT dump excavation spoil on Mandale meadow, calling the spoil 'landforms' does not detract from the fact you are dumping waste on a meadow and killing off a large area of meadow. Use your building spoil where you remove it, i.e. on the new housing estate. Residents do not want your rubbish dumping on them and scarring the landscape. NO ROAD through MANDALE.</p> <p>*p42, 4.8 please keep LAPs and LEAPs in the new housing development and not on the existing semi wild, public open green space. There is ample opportunity to play and stay healthy in the existing green space, local children play all sorts of imaginative games and stay fit and healthy on our natural green space. Man made paly areas tend to get trashed by teenagers.</p> <p>*p43, 4.9 the first LAP pictured on this page is too small for a play area, it needs to be at least 5x as large. The NEAP shown at Houston is a good example of how to use excavated spoil on the new estate, and with encouragement, young children can use such equipment as an acted retelling of a story such as The Billy</p> <p>Goats Gruff as well as being fun for slightly older children. The LEAP shown in Rome is not adventurous enough. The mounds could be higher for sledging down in the winter for example. again, keep LAPS, LEAPs and NEAPs on the new estate.</p> <p>*p44, 4.12. Public art and signage is not needed on the existing open public greenspace although some more informative signs next to the gravel path would be a good idea as the old signage is worn out and fails to show much of the wildlife we have in the area. Interestingly, MBC are fully aware that we have water voles in and around the beck as the signage says so. Art doesn't lend itself to natural spaces as it is obviously a man made structure.</p> <p>*p45, Hedgerows are welcome habitats and provide both shelter and food for wildlife, the denser planted hedges are best to provide more shelter and safety for wildlife. 4.13 please leave the existing open public green space exactly as it is other than some hedging by the Mandale metal fence which should stay in place to prevent entrance to Travellers and off road drug runners bikes.</p> <p>*p46, 4.14. The problem with the 'vision' of the country park is that it is too manicured and formal, which is the opposite of what MBC claim is their intention. Leave Mandale and the BB Valley as they are - richly biodiverse, semi wild natural green spaces. Where the council see a beautiful country park all we see is another town park such as Albert park, too manicured, too manmade to be a beautiful rural retreat.</p> <p>*p47, 4.15, retaining the existing open public semi wild green space as it is, is much more important to residents than green flag status.</p> <p>*p48, 4.16. Leaving the existing open public green space as it is with NO ROAD through Mandale, just a loop road through the estate, means the manicured 'country park' will flow into the semi wild existing area and make a pleasing change to the heavily manufactured park.</p> <p>*p50, 4.17, there is no need for 'an indicative gateway' at the northeren end, i.e. Mandale meadow as it is well known and well used. Just don't build the road through the meadow and save a lot of money. Residents don't need or want an increase in air and noise pollution.</p> <p>The combination of road, 4.5m path and all the planting and dumping of construction spoil on Mandale is not wanted. The 'naturalistic earth mounds' created from excavated spoil will not grow healthy trees and shrubs. Central reservations will not be needed if there is no Stainton Way Western Extension. No road through Mandale, just a loop road through the new development.</p>

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				<p>*p51, it isn't clear where the SuDs etc in the map on p51 are going to be, hopefully not on Mandale meadow. All the grasses and plants in Mandale meadow are vital for wildlife. Councillors may just see long grass on a map, it isn't what residents and wildlife see.</p> <p>*p52, the 4.5 m multi use path is far too wide for a rural country park look.</p>
Trevor King				We can't control what happens on privately owned land but we want NO ROAD THROUGH MANDALE MEADOW/WOODS.
Val Young				No road through the meadow

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	Story Homes	Chris Smith	Lichfields	<p>Play Provision</p> <p>3.1 Page 42 sets out the proposed approach to play provision which has been influenced by the Fields in Trust approach. The plan on page 42 shows the proposed locations for the NEAP (Neighbourhood Play Area), LEAPs (Local Equipped Play Area) and LAPs (Local Play Areas).</p> <p>Whilst the importance of providing play areas and equipment is recognised, the plan suggests an onerous approach which does not appear to be justified. It is logical to position a NEAP centrally within the site and close to the proposed community hub/local centre and the Developer Group agree with this approach. However, the number and location of LEAPs and LAPs requires further attention.</p> <p>3.3 For instance, the plan on page 42 shows a LEAP in the northern part of the site and it is evident that only two development cells and a limited number of properties will benefit from this. It is considered that the two LEAPs could be replaced with one LEAP and its 'zone' would still cover the development cells in the northern part of the site. Furthermore, this could also be located towards the eastern edge to allow access from the Country Park.</p> <p>3.4 Similarly, there also appears to be considerable overlap in the LAP 'zones' which indicates that the amount is not justified. As explained on page 43 of the document, LAPs can be small areas of open space to allow play which are available within a short walk from the doorstep.</p> <p>3.5 The location of small areas of open space within the development cells is for the detailed design stage and goes beyond the purpose of this Masterplan.</p> <p>3.6 There is also a concern that the play provision has been shown without any consideration of viability. Should this level of provision be considered to be necessary it should be evidenced and supported by a viability assessment.</p>
Dave Blackburn				<p>The proposed road is bad enough but you want to put pathways all over the green. You are leaving no wild habitat just manicured fields with official pathways. We can go to the parks for that. Blue bell beck is unique wild place that should be left as it is for future generations. I have seen foxes badgers stoats hares rabbits hedgehogs frogs and newts further up the field are the deer, you are going to upset all this with the loss of your green habitats. Please think again of the wide variety of insects and birds. A lot of this will be chased out and lost.</p>
Gill Sullivan				<p>I have spoken to other residents who have flagged up the myriad of paths going over mandale field. Why? If you intend to give the rest of the field as Mr waters said to the public leave it green. As Mrs Hammond says save your money, leave fields not walkways. You state this will be a welcoming place, health safe and secure, your new Country Park. We already have a welcome place. All safe and secure, mandale field. MBC intend to remove this space, a much loved part of our families way of life. It is deeply depressing to imagine its demise. Our lovely mixed of wetland and old woods with its thriving meadow hold only monetary value to MBC. I know the residents know and MBC know full well the damage that, that 5% of road will do to that field. MR Waters and other members are clearly having difficulty understanding that. MBC cry being but we are leaving 95% untouched. Its not the 95% untouched its the 5% converted to a road that makes all the difference.</p> <p>1500 cars and congestion, pollution, noise and air, this routines the field 100% and totally changes the character of our neighbourhood. Please think again.</p>

Question 6 – Please provide any comments you may have on Chapter 5.0 Central Hub Study

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Name	Organisation (where applicable)	Agent	Organisation	Please provide any comments you may have on Chapter 5.0 Central Hub Study
Alexander Henegan				An “aspirational centre” is a curious term but perhaps it suggests a place where people want to live and work. A school, shops and other amenities are essential to give the housing estate some identity as a place where life can be fulfilling.
Aiden Woodward				All of the pictures look good and it looks like a nice local area, though there is a risk of anti social behaviour if left unchecked which would put people off using the area. Is there a possibility of having a smaller concrete sports area as well as the sports field e.g. with basketball hoops and smaller goals for 5 a side for use when the fields get muddy during the wetter times of year?
Aji Varghese		Aji Varghese		Good
Alan Liddle	Stainton and Thornton Parish Council			The central hub is important and a suitable meeting venue/community hall should be incorporated in the development. It should also consider putting in Electric Charging Points as part of the development.
Andrea Spooner				The community centre on Trimdon avenue closed due to lack of use and youths hanging around causing trouble so we are just moving the problem. Sad, but unfortunately, a sign of the times. The existing area provides far more benefits to the local community than a community centre would. Existing facilities such as the library in Acklam and other existing spaces such as empty shops on Acklam road would be available to use if deemed necessary.
angela barber				AS a resident of this area, I fear my house value will lower so close to this new road. There will be No where to walk my dog There would be Little or no space for children to play safety in area An area to exercise my self and enjoy picnics with the family Local wild life will loose there home and endangered species plants and animals will loose there environment Why should be suffer ill health because of further pollution We pay council tax for green space- will our council tax lower-expect not Increased noise during the day and night- lack of sleep and depression result All you want to do is provide new access road to the estate- why cannot they come around down downside and Acklam road instead as always has happened. I feel the new road will increase traffic into Middlesbrough through our area. I do not want or need the road as I do not drive- I doubt it will be included on the bus route, so it does me no good. In fact everything about it is BAD
Angela O’Byrne				Hands off our meadow!
Barbara Keville				Central Hub Area for youths to congregate. Local shops....more takeaways.
Barry Jobson	South Tees Green Party			However, we still oppose this development, and consider the Stainsby scheme to be unsustainable, with it being too reliant on car usage for residents to move around, as other than the primary school and shops, there are no employment opportunities for residents.
Barry pinkney				Country park no
Bethany Thatcher				I understand the surrounding primary schools are already very high in numbers and it is therefore imperative for a primary school to be built. This, together with a shops parade, will be necessary to serve the local community.

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Carol Newmarch				I object to the whole plan
Carolyn Dodds				I would like to see provision for a Community Centre with activities for both young and seniors alike. It might be advantageous to site CCTV around the hub and children 's Play areas. It is often found that education facilities are neglected in proposed housing developments. I am requesting the developers stick to the Plan and construct a school as this is a large housing development of 1670 homes making this a necessity. Added to the fact the houses proposed next to the Sporting Lodge will also need education facilities. The Acklam primary schools are full to capacity and I understand this can change but looking ahead it seems necessary.
Catherine Howell (Boyle)				5.2 Traffic on Acklam Road caused by access to secondary and primary schools is already excessive. An additional school is being built at Sandy Flatts. How will the development of another school help with these issues? How is the increased traffic to be accommodated? Is access via sustainable transport encouraged from the outset to embed it into the culture of the school?
Chris King				'The hub is ideally located to serve all local residences providing education, retail, amenities and appropriate built infrastructure to the Country Park. A transport 'super-stop' will be strategically placed to promote high-quality sustainable travel featuring real time displays for transport links and cycle parking for door to door connections.' Yes, if we could encourage alternative modes of transport, but given the number of houses and the subsequent number of cars, I remain uncertain as to how increasing traffic flow into congested existing routes can enhance things. Indeed the term 'super stop' is in danger of meaning something quite different for the many car owners likely to be stuck in queues whether on existing routes or indeed from within the proposed development.
Christine King				We can't control what happens on privately owned land but we want NO ROAD THROUGH MANDALE MEADOW/WOODS.
Claire Boyd				We can't control what happens on privately owned land but there should be NO ROAD THROUGH MANDALE MEADOW/WOODS.
CLAIRE KANE				The pictures are misleading it will be a black tarmac road for people to speed up and down destroying the habitats and wildlife - we already have foxes owls and bats around the area that are walking and flying around our 40 + year old estate because they have had their habitats destroyed by the new houses on Jack Simon Way estate

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Claire Welsh	Acorians Forest School Childcare			<p>We would like to propose that as part of the central hub an addition of a day care facility be added to provide childcare for the new housing development and surrounding areas. We have developed a hybrid program of learning that links closely with the Forest School ethos and a hygge approach, creating a warm, safe, home from home environment while developing a sustainable and environmentally nurturing outlook for our next generation. The nursery will aim to create a sense of community and belonging within the new development, creating strong links and instilling that deep rooted community spirit that sadly seems to be missing from the current climate.</p> <p>As well as giving children the tools they need to tend and care for the natural world around them, gain insight in to seasons and the flora and fauna which are important to those seasons and the importance of natural habitats it will develop their understanding and critical thinking for the future.</p> <p>As part of the nursery ethos, mental health plays a large role. Mental health in children is soothed in the outdoors, we know this from our own experience. The program we have created hopes to be able to provide the development of resilience in our children and a good basis to their health in general, focusing on good eating habits and regular exercise which we hope will avoid the many common pit falls our children are facing in the modern world. Play is the most vital work a child can do, it is how they develop all the skills they need to progress not just through their educational journey but their adult one too. As the program we have developed is a hybrid version it is compatible with the EYFS Framework while still holding on to the Forest School ethos of nurturing nature and the natural world. Catering for all children aged from 6 weeks to 5 years, both funded and non-funded places will be available. As the business grows it would create job and training opportunities. We have already developed links with colleges and training centers so would be able to offer placements for training.and more.</p> <p>We would love to be considered as part of the development, we believe that our business model reflects your aims and goals for the Stainsby development and we would love the opportunity to work with you to help to develop the proposed plan.</p> <p>If you would like more information about our business model please don't hesitate to get in touch.</p>
Claire Wilks				No hun due to houses not being developed. Already in area: the Acorn Centre and The Acklam Green Centre which are local.
Colin and Morag Bailey				<p>*p55, 5.1 It's impossible to tell from the pictures exactly what the Central Hub will look like but one is certainly needed for a large new estate, it is just hoped the developers will actually produce one and the promised green spaces - but they frequently fail to deliver.</p> <p>*p56, 5.2. any new school within a 'parkland' setting should be designed so all children can walk to school safely.</p> <p>*p57,5.3. There should be no road exiting at Mandale so any public transport should enter and exit via the loop road. If there is a cycle path exiting at Mandale it means people can commute by cycle or mobility scooter/wheelchair.</p>
Dennis Mccabe				This is a great addition to the area, this will help form a community
Emily quinnt				No road necessary. Do you want people and animals to get run over?
Francine Kenny				Yes to Country Park Yes to houses yes to road

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Gill Mollard				This housing allocation is not necessary and should stop. Any 'country park elements' should remain within the site already under construction and NOT damage the area on Mandale Meadow, Newfield Meadow or the connecting green spaces by the addition of landscaping features, footpaths, cycle paths, play equipment or 'desire lines'.
Glen Instone				Such places attract youths and the usual anti-social behaviour.
Graham Loughborough				Local shops, coffee/bistro bar micro pub. Will create local jobs for the estate.
Helen McGregor				The road for these facilities should come back out onto Low Lane.
Hilary Morley		Hilary Morley	Resident	Refer to chapter 1.0
Ian Etherington				Agree should prioritize non-motorised traffic.
Jacqueline Jones				I see no need for a visitor center. The central hub facility should contain the playground aspect of the scheme
Jacqueline Young				Who will be paying for this hub and how much will it cost?
James Wilson				shortage of school places, when you consider the struggle Middlesbrough, Stockton, Redcar all had more pupils than available places, plus doctors, dentists and probably more infrastructure than I have mentioned
Jamie Barber				Frankly, none of this should go ahead. You're planning to destroy natural green space and animal habitats that are at a premium in Middlesbrough.
Jenny McCullagh				Central hub just equals school. Increased traffic to the area, no doubt McDonalds and Subway. More standing traffic, more pollution, more disrespect to our area.
Jill Hadfield				No road through Mandale Meadows
Joan Pollak				I think that the proposed primary school should be placed as far away from the A19 as is reasonably possible to prevent harm from pollution. There appears to be a relatively small area allocated to the school on the plan and I feel that this is insufficient, given the difficulty many schools have faced recently, when social distancing has become necessary.

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Joanne Mitchell				<p>The A19 has already been widened at some cost in terms of physical and mental health and the general well being of residents. Issues exist which require resolution regarding noise and air pollution and ought not to be made worse still by this imposition.</p> <p>The road would destroy the meadow.</p> <p>These sad plans they have to ruin one of the most beautiful parts of Middlesbrough not to mention the habitat of wildlife that will disappear without the meadow. Greenspaces are very precious and need to be kept as they are. Years of conservation and preservation work has been invested. People moved close to the meadow being given to understand it is protected, a greenbelt where no building or any development may ever take place. It may never have been a designated greenbelt but it was the subject of a Deed of Covenant, the land having been gifted to the people of Middlesbrough in perpetuity. A road is being planned through the meadow to service a housing estate.</p> <p>At the end of the day destroying a pristine meadow which is rich in biodiversity and has never been 'treated' with pesticides and herbicides, is nothing short of ecocide and vandalism and makes a laughing stock of the council's One Planet Living and green strategies.</p> <p>There was always another option, the 'developers' could create an estate loop road similar to the one at Trimdon.</p> <p>The council has stated they won't even pay local residents a penny in compensation for taking our public amenity from us and subjecting us to increased air and noise pollution, damaging our health.</p> <p>the disruption, destruction, the irreparable damage, and the blatant disregard for people who have lived here and who have been involved during many years in the care and preservation, and who will have their lives destroyed, just beggars belief. Residents already suffer the constant excessive traffic noise...and the pollution inflicted upon them. To think that there are those with the power who lack the empathy and the understanding, or thought and consideration for people who use the meadow and those who live close by is unbelievable.</p> <p>The residents in the Ayresome ward would no longer be able to access Mandale meadow from Mandale Road. It will be impossible for people in the Ayresome ward, including those of us who live a 2 minute walk from the current entrance to Mandale meadow is because it would involve walking straight across two lanes of the proposed roundabout. The road on OUR meadow stopping us accessing OUR meadow is the issue. All this nonsense about "you are getting extra greenspace" is an insult when you are planning something which prevents us from accessing OUR green space.</p> <p>The remarkable thing about this is some of the language used in the document to describe the proposed plan: "To create a diverse ecosystem" etc when one already exists that will be destroyed by the building of so many houses. The plan to create this "diverse ecosystem" on land which has been polluted by herbicides, pesticides and pollution from the building process while destroying a pristine meadow which is genuinely a diverse ecosystem containing Red and Orange endangered species marked on this map of Mandale meadow. They know this because the map and the legend were included in the Greenstuff response to the 2018 LP.</p> <p>The area is home to rare species flora and fauna, which have taken decades to fully establish, all of this would be destroyed.</p> <p>The faux manufactured country park, cannot, and will never, as if by magic, replace this.</p> <p>Mandale Meadow is well used and loved by many, an accessible place in which we are able to walk and relax.</p> <p>Residents currently suffer excessive noise and air pollution levels 24/7 owing to the proximity of the A19...how can it possibly be considered in any way acceptable to bring yet another road even closer to homes.</p> <p>When the A19 was widened residents received compensation for the loss in value of their homes..but despite the proposed road being routed even closer we are told we would not receive any such award for all of our losses. ..the loss of peace, and of mental and physical health. .. These of course can never be compensated for. It is not what we want.</p> <p>The notice delivered to each resident shows a basic outline of the area concerned, part of which is Mandale Meadow at the top of the 'plan'. The notice gives the impression that the entire area will be Country Park, along with which would come 1700 houses, not forgetting the so called Relief Road, to 'relieve' congestion on the A19 and serve the 1700 new homes. The plan actually shows the road through Mandale Meadow which would discharge traffic onto Mandale Road, at a junction adjacent to residents homes where the current car park is now situated, actually at the foot of their gardens.</p>

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John and Janet Roebuck				<p>Central Hub</p> <p>There is no indication as to when the central hub retail facilities will be built, other than the primary school, which would be required before occupation of 50% of the dwellings.</p> <ul style="list-style-type: none"> • What assurance can be put in place that these retail facilities will also be developed? • Why isn't a similar assurance provided? • Where are the costings and funding streams for these elements of the development?
John David Duffy				<p>Central Hib looks brilliant and will provide facilities needed to support such a development. School, community centre, shops. To me the scheme looks to have been designed with everything in mind.</p> <p>The school and community can share sport fields etc. and having the visitors centre close by will create a superb hub for the scheme.</p>
Judith Harbron				No development
Judith Maunder	Friends of Mandale, Greenstuff, resident very local to Mandale Meadow			No objections to a central hub in the proposed estate. A school should be built asap as current schools are already full and it would be unfair to have a repeat of the problem when approx 100 children could not be allocated a school place because of migration to Middlesbrough. p57, happy with those plans. such a vast development needs basic facilities.
Julia Povey				We can't control what happens on privately owned land but there should be NO ROAD THROUGH MANDALE MEADOW/WOODS.
Julie Riley				NO ROAD THROUGH MANDALE
Kabaser Ajaib	Retailing	Kabaser Ajaib	Retailing	No through road
Keith Russell				No road through Mandale Meadow
Keith Young				No road through Mandale, you can do what you like on the private land
Laura McGregor				Construction materials and methods for the hub should go above and beyond BREEAM credit points or equivalent and provide an excellent example of sustainable design.
Leah Davey				<p>This housing allocation is not necessary and should stop.</p> <p>Any 'country park elements' should remain within the site already under construction and NOT damage the area on Mandale Meadow, Newfield Meadow or the connecting green spaces by the addition of landscaping features, footpaths, cycle paths, play equipment or 'desire lines'.</p>
Liam Harris				Please see sheet 1
Linda Barker				<p>This housing allocation is not necessary and should stop.</p> <p>Any 'country park elements' should remain within the site already under construction and NOT damage the area on Mandale Meadow, Newfield Meadow or the connecting green spaces by the addition of landscaping features, footpaths, cycle paths, play equipment or 'desire lines'.</p> <p>This site allocation will cause SIGNIFICANT detrimental impacts to the existing wildlife site at this location.</p>
Lynn Blagg				Leave site as it is to enable residents and others to use it in its natural state for exercise and pleasure and avoid destruction of wildlife habitats and ageing flora and fauna and avoid excess traffic in the area.

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M. E. Bailey				<p>*p55, 5.1 It's impossible to tell from the pictures exactly what the Central Hub will look like but one is certainly needed for a large new estate, it is just hoped the developers will actually produce one and the promised green spaces - but they frequently fail to deliver.</p> <p>*p56, 5.2. any new school within a 'parkland' setting should be designed so all children can walk to school safely.</p> <p>*p57,5.3. There should be no road exiting at Mandale so any public transport should enter and exit via the loop road. If there is a cycle path exiting at Mandale it means people can commute by cycle or mobility scooter/wheelchair.</p>
Malcolm Chilvers				<p>Like many people I have neither the time nor inclination to be able to read and absorb the information contained in all these chapters unlike yourselves who are being paid to do it.</p> <p>A Visitors Centre, will there be displays of what the area used to look like prior to desecration?</p>
Malcolm Watson				please see comments in attached file
Matthew Shippey				Don't want this. I walk in mandale meadow. Middlesbrough is lacking in green space and you want to build a road through to facilitate new housing. Middlesbrough Council you are a disgrace.
Michelle Hedger				No road through Mandale Meadow. Use the loop road option. If a road needs to be there make it a narrower road of 20mph. My children cannot play near the huge 30mph road planned. They can play now in the meadow. They will be hit by a car in this 'country park'. Please don't put all the footpaths in. We like to follow the natural winding dirt tracks. There is only lip service paid to these. We don't want artificial paths. It's beautiful, muddy and filled with nature.
Mike Conyard				Do not build on this land. This is much needed green space. There are no parks in this area and nowhere for people to walk, kids to play and people to live their lives. I find it disgusting that you propose to build on this beautiful piece of land. Also, who wants to live right next to the A19? These house will be worth literally nothing.. The land is also a buffer zone from the A19 to the residents of Heythrop Drive and beyond.
Miller Homes and Avant Homes	Miller Homes and Avant Homes	Mr Neil Morton	Savills	<p>It is noted that Policy H21(d) requires the development to provide a local retail centre and primary school to be provided when need arises. Our clients are fully committed to contributing appropriately and proportionally to the delivery of these facilities as part of an overall sustainable and mixed community.</p> <p>Our clients now agree with the location of the Central Hub within the masterplan and its proportional split in terms of land-take between the southern and northern ownerships. This should be retained in the final masterplan.</p> <p>Whilst we acknowledge the high aspirations of the Council in terms of the Central Hub and its contribution towards achieving Country Park Status, care must be taken that the bar is not set so high that it contravenes the planning requirements of the site. For example, the masterplan identifies a Visitor Centre which appears to go beyond the scope of the development plan policy requirements of the allocated site. Clearly our client has no objection with such a facility being delivered at the site, however the contribution of individual planning applications must remain in accordance with planning requirements. It is envisaged that these matters will be examined in greater detail through the planning application process. Chapter</p>

Please provide any comments you may have on Chapter 5.0 Central Hub Study				
Name	Organisation (where applicable)	Agent	Organisation	Please provide any comments you may have on Chapter 5.0 Central Hub Study
Nichola Davey				This housing allocation is not necessary and should stop. Any 'country park elements' should remain within the site already under construction and NOT damage the area on Mandale Meadow, Newfield Meadow or the connecting green spaces by the addition of landscaping features, footpaths, cycle paths, play equipment or 'desire lines'.
Nicola Smith				We DO NOT WANT IT!!!! WHY can't you build on areas where you have flatted hundreds of houses but not rebuilt? Keith Road or Marton Burn Road??? This is the only green space we have in Acklam and hundereds of residents rely on this being walking distance. Pre covid the traffic on mandale Road, Acklam Rd and levick Cresent is bad enough at peak times without further traffic. You have Acklam Grange Secondary school round the corner from the meadow with 1200+ students that also use these roads. This is already a very busy junction without diverting more traffic to it. What about adults and children's mental health?? Has the past year taught you nothing?? I think having a open greenspace with all the wildlife and plenty of room to exercise and get much needed fresh air and been an absolute saviour to hundereds of local residents. This project has not considered anything but money. Looking at the plans there will be very little green left and it will be surrounding a road and housing estate. Absolutely nothing like we have now. This is NOT ACCEPTABLE FROM ANYONE I KNOW WHO LIVES IN ACKLAM 😞😞😞😞😞😞😞😞😞😞
Nicolas Mitchell				This is another area where a bit of care is needed to avoid looking like a new town development, and again less may be best and cheaper for all concerned. Less concrete, more use of the kind of more natural walkways and please plenty of tree planting to encourage wildlife.
Peter Davey				This housing allocation is not necessary and should stop. Any 'country park elements' should remain within the site already under construction and NOT damage the area on Mandale Meadow, Newfield Meadow or the connecting green spaces by the addition of landscaping features, footpaths, cycle paths, play equipment or 'desire lines'.

Please provide any comments you may have on Chapter 5.0 Central Hub Study				
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Poppy Bowyer				<p>What is the long term goal for the hub?</p> <p>How will the unorthodox school and visitor centre be funded long term?</p> <p>Is there any car parking for the pitches/visitors centre?</p> <p>Will the artwork featured be locally sourced?</p> <p>Will the school work in conjunction with MIMA?</p> <p>Will the hub buildings be made from recycled or reclaimed materials?</p> <p>Will they have a living roof?</p> <p>Lots of community centres within the area were closed down over the last 10 years for lack of use. Is there interest or plans for the activities it might cater for?</p> <p>What local investment will this encourage?</p> <p>Will small business owners get an opportunity to set up here?</p> <p>How will this build be sustainable and help to boost local sustainability?</p>
Rob Atkin				No road through Mandale.
Sean Marshall				Incorporate a youth club to keep kids off the streets which will help with crime
Sofia Torre				What I can't comprehend is that you are not building something like this alongside the river tees, so that these facilities are available to all of middlesbrough, there is enough unused land and being by the water is a healthier option than building and littering of what is left of the green area. Really shocking vision on behalf of all the cross parties
Sonia Bowler				<p>This can happen without building a road through Mandale Meadow.</p> <p>No road through Mandale Meadow!</p>
Stephanie Atkin				No development on Mandale Meadow. Do what you will as part of the private scheme as long as all that development entails (ie access) can be accommodated within. Loop the road, no road through Mandale Meadow.
Stephen Connors				Thoughts previously mentioned
Steve Gorman				I love the idea of this central hub and believe it will add fantastic value to the whole masterplan. The idea of an area which has multi use and functions will add to the whole concept of the plan. From education to little cafes, bars, restaurants so it becomes a go to place for locals with the right mix of something for everyone, this could be a great addition for the area. I do think though that we have to get the balance right. If we end up with 2 or 3 takeaways there It would raise concerns over litter etc .
stewart jones				I see no reason for a visitor center

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Thomas Maunder				<p>*p55, 5.1 It's impossible to tell from the pictures exactly what the Central Hub will look like but one is certainly needed for a large new estate in order to avoid isolation and increased car use. It is to be hoped that the developers will actually produce one and the promised green spaces - but they frequently fail to deliver.</p> <p>*p56, 5.2. any new school within a 'parkland' setting should be designed so all children can walk to school safely.</p> <p>*p57,5.3. There should be no road exiting at Mandale so any public transport should enter and exit via the loop road. If there is a cycle path exiting at Mandale it means people can commute by cycle or mobility scooter/wheelchair.</p>
Trevor King				<p>We can't control what happens on privately owned land but we want NO ROAD THROUGH MANDALE MEADOW/WOODS.</p>
Val Young				No road through the meadow

Question 7 – Please provide any comments you may have on Chapter 6.0 Urban Principles

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Name	Organisation (where applicable)	Agent	Organisation	Please provide any comments you may have on Chapter 6.0 Urban Principles
Alexander Henegan				<p>“A Transportation Hub will be hosted as part of the Central Hub facility... The creation of a ‘super stop’ will featuring (sic) real time displays for transport...” This is one example, of a number within the document, where the use of a style guide for clear, accurate writing and the use of a proof-reader would have helped make The Stainsby Country Park and Masterplan proposal more effective.</p> <p>Size of garage can cause problems. Instances of garages that cannot accommodate large cars or even medium-sized saloons have been noted about recent housing developments in the Acklam area.</p>
Adam Merritt				<p>As the masterplan below states on 6.0 URBAN STRATEGY, 6.7 Parking, all houses have at least two parking spaces and there is to be no parking on a Primary Road which JSW is. With this in mind could the council please follow their own guidelines and make sure nobody is parking on JSW before an accident occurs. This could be done by placing double yellow lines on the road. I know it’s not a permanent surface, but it would solve the problem until it’s adopted and get residents used to parking elsewhere.</p>
Aiden Woodward				<p>House styles all look good with a modern look. For houses on the spine road easy access to off street parking is a must, currently on Jack Simon Way there are a significant number of cars parked on the street as their drives are at the back of the house, which only causes minimal congestion currently but will worsen as the area gets busier and traffic increases.</p>
Aji Varghese		Aji Varghese		Good
Alan Liddle	Stainton and Thornton Parish Council			<p>The primary traffic route should have a weight restriction on it to reduce the amount of potential heavy traffic using the route. There should also be parking restrictions on the primary route to allow buses to operate efficiently.</p> <p>We query the need for "low rise apartments", as the recent pandemic has shown all people need their own outside green space and even though they will be very close to the new country park, apartments should not be part of the development.</p>
Alan Turley				<p>Necessity: The development of a new spine road only becomes necessary if the additional phases of housing development are permitted. The current phases are adequately served by Jack Simon Way giving quick access to the A174 and A19 via Low Lane. The A174 junction of the A19 is controlled by automatic traffic signals at peak times providing for free flow of traffic. The proposed new road will run almost parallel to the existing route and from what I can see no impact assessment has been conducted on the likely congestion at the Mandale end, a route that is already congested at peak times. The proposed new route will do nothing to alleviate the flow of traffic through existing routes such as the Oval, Trimdon Avenue and Acklam Road as many of the estates children attend schools serviced by those routes.</p> <p>Road Safety: The existing portion of Jack Simon Way from the Low Lane roundabout to the junction of Hampstead Way has housing with only parking situated at the rear. As a result many residents park their vehicles along this stretch of road in some cases double parked. This makes the road narrow for vehicles travelling in either direction and presents a danger to the children in the area who are frequently playing in the vicinity. There is no scope to widen this stretch of road without removing a footpath in front of the existing houses. To increase the volume of traffic and introduce buses to the route will only serve to increase the existing level of risk to both drivers and pedestrians.</p> <p>The existing section of road is poorly lit which again adds to the level of risk during the hours of darkness.</p>

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angela barber				<p>AS a resident of this area, I fear my house value will lower so close to this new road.</p> <p>There will be No where to walk my dog</p> <p>There would be Little or no space for children to play safety in area</p> <p>An area to exercise my self and enjoy picnics with the family</p> <p>Local wild life will loose there home and endangered species plants and animals will loose there environment</p> <p>Why should be suffer ill health because of further pollution</p> <p>We pay council tax for green space- will our council tax lower-expect not</p> <p>Increased noise during the day and night- lack of sleep and depression result</p> <p>All you want to do is provide new access road to the estate- why cannot they come around down downside and Acklam road instead as always has happened. I feel the new road will increase traffic into Middlesbrough through our area.</p> <p>I do not want or need the road as I do not drive- I doubt it will be included on the bus route, so it does me no good.</p> <p>In fact everything about it is BAD</p>
Angela O'Byrne				Hands off our meadow!
Barbara Keville				<p>6.7 Parking</p> <p>Are the builders going to provide sufficient off street parking for each household? These days most families have a least 2 vehicles if not more. Parking on pavements is unacceptable but is the norm....garages are for storage or redevelopment.</p>
Barry pinkney				Country park no
Barry, Ann and Craig Pearson	Stainsby Hill Farm	Michael Mealing	Michael Mealing Planning	<p>Over recent years, in discussion with Planning Officers and accepting that it would represent development of previously developed land, it has been agreed that there is potential for the provision of additional dwellings within the Stainsby Hill Farm farmstead by way of both the conversion of outbuildings and the demolition and redevelopment of farm buildings.</p> <p>In that context, it has, again, been agreed that, within the Masterplan, there should be provision for the 'farmstead complex' to be linked for access purposes to the adjacent new housing development to the east.</p> <p>This will enable the current access to the farmstead, across a bridge over the A19, to be dedicated specifically to agricultural use in connection with my clients' land ownership to the western side of the A19, both to the north of Stainsby Grange Equestrian Centre and to the south of the Stainsby Medieval Village and Open Field System Scheduled Ancient Monument.</p> <p>The Masterplan should include provision for a road access to be created into the proposed wider housing area from what will become the former farmstead complex which will enable the segregation of agricultural vehicles and machinery from residential activity generated within the 'former' farmstead.</p>

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Barry, Ann and Craig Pearson		Michael Mealing	Michael Mealing (Planning)	<p>Over recent years, in discussion with Planning Officers and accepting that it would represent development of previously developed land, it has been agreed that there is potential for the provision of additional dwellings within the Stainsby Hill Farm farmstead by way of both the conversion of outbuildings and the demolition and redevelopment of farm buildings.</p> <p>In that context, it has, again, been agreed that, within the Masterplan, there should be provision for the 'farmstead complex' to be linked for access purposes to the adjacent new housing development to the east.</p> <p>This will enable the current access to the farmstead, across a bridge over the A19, to be dedicated specifically to agricultural use in connection with my clients' land ownership to the western side of the A19, both to the north of Stainsby Grange Equestrian Centre and to the south of the Stainsby Medieval Village and Open Field System Scheduled Ancient Monument.</p> <p>The Masterplan should include provision for a road access to be created into the proposed wider housing area from what will become the former farmstead complex which will enable the segregation of agricultural vehicles and machinery from residential activity generated within the 'former' farmstead.</p>
Ben Stephenson	Persimmon Homes Teesside			<p>Section 6.7 Parking</p> <p>Persimmon Homes agree that the careful integration of car parking within the streetscene is an important consideration for any layout. The imposition of additional requirements above and beyond those contained within the Tees Valley Design Guide & Specification is therefore at odds with this by excluding suitably sized garages from contributing to the parking provision, thereby requiring more parking spaces within a scheme at the expense of the streetscene.</p> <p>Given these additional requirements it is imperative that the 'indicative' solutions offered by the images supporting this section are realistic otherwise there will be major discrepancies between expectation and reality when the detailed planning applications come forward. For example, the top image of the four properties served from a private drive does not illustrate each dwelling having a minimum of 2 designated parking spaces, excluding the garage.</p> <p>At a conservative estimate, the middle image of the three shows a block of 6 houses (minimum –based on footpath links) on a chamfered corner with only 9 spaces identified to the rear. To compound matters some of the plots do not appear to have a private garden area either to the front or rear due to the parking, something which would immediately be highlighted as an issue at the detailed planning stages. In addition, the block of 8 parking spaces in the left of this image would also fail another requirement of the document when it states, "In all cases the width of the driveway at the point where it meets the footpath or road cannot be greater than 75% of this property boundary".</p> <p>These examples are therefore potentially misleading and we would suggest that they are replaced with accurate illustrations of parking solutions. We would also suggest that the document allows sufficient flexibility for individual plots to deviate from the 75% requirement outlined above where it cannot be achieved otherwise it risks excluding smaller 2, 3 and potentially some 4 bedroom properties due to their width as they will be unable to accommodate two car parking spaces side by side whilst retaining 25% of the frontage as garden area despite otherwise being fully space-standard compliant. To overcome this single width driveways would need to be 12m long which would severely hamper the density and viability of the development.</p> <p>There also appears to be no justification or reasoning as to why "frontage parking may occur only to one side of secondary or tertiary roads and only where plots have sufficient depth (8m minimum from curtilage edge to front of dwelling) to enable integration of landscape." A standard car parking space is 6m long and whilst the Council reference the need to provide a 1m landscaping strip it is unclear how the additional 1 metre is justified, or how the Council will maintain control of this 1m wide landscaping strip once the properties are occupied.</p> <ul style="list-style-type: none"> · What justification is there to exclude garages from contributing towards the overall parking provision? · Has the impact of restricting traditional integral garages such as those identified on page 69 of the document been considered in relation to housing needs / purchaser's wishes? · Has the requirement for no more than 75% of a plot frontage to be car parking been considered in relation to the sites ability to provide a suitable housing mix? · Is the document sufficiently flexible to allow a suitable housing mix to be achieved? · What information has been used to identify and justify a requirement for front gardens to be 8m long when parking spaces are required to

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				<p>be 6m long?</p> <ul style="list-style-type: none"> · What controls are in place for the Council to manage and maintain the 1m landscaping strip at the end of driveways? <p>Are there any constructed examples of parking solutions which the document can refer to rather than the unrealistic images?</p> <ul style="list-style-type: none"> · What analysis has been done to identify the viability impact of the parking strategy on the overall development? <p>Section 6.10.3 Feature Walls Whilst we understand the design aspect of wishing to provide feature walls, the suggested 'feature' appears to be consist of holes or gaps in the boundary. Whilst the aesthetic qualities of the design can be appreciated our experience would suggest that future residents of the estate would rather have privacy in their rear gardens and therefore there is a risk that any holes or gaps will be infilled with an array of materials which will ultimately result in a disjointed appearance throughout the estate.</p> <ul style="list-style-type: none"> · Has potential purchaser's requirements / expectations for privacy been considered? · How can the Council adequately control the retention of feature walls once constructed? <p>Section 6.11 Boundary Treatment Indicative Study Again our experience suggests that residents would prefer privacy within their rear gardens. The guidance suggesting timber fence within trellis in rear gardens is therefore at odds with resident's expectations and will likely be changed. Given the position of these boundaries with the development block, out of the view of the public areas control of these boundaries will be difficult and the original design will add nothing to the general character of the estate. We therefore suggest standard 1.8m high fencing should be acceptable in these locations.</p> <ul style="list-style-type: none"> · Has potential purchaser's requirements / expectations for privacy been considered? · What evidence is there to justify this requirement given that the boundaries will not be visible from any public areas? · How can the Council adequately control the retention of these boundaries once constructed? <p>Section</p>
Carol Newmarch				I object to the whole plan
Catherine Howell (Boyle)				<p>6.1 I do not agree with building the SWWE and believe the access road should be looped through the estate. 30mph is too high for any road within this scheme to be used safely by cyclists and pedestrians and also encourages motorists to use it to avoid congestion on the A19/A174.</p> <p>"Future resilience of the local road network." We should be moving to a strategy to reduce car usage by making it the least convenient mode of transport. Accommodating increasing traffic numbers by widening/expanding roads has been proved to increase still further car numbers. This does not sit within MBC's green strategy for sustainability. Can roads be restricted to use just by sustainable/public transport, electric vehicles only?</p> <p>6.6 Are sustainable design features being incorporated into housing? Solar panels, passivhaus standards etc? If not, why not?</p>
Charlotte McAdam				One access point through Jack Simon Way would be sufficient - 'impacts of roads should be minimised' - impact of building a road on Mandale Meadow cannot be minimised.

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Chris King				<p>The primary route, also known as the Stainton Way Western Extension, will run north-south through the Stainsby site, providing vehicular access to the development as a whole as well as improving the future resilience of the local road network.</p> <p>I suspect the word resilience here may be suitable for those wishing to gain access to the project but exiting the site may be a lot more challenging given the current levels of traffic flow along Mandale road and indeed future projections over the next 10 years.</p> <p>'Crossing Points A series of crossing points have been developed in conjunction with Middlesbrough Council which aim to encourage pedestrian priority movement whilst creating natural traffic calming features at intervals between the paths and road intersections throughout the masterplan.'</p> <p>Pedestrians including children walking to school from Thornaby along Mandale Rd. In what ways will their safety be maintained or indeed enhanced by a further road junction from the proposed development?</p>
Christine King				We can't control what happens on privately owned land but we want NO ROAD THROUGH MANDALE MEADOW/WOODS.
Claire Boyd				We can't control what happens on privately owned land but there should be NO ROAD THROUGH MANDALE MEADOW/WOODS.
Claire Di Cicco				No to a road.
CLAIRE KANE				The pictures are misleading it will be a black tarmac road for people to speed up and down destroying the habitats and wildlife - we already have foxes owls and bats around the area that are walking and flying around our 40 + year old estate because they have had their habitats destroyed by the new houses on Jack Simon Way estate
Claire Wilks				Traffic will be busy as mentioned in point 5 above, causing pollution and dangers to those using the area. A 'natural' country park cannot have a road running through it causing noise and pollution to the air and wildlife.

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Colin and Morag Bailey				<p>*p59, 6. It isn't possible to prioritise pedestrians where there are roads. No road through Mandale meadow ensures public safety from vehicular accidents and lung and heart damage from toxic emissions. It has been calculated that less than 50% of people will be using electric cars by 2040 so that is a lot of people's health the council will damage through toxic emissions. You need roads on a housing estate, you do not need them on the existing open public green spaces. A spine road through Mandale is dangerous for residents and wildlife. We have deer, hedgehogs and a variety of small mammals on Mandale woodland and Farley Copse which will die on the road. We need to start protecting nature and wildlife for the sake of residents and the planet.</p> <p>*p60,6.1, 30mph is far too fast for the new estate, it should be 20mph or less on all roads. 'Strategies' to slow traffic don't work as feckless drivers see it as a form of a Brands Hatch race course.</p> <p>No Stainton Way Western Extension through Mandale meadow.</p> <p>'improving the future resilience of the local road network' This is not necessary as people will not return to the same way of working due to the covid pandemic, more work will be done at home rather than commuting to an office.</p> <p>*p61, 6.2 It's interesting that the combined width of the multi use path and the green wedge in between the road and the path are wide enough to create a dual carriageway at some future date. Pual Clarke is on record as saying a dual carriageway is a future possibility if there is a significant increase in house building in South Middlesbrough. No Road through Mandale, a loop road to serve the new estate only.</p> <p>*p61/62, the paths around the estate are too wide and take up to much valuable green space.</p> <p>*p63, 6.3. residents of the Ayresome ward do not want buses coming over the meadow and certainly not a bus hub in the middle of the meadow. This is ecocide.. No road through Mandale.</p> <p>*p64. the multi use path is not a good idea.</p> <p>*p65, 6.5.1, it's hard to see how pedestrians have priority on a 30mph road with no zebra crossings. No road over Mandale meadow.</p> <p>*p66, a road through a country park is an oxymoron and isn't safe for pedestrians or wildlife.</p> <p>*p68, p69, 6.7, the parking designs seem clunky and unrealistic and look like a throwback to Victorian housing when people only had access to garages/stables via the back alleys. These days households generally have multiple cars and should be able to access drives/garages from the front aspect of their homes. All new homes should have electric charge points for EVs at the front of their homes.</p>

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David John Parker	Stagecoach North East			<p>We have a number of observations regarding the Masterplan, which we outline below:</p> <p>Our greatest concern regarding the Stainsby masterplan is the absence of a firm binding requirement to establish a definitive s106 developer “pump-priming” funding contribution commitment towards public transport revenue support, until such time as public transport provision can become commercially self-sustaining.</p> <p>Whilst we note page 63 of the masterplan offers warm words: “Connectivity is central to the design focus at Stainsby. Bus routes will be integrated along the main Primary Road and Secondary road to ensure that no dwelling is more than 400m from a bus stop. These routes will form part of the larger Middlesbrough network creating a sustainable transport network for door to door connections.... Bus routes and details will be advanced with key partners throughout the detailed transport network development process,” we are concerned that this commitment is entirely infrastructure based, and as such makes no recognition of the necessary bus service revenue support that will be required.</p> <p>We consider that, ultimately, a development of some 1,125 units should be sufficient to sustain a commercial bus service once it is built out and occupied. However, we equally recognise it will be many years before the development reaches such a point of sustainability. We are thus greatly concerned that the masterplan does not seek to ring fence a firm public transport revenue support commitment for the many years associated with the construction phase of the development, and we consider this element really should be codified as an explicit requirement for planning consent approval within the masterplan.</p> <p>By way of comparison, there are a number of housing development sites being established elsewhere across the region, many of which are much smaller than the Stainsby site, where such a developer contribution is a core dependency within the masterplan framework.</p> <p>We therefore wish to express the considered view that the Stainsby masterplan must be amended to incorporate a definitive developer commitment as part of its framework. Without such a binding requirement, the provision of public transport will undoubtedly be compromised, and resultant travel choices to, from, and within the development will be pre-determined towards unsustainable modes. Not only would this be contrary to national planning policies, it would also present a great disservice to the wider sustainability ambitions of the development.</p> <p>I would note that Stagecoach has a strong proven track-record in the development and delivery of such 'pump-primed' bus services. Our standard operating model has been to offer quotations to developers on a “minimum subsidy” basis, with revenues taken on the bus service accruing to Stagecoach. This sharing of the risk profile has many practical benefits, but most particularly it offers stronger incentives for both parties to encourage sustainable and increased public transport demand. As the quantum of build increases and occupancy rises, the defined developer contribution so reduces; this is achieved by Stagecoach offsetting the revenue generated by customer yield against the costs associated in providing the bus service. Such a proven methodology has been very attractive to sustainable housing developers and local authorities elsewhere in the region, and we can see no impediment as to why such a methodology should also not be adopted at this most significant development at Stainsby.</p> <p>We are encouraged the masterplan provides opportunity for both primary and secondary highways facility through the development. We recognise these primary and secondary streetscapes afford highways gauges of 7.3m and 6m respectively. We would however, respectfully highlight that the rigid width of a modern low floor bus is 2.55m, and that this calculation excludes the vehicle’s wing mirrors, which on a low floor bus can be quite substantial. Once these wing mirrors are included, the effective width of the bus is only fractionally under 3m. We therefore consider that an overall highways gauge of 6m may well prove problematic in the context of comparatively frequent bus journey flows, especially if there is a curvature in the highway. As an aspect of general design, we would assert that any highways traversed by bus services are designed to allow for as commodious an approach as practical, with the offer of gradual turning manoeuvres, with ideally no vertical traffic calming features (or where considered absolutely necessary, to be of a design which is accommodating to regular bus operations): We especially highlight that more commodious highway designs ensure reduced transit times for public transport, and thereby reduce otherwise avoidable delays, ensuring the public transport offer can be as attractive as possible to potential customers, and those moving on to the development. As such, we would be far more comfortable with secondary carriageway gauges of at least 3.3m</p>

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				<p>(6.6m total).</p> <p>We are supportive that the masterplan affords vehicular access at the northern and south-eastern extents of the development site. Given the scale of the development, we support the suggestion that the roundabout link at Mandale Road/ Levick Crescent is enlarged, and is upgraded to signalised traffic control. We would also suggest that a similar methodology may well also be appropriate at the south-eastern corner of the development at Jack Simon Way/ A1044, as it could be expected that this will also require a strong degree of access/ egress.</p> <p>We would also be grateful if further consideration could please be given towards the establishment of a dedicated public transport “bus gate” between the development and the Baldoon Sands area (perhaps as an extension of the Malltraeth Sands residential road?). In recognising there is already a regular, well established, bus service to Baldoon Sands, we consider that the extension of this bus service to the wider Stainsby development could offer a very cost-effective way of developing public transport links, at least during the development phases. Alternatively, if such a bus link were considered inappropriate, we would request the importance of establishing stronger pedestrian/ cycling links between the development and Baldoon Sands than the masterplan current affords. Whilst such a pedestrian linkage would be towards the upper end of 400m between the development and the existing bus stop provision on Baldoon Sands, we nonetheless consider such an improved east-west connectivity would provide greater connectivity and sustainability to the site than the masterplan currently offers.</p>
Dennis Mccabe				The principles of this development will meet the need for the people living in the area
Emily quinnt				No road necessary. Do you want people and animals to get run over?
Francine Kenny				Yes to Country Park Yes to houses yes to road
Gill Mollard				<p>This housing allocation is not necessary and should stop.</p> <p>Any 'country park elements' should remain within the site already under construction and NOT damage the area on Mandale Meadow, Newfield Meadow or the connecting green spaces by the addition of landscaping features, footpaths, cycle paths, play equipment or 'desire lines'.</p>
Helen McGregor				A primary road going north to south through the estate will enable traffic to travel at greater speed. A loop road will limit speed and also volume of traffic which will only have 3 miles to travel onto the A19 before reaching Mandale roundabout.
Hilary Morley		Hilary Morley	Resident	Refer to chapter 1.0
Ian Etherington				Would like to see the council push for better house style designs from housebuilders. Would also hope to discourage vehicle use as much as possible.
Ian Marron				Under section 6.1 of the primary route, in the first chapter change the speed limit to a max of 20 mph as per the other roads in the Acklam area. Remove reference to A1130 North road.

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jamie barber				frankly, none of this should go ahead. you're planning to destroy natural green space and animal habitats that are at a premium in Middlesbrough.
Jill Hadfield				No road through Mandale Meadows
Joanne Mitchell				<p>The A19 has already been widened at some cost in terms of physical and mental health and the general well being of residents. Issues exist which require resolution regarding noise and air pollution and ought not to be made worse still by this imposition.</p> <p>The road would destroy the meadow.</p> <p>These sad plans they have to ruin one of the most beautiful parts of Middlesbrough not to mention the habitat of wildlife that will disappear without the meadow. Greenspaces are very precious and need to be kept as they are. Years of conservation and preservation work has been invested. People moved close to the meadow being given to understand it is protected, a greenbelt where no building or any development may ever take place. It may never have been a designated greenbelt but it was the subject of a Deed of Covenant, the land having been gifted to the people of Middlesbrough in perpetuity. A road is being planned through the meadow to service a housing estate. At the end of the day destroying a pristine meadow which is rich in biodiversity and has never been 'treated' with pesticides and herbicides, is nothing short of ecocide and vandalism and makes a laughing stock of the council's One Planet Living and green strategies.</p> <p>There was always another option, the 'developers' could create an estate loop road similar to the one at Trimdon.</p> <p>The council has stated they won't even pay local residents a penny in compensation for taking our public amenity from us and subjecting us to increased air and noise pollution, damaging our health.</p> <p>the disruption, destruction, the irreparable damage, and the blatant disregard for people who have lived here and who have been involved during many years in the care and preservation, and who will have their lives destroyed, just beggars belief. Residents already suffer the constant excessive traffic noise...and the pollution inflicted upon them. To think that there are those with the power who lack the empathy and the understanding, or thought and consideration for people who use the meadow and those who live close by is unbelievable.</p> <p>The residents in the Ayresome ward would no longer be able to access Mandale meadow from Mandale Road. It will be impossible for people in the Ayresome ward, including those of us who live a 2 minute walk from the current entrance to Mandale meadow is because it would involve walking straight across two lanes of the proposed roundabout. The road on OUR meadow stopping us accessing OUR meadow is the issue. All this nonsense about "you are getting extra greenspace" is an insult when you are planning something which prevents us from accessing OUR green space.</p> <p>The remarkable thing about this is some of the language used in the document to describe the proposed plan: "To create a diverse ecosystem" etc when one already exists that will be destroyed by the building of so many houses. The plan to create this "diverse ecosystem" on land which has been polluted by herbicides, pesticides and pollution from the building process while destroying a pristine meadow which is genuinely a diverse ecosystem containing Red and Orange endangered species marked on this map of Mandale meadow. They know this because the map and the legend were included in the Greenstuff response to the 2018 LP.</p> <p>The area is home to rare species flora and fauna, which have taken decades to fully establish, all of this would be destroyed.</p> <p>The faux manufactured country park, cannot, and will never, as if by magic, replace this.</p> <p>Mandale Meadow is well used and loved by many, an accessible place in which we are able to walk and relax.</p> <p>Residents currently suffer excessive noise and air pollution levels 24/7 owing to the proximity of the A19...how can it possibly be considered in any way acceptable to bring yet another road even closer to homes.</p> <p>When the A19 was widened residents received compensation for the loss in value of their homes..but despite the proposed road being routed even closer we are told we would not receive any such award for all of our losses. ..the loss of peace, and of mental and physical health. .. These of course can never be compensated for. It is not what we want.</p> <p>The notice delivered to each resident shows a basic outline of the area concerned, part of which is Mandale Meadow at the top of the 'plan'. The notice gives the impression that the entire area will be Country Park, along with which would come 1700 houses, not forgetting the so called Relief Road, to 'relieve' congestion on the A19 and serve the 1700 new homes. The plan actually shows the road through Mandale Meadow which would discharge traffic onto Mandale Road, at a junction adjacent to residents homes where the current car park is now situated, actually at the foot of their gardens.</p>

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Joanne Sellers				Ive noticed on the plan for the park area that theres an area for young people and children. In Oct/Nov 99 I emailed the mayor with a suggestion for a separate area for teens alongside playgrounds for younger children that Id seen in Budapest. It was basically several different types of benches with charge points for mobiles and as it was budapest, small solar panels on the top to power them. I thought it was an excellent idea if there was funding available and I cant see it as being massively expensive. Unfortunately the mayor didnt bother replying however if you wish to see the photos of the benches I took whilst I was there I would be happy to share. I think we should have something for young people which also takes them out of the play areas meant for younger children.
John David Duffy				<p>This bit is great because it provides codes that developers will need to abide by. I am not that interested in this section as I will not be looking to purchase a property in this area, but I will be using the green open space provided by the development of these houses.</p> <p>The road looks to have been designed to include speed reducing measures. I really like the ideas of crossing points as this will make drivers think twice about speeding in these areas, plis the roundaboutys will provide natural speed reduction.</p> <p>I think parking has also been thought about. The main road looks to have no housing served from it and the local centre seems to provided parking facilities. I cannot see a reason why people would park on this road, meaning it will be freed up to support what it is there to be done.</p>
Jonathan Hills				<p>How will the creation of the park affect the residents currently living in the boundary?</p> <p>Will they still be dependent on private landscaping companies or green area maintenance?</p>
Judith Harbron				No development
Judith Maunder	Friends of Mandale, Greenstuff, resident very local to Mandale Meadow			<p>Please leave Mandale meadow alone and DO NOT put a road through it.</p> <p>p59, no country park has a major road through it. An estate road only was in the original plan and that is how it should remain. Residents of phase one who were told it would be a dead end estate road are deeply unhappy about a 'spine road' roaring past their houses. They have been lied to or misled. The spine road should not happen. Let South Middlesbrough solve it's own problems, not dump them on Ayresome ward residents.</p> <p>there should absolutely not be a bus route through Mandale meadow. Use an estate loop road.</p> <p>p60, 6.1, there should only be estate roads with a maximum speed of 20 mph.</p> <p>6.2.1. the herbage by the roads should be a permanent fixture and not an excuse to widen the roads in the future.</p> <p>6.3 putting a bus linkage on Mandale meadow is an absolute outrage. it will create further dangers and noise and air pollution for residents.</p> <p>p64. lighting is bad for wildlife - roosting birds etc.</p> <p>p65, the islands in the road could pose an even greater danger to wildlife</p> <p>p66, secondary roads going over green corridors stops the corridors from being green as they become death traps for wildlife</p> <p>p69, how will Ayresome ward residents access the 'country park' when it is impossible to walk near a busy road because of lung disease when there is no parking on the primary road - if the foolish plan to put a road through Mandale goes ahead?</p>

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Julia Povey				We can't control what happens on privately owned land but there should be NO ROAD THROUGH MANDALE MEADOW/WOODS.
Julie Riley				NO ROAD THROUGH MANDALE
Kabaser Ajaib	Retailing	Kabaser Ajaib	Retailing	Encourage wildlife don't endanger it
Keith Russell				No road through Mandale Meadow
Keith Young				No road through Mandale, you can do what you like on the private land
Lauren Teate				Page 60. Chapter 6.1 - Maximum speed of primary road should not exceed 20mph, especially where it enters the Country Park. A road should not be cutting through a Country Park at all. Page 70. Chapter 6.10.1. There must be gaps put in the fencing to allow hedgehogs to move from garden to garden. The developers have to be held responsible for this since fragmentation of habitat is the main cause for hedgehog population declines over recent years. A high population of hedgehogs use the farm fields and Bluebell Beck and something as simple as a hole in the fences can benefit them incredibly. This site offers training and advice for developers. https://www.hedgehogstreet.org/about-our-hedgehog-street-campaign/wider-work/
Leah Davey				This housing allocation is not necessary and should stop. Any 'country park elements' should remain within the site already under construction and NOT damage the area on Mandale Meadow, Newfield Meadow or the connecting green spaces by the addition of landscaping features, footpaths, cycle paths, play equipment or 'desire lines'.
Liam Harris				Please see sheet 1
Linda Barker				This housing allocation is not necessary and should stop. Any 'country park elements' should remain within the site already under construction and NOT damage the area on Mandale Meadow, Newfield Meadow or the connecting green spaces by the addition of landscaping features, footpaths, cycle paths, play equipment or 'desire lines'. This site allocation will cause SIGNIFICANT detrimental impacts to the existing wildlife site at this location.
Lynn Blagg				Leave site as it is to enable residents and others to use it in its natural state for exercise and pleasure and avoid destruction of wildlife habitats and ageing flora and fauna and avoid excess traffic in the area.

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M. E. Bailey				<p>*p59, 6. It isn't possible to prioritise pedestrians where there are roads. No road through Mandale meadow ensures public safety from vehicular accidents and lung and heart damage from toxic emissions. It has been calculated that less than 50% of people will be using electric cars by 2040 so that is a lot of people's health the council will damage through toxic emissions. You need roads on a housing estate, you do not need them on the existing open public green spaces. A spine road through Mandale is dangerous for residents and wildlife. We have deer, hedgehogs and a variety of small mammals on Mandale woodland and Farley Copse which will die on the road. We need to start protecting nature and wildlife for the sake of residents and the planet.</p> <p>*p60,6.1, 30mph is far too fast for the new estate, it should be 20mph or less on all roads. 'Strategies' to slow traffic don't work as feckless drivers see it as a form of a Brands Hatch race course.</p> <p>No Stainton Way Western Extension through Mandale meadow.</p> <p>'improving the future resilience of the local road network' This is not necessary as people will not return to the same way of working due to the covid pandemic, more work will be done at home rather than commuting to an office.</p> <p>*p61, 6.2 It's interesting that the combined width of the multi use path and the green wedge in between the road and the path are wide enough to create a dual carriageway at some future date. Pual Clarke is on record as saying a dual carriageway is a future possibility if there is a significant increase in house building in South Middlesbrough. No Road through Mandale, a loop road to serve the new estate only.</p> <p>*p61/62, the paths around the estate are too wide and take up to much valuable green space.</p> <p>*p63, 6.3. residents of the Ayresome ward do not want buses coming over the meadow and certainly not a bus hub in the middle of the meadow. This is ecocide.. No road through Mandale.</p> <p>*p64. the multi use path is not a good idea.</p> <p>*p65, 6.5.1, it's hard to see how pedestrians have priority on a 30mph road with no zebra crossings. No road over Mandale meadow.</p> <p>*p66, a road through a country park is an oxymoron and isn't safe for pedestrians or wildlife.</p> <p>*p68, p69, 6.7, the parking designs seem clunky and unrealistic and look like a throwback to Victorian housing when people only had access to garages/stables via the back alleys. These days households generally have multiple cars and should be able to access drives/garages from the front aspect of their homes. All new homes should have electric charge points for EVs at the front of their homes.</p>
Malcolm Chilvers				<p>Like many people I have neither the time nor inclination to be able to read and absorb the information contained in all these chapters unlike yourselves who are being paid to do it.</p> <p>"The key aim here is to create spaces that continually link to the wider country park green network, prioritising the public pedestrian movement over vehicular access." by putting a main road through what are already pedestrian areas.</p>
Malcolm Watson				please see comments in attached file

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Malcolm Watson				<p>We have seen how builders trample over the Council's plans, most recently when the number of buildings on the St David's site was increased at the behest of the builder from the 115 allocated in the Local Plan to a new total of 139 - an increase of 17%. What safeguards are there to ensure that the same thing cannot happen with Stainsby ? At the 2019 consultation exhibitions I asked the same question and was told that anything the builders put forward as plans would have to be agreed by the Council's Planning Department and would have to comply with the Local Plan but that is patently not always the case. Part of the justification for the Stainsby development is to provide "The mix, style and layout of the houses must create a high quality and spacious development" (para 6.6). An increase in the number of houses could only be achieved by lowering the tone - making them smaller, making them closer together etc</p> <p>The Masterplan contains many aspirational pictures but if the builders choose to not include trees, cycle paths, green spaces etc in their plans, what level of veto does the Council realistically have ?</p>
Margaret Jackson				<p>6.3 Bus Connectivity Although there is the intention to provide a bus service for the new development, as it currently stands this will depend on private companies deciding it is profitable for them to provide the service. One of the difficulties non-driving residents of Ingleby Barwick have is the lack of buses in the evenings, and a similar experience can be expected for this area too.</p>
Matthew Shippey				<p>Don't want this. I walk in mandale meadow. Middlesbrough is lacking in green space and you want to build a road through to facilitate new housing. Middlesbrough Council you are a disgrace.</p>
Michael Miller	Natural England			<p>Natural England promotes the use of Green infrastructure and integration into Urban development principles.</p>
Michael Starford				<p>6.2 Highway – the combined primary road/cycleway is wide and open. It may encourage off road "joy riding"; a hazard on the green area in the past.</p>
Michelle Hedger				<p>No road through Mandale Meadow. Use the loop road option. If a road needs to be there make it a narrower road of 20mph. My children cannot play near the huge 30mph road planned. They can play now in the meadow. They will be hit by a car in this 'country park'. Please don't put all the footpaths in. We like to follow the natural winding dirt tracks. There is only lip service paid to these. We don't want artificial paths. It's beautiful, muddy and filled with nature.</p>
Mike Conyard				<p>Do not build on this land. This is much needed green space. There are no parks in this area and nowhere for people to walk, kids to play and people to live their lives. I find it disgusting that you propose to build on this beautiful piece of land. Also, who wants to live right next to the A19? These house will be worth literally nothing.. The land is also a buffer zone from the A19 to the residents of Heythrop Drive and beyond.</p>

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Miller Homes and Avant Homes	Miller Homes and Avant Homes	Mr Neil Morton	Savills	<p>As a general comment, both Chapter 6 Urban Principles and Chapter 7 Urban Design Studies, provide a significant level of detail on the development proposals for the allocated site which in our view goes beyond the role of the document as guidance. In our view this level of detail and some of the language used appears unnecessarily prescriptive. The final masterplan would benefit greatly by making it clear that the information is for illustrative purposes only and that other forms of design which also achieve high quality development would be equally acceptable. Planning applications and their housebuilders who have strong track records of delivering exemplary places, should be permitted an appropriate level of flexibility to develop their own designs where these would also achieve good design without being restricted by the “requirements” and “bars” of the masterplan. As paragraph 126 of the NPPF makes clear that within plans and SPDs:</p> <p>their level of detail and degree of prescription should be tailored to the circumstances in each place, and should allow a suitable degree of variety where this would be justified.” With this in mind, we do not provide another response to all of the very detailed and lengthy design information and specifications set out (we do however enclose a copy of previous representations submitted by Lichfields on behalf of the site developers which should be taken forward as submissions to this consultation as the points remain valid), which includes items such as primary and secondary footpath/roads widths, homes zones, private drives, pavements, cycle routes, crossings, housing mix and layout, parking, garages, boundary treatments, etc., other than to say that these are all matters which will be examined in detail as part of the planning application process and the masterplan, as design guidance, should not dictate or prescribe the final details where other proposals also acceptable and achieve good design. Chapter</p>
Nichola Davey				<p>This housing allocation is not necessary and should stop. Any 'country park elements' should remain within the site already under construction and NOT damage the area on Mandale Meadow, Newfield Meadow or the connecting green spaces by the addition of landscaping features, footpaths, cycle paths, play equipment or 'desire lines'.</p>
Nicola Smith				<p>We DO NOT WANT IT!!!!!! WHY can't you build on areas where you have flatted hundreds of houses but not rebuilt? Keith Road or Marton Burn Road??? This is the only green space we have in Acklam and hundereds of residents rely on this being walking distance. Pre covid the traffic on mandale Road, Acklam Rd and levick Cresent is bad enough at peak times without further traffic. You have Acklam Grange Secondary school round the corner from the meadow with 1200+ students that also use these roads. This is already a very busy junction without diverting more traffic to it. What about adults and children's mental health?? Has the past year taught you nothing?? I think having a open greenspace with all the wildlife and plenty of room to exercise and get much needed fresh air and been an absolute saviour to hundereds of local residents. This project has not considered anything but money. Looking at the plans there will be very little green left and it will be surrounding a road and housing estate. Absolutely nothing like we have now. This is NOT ACCEPTABLE FROM ANYONE I KNOW WHO LIVES IN ACKLAM 😞😞😞😞😞😞😞😞</p>
Nicolas Mitchell				<p>Would an underpass rather than the crossing planned avoid cars and people clashing as is the case of the walkway under Trimdon Avenue. I know that needs cheering up and lighting maybe restoring... but it this a pedestrian route over the road as in Coulby Newham? Not sure that will enhance the appearance?</p>

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Peter Davey				This housing allocation is not necessary and should stop. Any 'country park elements' should remain within the site already under construction and NOT damage the area on Mandale Meadow, Newfield Meadow or the connecting green spaces by the addition of landscaping features, footpaths, cycle paths, play equipment or 'desire lines'.
Poppy Bowyer				Will privacy of the estate be compromised? Could the feeling of an encroaching motorway and public access put potential buyers off?
Rob Atkin				No road through Mandale.
Ronald Dennison				This plan clearly shows that this is not an open Country Park: it is a housing estate with 1500 homes ("homes within a park", according to paragraph 1.3.1), with public-access open land to the west and east. Paragraph 1.3.2 calculates that the Country Park consists of 53% of the total development area. But some of this land is grass verge, urban trees, urban bushes etc - not "Country Park". I object to the building of houses within this Country Park.
Sofia Torre				On average a house with four to five bedrooms will have more than 2 cars, my 3 bedroom semi detached has to have 3 parking spaces, in the garage, on the drive and in front of the house. So you will restrict the number of cars a property will have, come on wake up and smell the coffee, its going to be one mess, green land changed into a huge carpark.
Sonia Bowler				The description of the primary road suggests a width of 7.3m with a landscaped zone either side of it with a minimum width of 3m and also a cycle path of minimum width 4.5m At the junction with the A1130 roundabout I would expect there to be some sort of pedestrian access to the site also so then a further path width of approximately 2m on either side of this. That would make a total width of the entrance road, accompanying landscaped zone, cycle path and footpaths at the A1130 junction of 21.8m! Will there be any of Mandale Meadow left? No road through Mandale Meadow!
Stephanie Atkin				No road through Mandale Meadow.
Stephen Connors				Thoughts previously mentioned
Steve Gorman				With regards to the housing mix I would like to stress, we have a fabulous opportunity here to do something different in the way of housing design. I don't want to see a mini Ingleby Barwick, by that I mean everything looks the same. I want to see design and technology work hand in hand, Eco designed with the latest technology to not only make the home look attractive, but to be innovative as well in design and appearance. Do we dare to be different???

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Steven Brown				<p>6.0 URBAN STRATEGY 6.7 Parking Considerate parking design is essential throughout the development to minimise the impact of vehicles on the landscaped masterplan and urban design principles.</p> <p>What provisions will be made to deter cars parking on pavements like all the other new estates.</p> <p>6.0 URBAN STRATEGY 6.11 Boundary Treatment Indicative Study</p> <p>Stainsby Hall Farm residents and adjacent new build should have at least had trees planted not just a basic timber fence</p>
stewart jones				Agree
The Countryside Charity North Yorkshire CPRENY	CPRENY			It is considered that the parking sections within section 6 and 7 of the 'Urban Strategy' chapters should include reference to the provision of electric vehicle charge points. Not all houses will have garages with suitable charge points, therefore, given the Government's commitment to electric vehicles it is considered that thought should be given to how owners should charge vehicles, especially in those areas where frontage parking is being discouraged and landscape and boundary treatments are being proposed at a detailed level.

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Thomas Maunder				<p>*p59, 6. It isn't possible to prioritise pedestrians where there are roads. No road through Mandale meadow ensures public safety from vehicular accidents and lung and heart damage from toxic emissions. It has been calculated that less than 50% of people will be using electric cars by 2040 so that is a lot of people's health the council will damage through toxic emissions. You need roads on a housing estate, you do not need them on the existing open public green spaces. A spine road through Mandale is dangerous for residents and wildlife. We have deer, hedgehogs and a variety of small mammals on Mandale woodland and Farley Copse which will die on the road. We need to start protecting nature and wildlife for the sake of residents and the planet.</p> <p>*p60,6.1, 30mph is far too fast for the new estate, it should be 20mph or less on all roads. 'Strategies' to slow traffic don't work as feckless drivers see it as a form of a Brands Hatch race course.</p> <p>No Stainton Way Western Extension through Mandale meadow.</p> <p>'improving the future resilience of the local road network' This is not necessary as people will not return to the same way of working due to the covid pandemic, more work will be done at home rather than commuting to an office.</p> <p>*p61, 6.2 It's interesting that the combined width of the multi use path and the green wedge in between the road and the path are wide enough to create a dual carriageway at some future date. Pual Clarke is on record as saying a dual carriageway is a future possibility if there is a significant increase in house building in South Middlesbrough. No Road through Mandale, a loop road to serve the new estate only.</p> <p>*p61/62, the paths around the estate are too wide and take up to much valuable green space.</p> <p>*p63, 6.3. residents of the Ayresome ward do not want buses coming over the meadow and certainly not a bus hub in the middle of the meadow. This is ecocide.. No road through Mandale.</p> <p>*p64. the multi use path is not a good idea.</p> <p>*p65, 6.5.1, it's hard to see how pedestrians have priority on a 30mph road with no zebra crossings. No road over Mandale meadow.</p> <p>*p66, a road through a country park is an oxymoron and isn't safe for pedestrians or wildlife.</p> <p>*p68, p69, 6.7, the parking designs seem clunky and unrealistic and look like a throwback to Victorian housing when people only had access to garages/stables via the back alleys. These days households generally have multiple cars and should be able to access drives/garages from the front aspect of their homes. All new homes should have electric charge points for EVs at the front of their homes.</p>
Trevor King				<p>We can't control what happens on privately owned land but we want NO ROAD THROUGH MANDALE MEADOW/WOODS.</p>
Val Young				No road through the meadow

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	Story Homes	Chris Smith	Lichfields	<p>Urban Strategy Road Types</p> <p>3.7 The document includes considerable detail in the specification for road types but then references on page 61 that “All designs will be developed in conjunction with Highway Designers & Transport Engineers to ensure quality for the future”. It is therefore unclear if the proposed specifications in the Masterplan are technically sound.</p> <p>3.8 Page 60 in the document refers to ‘Manual for Streets 2’ as the guidance which should be used as a basis to inform the internal layout. It is also unclear if the Masterplan supersedes the requirements in the Manual for Streets 2 guidance or if there is a hierarchy in which the Masterplan stands against other guidance documents. At the detailed design stage, there is likely to be conflicting information in the various guidance documents which should be avoided and simplified.</p> <p>3.9 Pages 61-62 include the required road widths for the various road types. 6m for secondary roads and 5m for tertiary roads seems excessive when 4.7m tends to be the standard road width within residential layouts. The wider roads set out in the document also conflict with the text on page 60 which seeks secondary and tertiary roads to be designed to “naturally restrain vehicles speeds”. Increasing the road widths beyond standard is unlikely to contribute towards this.</p> <p>3.10 In addition, these specifications also exceed the road widths in the development which has already been completed in the southern part of the site. It would be logical for the Masterplan to deliver a consistent approach throughout the whole site.</p> <p>Crossing Points</p> <p>3.11 Pages 64-66 set out the specifications for footpaths and crossing points. It is unclear if bus operators have had any input into these designs. For instance, as shown on page 65, there appear to be a number of crossing points on the primary road but it is important to ensure these designs do not prejudice bus movements.</p> <p>Parking</p> <p>3.12 Page 68 states that: All properties must be provided with parking in accordance with the Tees Valley Design Guide & Specification. In addition to these guidelines the following rules apply:</p> <ul style="list-style-type: none"> - Regardless of size all houses must have a minimum of 2 designated parking spaces. - Garages cannot be counted as a parking space. - Integrated covered parking and open garages can be included within the parking provision. <p>3.13 There does not appear to be any justification to deviate from the Tees Valley Design Guide & Specification and, as explained above, it is unclear if there is a hierarchy in which the Masterplan stands against other guidance documents. It also seems odd that garages cannot be counted as parking spaces whereas integrated covered parking and open garages can be included.</p> <p>3.14 Page 69 then proceeds to set out a garage type matrix with the garage types which will be acceptable within each part of the road hierarchy. Story Homes (and the wider Developer Group) does not agree with the approach to dictate the design principles to this extent. In complying with this matrix it could actually result in street scenes with very little variation or interest. It is standard practice for applicants to submit street scene images or visualisations of key areas within a development proposal and this would allow the Council to assess the appearance of a development proposal at the application stage. The Masterplan could instead encourage the submission of street scene visualisations for areas of the site to be agreed with the Council.</p> <p>Boundary Treatments</p> <p>3.15 Pages 70-73 details the approach for boundary treatments. Whilst the designs on page 70 are logical, page 71 includes some feature wall examples with holes (in walls) and missing panels (in fences).</p> <p>3.16 Story Homes (and the wider Developer Group) does not consider that this will be effective in practice. Boundary treatments essentially serve a purpose to define and provide privacy to private spaces and, if implemented, these holes are likely to be covered up.</p> <p>3.17 If these designs are to be retained in the Masterplan, it should be made explicit that they are optional and not mandatory.</p>
Dave Blackburn				Noise levels from the 1000s of cars that will use the road everyday is going to be worse than the a19 buzz, 5 years of building is going to be unacceptable, as we are in our 70s. Our piece and quite will be shattered. Pollution will be worse so put a loop road in. Resident know that this is likely to be a rat run for commercial vehicles with their racket and petrol fumes.

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Gill Sullivan				<p>MBC have told residents that there was originally 1660 houses earmarked for farmers field. That has now gone up to 1760 due to the 100 taken from mandale. So your capital receipts will not have fallen as far as you state. You will still get the money and council tax. MBC know full well that developers routinely add another 20% of housing to a development as they have in coulby and nunthorpe.. This is 352 extra houses making 2012 houses a big mark up by anyone's standards. So 2012 houses means 4000 adults 3-4000 cars, 2000 children, this will double acklam population. MBC are making significant changes to the towns infra and its population. The agreement for the significant changes which will cause major changes to resident lives was made mainly behind close doors at mbc. Their so called full consultation in 2014 was no such thing. As MBC have done for may years now, they take the least action possible, only what is legally required. This is a democracy MBC have duties to residents, the council serve their residents and o this issue MBC have failed. There is no way on a project of this magnitude that they can justify their mega and poor advertising strategy. Nor can they justify keeping residents in the dark about the thin pencilled line representing the spine road. the new residents were told it was an estate road. So at least MBC are constant in their dis information.</p> <p>More road equals more nature destroyed. All road are a barrier to wildlife. This road service road, through mandale will destroy and fragment our wild habitats. The noise and light pollution will cause unesimated damage too. As the character is changed for ever, never to be a peaceful haven for our wild life. it speaks volumes for our council and our wider society when a road becomes more important then a major part of acklam heritage and our landscape.</p> <p>Gov Council and Developers are over reaching themselves with scant regards for residents needs and wishes.. This must stop before Middlesbrough is one big housing estate. Somewhere one would not choose to live. as all its redeeming features will be under brick and concrete. deeply unattractive.</p>

Question 8 – Please provide any comments you may have on Chapter 7.0 Urban Strategy Studies

Please provide any comments you may have on Chapter 7.0 Urban Strategy Studies				
Name	Organisation (where applicable)	Agent	Organisation	Please provide any comments you may have on Chapter 7.0 Urban Strategy Studies
Aji Varghese		Aji Varghese		Good
angela barber				<p>AS a resident of this area, I fear my house value will lower so close to this new road.</p> <p>There will be No where to walk my dog</p> <p>There would be Little or no space for children to play safety in area</p> <p>An area to exercise my self and enjoy picnics with the family</p> <p>Local wild life will loose there home and endangered species plants and animals will loose there environment</p> <p>Why should be suffer ill health because of further pollution</p> <p>We pay council tax for green space- will our council tax lower-expect not</p> <p>Increased noise during the day and night- lack of sleep and depression result</p> <p>All you want to do is provide new access road to the estate- why cannot they come around down downside and Acklam road instead as always has happened. I feel the new road will increase traffic into Middlesbrough through our area.</p> <p>I do not want or need the road as I do not drive- I doubt it will be included on the bus route, so it does me no good.</p> <p>In fact everything about it is BAD</p>
Angela O'Byrne				Hands off our meadow!
Barry pinkney				Country park no
Ben Stephenson	Persimmon Homes Teesside			<p>Section 7.1.3 Secondary Road Condition 1</p> <p>There are discrepancies between what is explained within the text, what has previously been explained earlier within the document in respect of Secondary Roads and what is shown in the section drawing accompanying the text in relation to parking requirements, verge widths and shared footpath surfaces widths. The Masterplan should be clear in its expectations.</p> <p>Section 7.1.6 Home Zone Condition</p> <p>Usually as the priority of the road reduces within the hierarchy so does the width of the street to create more enclosed, pedestrian focused areas to encourage slower vehicular speeds. As evidenced by the various section drawings the hierarchy in this case is expected to widen. Persimmon Homes would query whether this is intentional.</p> <p>Section 7.2.4 Landscape Edge Condition 4</p> <p>Whilst it is appreciated that this section aims to primarily deal with the transition of properties around a corner with the open space, the image and section drawing fail to identify how suitable parking provision can be integrated within this arrangement. It suggests that rear gardens of 4m in depth will be acceptable on plots fronting out over the public open space.</p>
Carol Newmarch				I object to the whole plan

Please provide any comments you may have on Chapter 7.0 Urban Strategy Studies				
Name	Organisation (where applicable)	Agent	Organisation	Please provide any comments you may have on Chapter 7.0 Urban Strategy Studies
Chris King				<p>Roads (Circulation and Access)</p> <p>A series of studies have been undertaken to give examples for application of the varying road hierarchies in relation to defining the development. The extent of possible applications haven't been exhausted but begin to set a standard of guidance that is acceptable for developments. Further options can be developed alongside this guidance to be reviewed by Middlesbrough Council.</p> <p>I understand that this is largely concerned with details within the proposed development but the edges also include those areas in which the project interacts with neighbouring areas. Information on such access points, such as the one with Mandale would have been most informative.</p>
Christine King				We can't control what happens on privately owned land but we want NO ROAD THROUGH MANDALE MEADOW/WOODS.
Christopher Bieda				<p>AS PER THIS QUOTE FROM THE DOCUMENT</p> <p>"Where building frontages face on to a Primary Road, there should always be a private access road or shared surface access set back from the Primary road behind a min 6m landscaping zone. This landscaping buffer will create separation so no dwellings can be access directly from any Primary Road. No front of house parking is permitted in these areas, all parking should be in-curtilage."</p> <p>Housing already faces onto the main road so this point is both out of date and irrelevant. Thus the road study cannot be considered complete and acceptable. A reduction in speed limit is required.</p>
Claire Boyd				We can't control what happens on privately owned land but there should be NO ROAD THROUGH MANDALE MEADOW/WOODS.
Claire Di Cicco				No to a road.
CLAIRE KANE				The pictures are misleading it will be a black tarmac road for people to speed up and down destroying the habitats and wildlife - we already have foxes owls and bats around the area that are walking and flying around our 40 + year old estate because they have had their habitats destroyed by the new houses on Jack Simon Way estate
Claire Wilks				No road

Please provide any comments you may have on Chapter 7.0 Urban Strategy Studies				
Name	Organisation (where applicable)	Agent	Organisation	Please provide any comments you may have on Chapter 7.0 Urban Strategy Studies
Colin and Morag Bailey				<p>*p76 7.1.1. There should be no primary road through Mandale meadow. 'Where building frontages face on to a Primary Road, there should always be a private access road or shared surface access set back from the Primary road behind a min 6m landscaping zone.' This is a good strategy for the aesthetics of an estate but not sure it is sufficient for a wildlife refuge and as green corridors they are really not sufficient, 15 to 20m would be more realistic.</p> <p>*p84, 7.2.1. New homes should have green energy heating as standard as it will not be possible to buy new gas boilers in just four years time (2025), this should be done before drives are constructed.</p> <p>*p86, not keen on drives behind houses, many people will not be prepared to pay a high price for a new home with shared rear parking. A 10m rear garden is very small, they ought to be larger.</p> <p>*p90, 7.3. and 7.3.1 there should be no nodal points on Mandale meadow. No spine road through Mandale meadow.</p> <p>*92, it's not a good idea to have car parking in or next to back gardens because of toxic pollution in private seating areas.</p>
Emily quinnt				No road necessary. Do you want people and animals to get run over?
Francine Kenny				Yes to Country Park Yes to houses yes to road
Gill Mollard				<p>This housing allocation is not necessary and should stop.</p> <p>Any 'country park elements' should remain within the site already under construction and NOT damage the area on Mandale Meadow, Newfield Meadow or the connecting green spaces by the addition of landscaping features, footpaths, cycle paths, play equipment or 'desire lines'.</p>
Helen McGregor				Instead of the primary road cutting through Mandale meadow and woods, I suggest it is built at a loop round the edges of the new development which would not interfere with the housing plans. This would also preserve the cherished wooded and meadow area near the Mandale roundabout.
Hilary Morley		Hilary Morley	Resident	Refer to chapter 1.0
Ian Marron				<p>Under section 7.1 page 75 remove reference to North and show proposed 2nd South link primary road onto B1363 adjacent Stainsby hall farm.</p> <p>Adjust figures/ diagrams to remove any link to Mandale Road.</p>
jamie barber				frankly, none of this should go ahead. you're planning to destroy natural green space and animal habitats that are at a premium in Middlesbrough.

Please provide any comments you may have on Chapter 7.0 Urban Strategy Studies				
Name	Organisation (where applicable)	Agent	Organisation	Please provide any comments you may have on Chapter 7.0 Urban Strategy Studies
janice slater				<p>Urban strategy studies ... roads 7.1 potential expansion position</p> <p>An extension to the proposed road may be required to serve the site as a whole, accessing the site from its northern boundary. This is subject to AREA TRAFFIC ASSESSMENTS AND CONSULTATION WITH THE HIGHWAYS. As highways have already been consulted with and the SWWE road has been confirmed necessary surely this information has already been gleaned If it hasn't then highways has not completed its site assessment and this consultation is actually premature and therefore unnecessary . If highways has completed its assessment then this information should be available now and I request that the public and myself are given the whole information regarding to the road ...its extension and, or potential width prior to the acceptance of this plan.</p> <p>In the consultation report from Lewis Young dated 23/1/19 Stainsby draft.. it was mentioned that one of the positives from the event consultation was that .. officers were able to tackle the misinformation that the SWWE would be dual carriageway. It was also stated that the proposed width of the SWWE road was 7.3 metres. Could confirmation of these two statements be forthcoming ... to allow the public to actually visualize the extent and size of the road especially with reference to the item above which is in the new plan and refers to extension of the new road.</p> <p>Referring to traffic speeds within the country park and housing estate.. As the whole of Middlesbrough has undergone a 20 MPH speed limit in All residential areas... which is now also enforceable. Is it correct in assuming that this whole estate ..(especially as it is not only a family housing estate but will also be a well visited country park enjoyed by residents and locals and visitors).....will also be a 20 MPH zone throughout the whole area.?????? all this information should be made available to the public prior to any formal acceptance of the Stainsby country park master plan.</p>
Jill Hadfield				No road through Mandale Meadows

Please provide any comments you may have on Chapter 7.0 Urban Strategy Studies				
Name	Organisation (where applicable)	Agent	Organisation	Please provide any comments you may have on Chapter 7.0 Urban Strategy Studies
Joanne Mitchell				<p>The A19 has already been widened at some cost in terms of physical and mental health and the general well being of residents. Issues exist which require resolution regarding noise and air pollution and ought not to be made worse still by this imposition.</p> <p>The road would destroy the meadow.</p> <p>These sad plans they have to ruin one of the most beautiful parts of Middlesbrough not to mention the habitat of wildlife that will disappear without the meadow. Greenspaces are very precious and need to be kept as they are. Years of conservation and preservation work has been invested. People moved close to the meadow being given to understand it is protected, a greenbelt where no building or any development may ever take place. It may never have been a designated greenbelt but it was the subject of a Deed of Covenant, the land having been gifted to the people of Middlesbrough in perpetuity. A road is being planned through the meadow to service a housing estate.</p> <p>At the end of the day destroying a pristine meadow which is rich in biodiversity and has never been 'treated' with pesticides and herbicides, is nothing short of ecocide and vandalism and makes a laughing stock of the council's One Planet Living and green strategies.</p> <p>There was always another option, the 'developers' could create an estate loop road similar to the one at Trimdon.</p> <p>The council has stated they won't even pay local residents a penny in compensation for taking our public amenity from us and subjecting us to increased air and noise pollution, damaging our health.</p> <p>the disruption, destruction, the irreparable damage, and the blatant disregard for people who have lived here and who have been involved during many years in the care and preservation, and who will have their lives destroyed, just beggars belief. Residents already suffer the constant excessive traffic noise...and the pollution inflicted upon them. To think that there are those with the power who lack the empathy and the understanding, or thought and consideration for people who use the meadow and those who live close by is unbelievable.</p> <p>The residents in the Ayresome ward would no longer be able to access Mandale meadow from Mandale Road. It will be impossible for people in the Ayresome ward, including those of us who live a 2 minute walk from the current entrance to Mandale meadow is because it would involve walking straight across two lanes of the proposed roundabout. The road on OUR meadow stopping us accessing OUR meadow is the issue. All this nonsense about "you are getting extra greenspace" is an insult when you are planning something which prevents us from accessing OUR green space.</p> <p>The remarkable thing about this is some of the language used in the document to describe the proposed plan: "To create a diverse ecosystem" etc when one already exists that will be destroyed by the building of so many houses. The plan to create this "diverse ecosystem" on land which has been polluted by herbicides, pesticides and pollution from the building process while destroying a pristine meadow which is genuinely a diverse ecosystem containing Red and Orange endangered species marked on this map of Mandale meadow. They know this because the map and the legend were included in the Greenstuff response to the 2018 LP.</p> <p>The area is home to rare species flora and fauna, which have taken decades to fully establish, all of this would be destroyed.</p> <p>The faux manufactured country park, cannot, and will never, as if by magic, replace this.</p> <p>Mandale Meadow is well used and loved by many, an accessible place in which we are able to walk and relax.</p> <p>Residents currently suffer excessive noise and air pollution levels 24/7 owing to the proximity of the A19...how can it possibly be considered in any way acceptable to bring yet another road even closer to homes.</p> <p>When the A19 was widened residents received compensation for the loss in value of their homes..but despite the proposed road being routed even closer we are told we would not receive any such award for all of our losses. ..the loss of peace, and of mental and physical health. .. These of course can never be compensated for. It is not what we want.</p> <p>The notice delivered to each resident shows a basic outline of the area concerned, part of which is Mandale Meadow at the top of the 'plan'. The notice gives the impression that the entire area will be Country Park, along with which would come 1700 houses, not forgetting the so called Relief Road, to 'relieve' congestion on the A19 and serve the 1700 new homes. The plan actually shows the road through Mandale Meadow which would discharge traffic onto Mandale Road, at a junction adjacent to residents homes where the current car park is now situated, actually at the foot of their gardens.</p>

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Name	Organisation (where applicable)	Agent	Organisation	Please provide any comments you may have on Chapter 7.0 Urban Strategy Studies
John and Janet Roebuck				<p>Urban strategy – Roads, circulation and access p60 / Urban strategy studies p75</p> <ul style="list-style-type: none"> • There is no reference to traffic volumes likely to use the primary road, either from the north or the south access points • Does the initial stretch of the Jack Simon Way meet the requirements set out in paragraph 6.1. Primary, including gateway and crossing features and landscape features, given that there is no separate cycle path • Does the initial stretch of the Jack Simon Way, comply with the Primary Road Condition 1 set out on page 76? If not, are alterations going to be made to that part of the road, to address any shortcomings identified? <p>Bus linkages</p> <ul style="list-style-type: none"> • There are no bus stops shown in relation to the existing development on Jack Simon Way. Do the dwellings on the initial stage of the development meet this requirement of being within 400m? • Please explain how a bus priority road will work in relation to the Primary Road, given it is a single carriageway? • Does the plan allow for Bus lay-bys to be provided on the Primary Road?
John David Duffy				This bit is great because it provides codes that developers will need to abide by. I am not that interested in this section as I will not be looking to purchase a property in this area, but I will be using the green open space provided by the development of these houses.
Judith Harbron				No development
Judith Maunder	Friends of Mandale, Greenstuff, resident very local to Mandale Meadow			concept: the more green spaces around housing the better but the planting should have some properly wild elements for the sake of wildlife. p90. a lot of semi mature trees and native shrubs should be planted from the outset.
Julia Povey				We can't control what happens on privately owned land but there should be NO ROAD THROUGH MANDALE MEADOW/WOODS.
Julie Riley				NO ROAD THROUGH MANDALE
Kabaser Ajaib	Retailing	Kabaser Ajaib	Retailing	No through road
Keith Russell				No road through Mandale Meadow
Keith Young				No road through Mandale, you can do what you like on the private land
Leah Davey				This housing allocation is not necessary and should stop. Any 'country park elements' should remain within the site already under construction and NOT damage the area on Mandale Meadow, Newfield Meadow or the connecting green spaces by the addition of landscaping features, footpaths, cycle paths, play equipment or 'desire lines'.
Liam Harris				Please see sheet 1
Linda Barker				This housing allocation is not necessary and should stop. Any 'country park elements' should remain within the site already under construction and NOT damage the area on Mandale Meadow, Newfield Meadow or the connecting green spaces by the addition of landscaping features, footpaths, cycle paths, play equipment or 'desire lines'. This site allocation will cause SIGNIFICANT detrimental impacts to the existing wildlife site at this location.

Please provide any comments you may have on Chapter 7.0 Urban Strategy Studies				
Name	Organisation (where applicable)	Agent	Organisation	Please provide any comments you may have on Chapter 7.0 Urban Strategy Studies
Lynn Blagg				Leave site as it is to enable residents and others to use it in its natural state for exercise and pleasure and avoid destruction of wildlife habitats and ageing flora and fauna and avoid excess traffic in the area.
M. E. Bailey				<p>*p76 7.1.1. There should be no primary road through Mandale meadow.</p> <p>'Where building frontages face on to a Primary Road, there should always be a private access road or shared surface access set back from the Primary road behind a min 6m landscaping zone.' This is a good strategy for the aesthetics of an estate but not sure it is sufficient for a wildlife refuge and as green corridors they are really not sufficient, 15 to 20m would be more realistic.</p> <p>*p84, 7.2.1. New homes should have green energy heating as standard as it will not be possible to buy new gas boilers in just four years time (2025), this should be done before drives are constructed.</p> <p>*p86, not keen on drives behind houses, many people will not be prepared to pay a high price for a new home with shared rear parking. A 10m rear garden is very small, they ought to be larger.</p> <p>*p90, 7.3. and 7.3.1 there should be no nodal points on Mandale meadow. No spine road through Mandale meadow.</p> <p>*92, it's not a good idea to have car parking in or next to back gardens because of toxic pollution in private seating areas.</p>
Malcolm Chilvers				Like many people I have neither the time nor inclination to be able to read and absorb the information contained in all these chapters unlike yourselves who are being paid to do it.
Malcolm Watson				please see comments in attached file
Matthew Shippey				Don't want this. I walk in mandale meadow. Middlesbrough is lacking in green space and you want to build a road through to facilitate new housing. Middlesbrough Council you are a disgrace.
Michelle Hedger				No road through Mandale Meadow. Use the loop road option. If a road needs to be there make it a narrower road of 20mph. My children cannot play near the huge 30mph road planned. They can play now in the meadow. They will be hit by a car in this 'country park'. Please don't put all the footpaths in. We like to follow the natural winding dirt tracks. There is only lip service paid to these. We don't want artificial paths. It's beautiful, muddy and filled with nature.
Mike Conyard				Of all of the areas of waste land in Middlesbrough which are ripe for regeneration, why choice an area of beauty to destroy? Why not redevelop along the river?? Wouldn't that make more sense?
Miller Homes and Avant Homes	Miller Homes and Avant Homes	Mr Neil Morton	Savills	See comments above and enclosures in relation to Chapters 6 and 7.
Nichola Davey				This housing allocation is not necessary and should stop. Any 'country park elements' should remain within the site already under construction and NOT damage the area on Mandale Meadow, Newfield Meadow or the connecting green spaces by the addition of landscaping features, footpaths, cycle paths, play equipment or 'desire lines'.

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Name	Organisation (where applicable)	Agent	Organisation	Please provide any comments you may have on Chapter 7.0 Urban Strategy Studies
Nicola Smith				<p>We DO NOT WANT IT!!!!</p> <p>WHY can't you build on areas where you have flatted hundreds of houses but not rebuilt? Keith Road or Marton Burn Road??? This is the only green space we have in Acklam and hundereds of residents rely on this being walking distance. Pre covid the traffic on mandale Road, Acklam Rd and levick Cresent is bad enough at peak times without further traffic. You have Acklam Grange Secondary school round the corner from the meadow with 1200+ students that also use these roads. This is already a very busy junction without diverting more traffic to it. What about adults and children's mental health?? Has the past year taught you nothing?? I think having a open greenspace with all the wildlife and plenty of room to exercise and get much needed fresh air and been an absolute saviour to hundereds of local residents. This project has not considered anything but money. Looking at the plans there will be very little green left and it will be surrounding a road and housing estate. Absolutely nothing like we have now. This is NOT ACCEPTABLE FROM ANYONE I KNOW WHO LIVES IN ACKLAM 😞😞😞😞😞😞😞😞😞😞</p>
Peter Davey				<p>This housing allocation is not necessary and should stop.</p> <p>Any 'country park elements' should remain within the site already under construction and NOT damage the area on Mandale Meadow, Newfield Meadow or the connecting green spaces by the addition of landscaping features, footpaths, cycle paths, play equipment or 'desire lines'.</p>
Poppy Bowyer				<p>Is the amount of bus stops within the estate too ambitious?</p> <p>Will the area be able to comfortably manage the potential volume of traffic moving through it?</p> <p>Has the amount of traffic that may use the through road at peak times been considered?</p> <p>Will permeable paving, roads and driveways be used?</p> <p>Will greenery and the ponds be the mains sources of drainage?</p> <p>Will feature walls be used for privacy or screening?</p> <p>The design for the street lined green zones, have these been calculated for the project and the 1500 homes or is it a generic design idea?</p> <p>Will all the green edging and open spaces (even on private drives) be council property and responsibility?</p> <p>Will care be taken to not over manage green zones?</p> <p>420 places within the school and nursery. Was this a good plan? Does this provide a suitable solution to the brief?</p> <p>With the potential for a much larger volume of applications of children from the estate will this create problems further down the line?</p> <p>Does this address the issues of suitable schooling faced within the area for many years now? Or could this lead to further problems of overcrowding within the area? What are all the alternatives?</p>

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Rob Atkin				No road through Mandale.
Sofia Torre				encourage ownership of the public realm : excuse me we had that area for the people of Middlesbrough and when we said no, you still did what you wanted. The only way you are going to encourage ownership is to have high walled and gated with a security patrolling. People will not pick up litter, but hey ho initially you will have the service and then remove it because its not cost effective and not part of the £3million revenue you are going to make, what about our streets, we have residents doing your job. Frontages so that the green open space feels like it isn't limited to outside of the development lines. We had that now you are cluttering our green space not only for non diverse residents but cluttered with a polluting spine road
Sonia Bowler				The primary road and pathways appear to be extremely close to the exiting properties near the junction of Heythrop Drive and the A1130! No road through Mandale Meadow!
Stephanie Atkin				No road through Mandale Meadow.
Stephen Connors				Thoughts previously mentioned
stewart jones				Agree
Thomas Maunder				*p76 7.1.1. There should be no primary road through Mandale meadow. 'Where building frontages face on to a Primary Road, there should always be a private access road or shared surface access set back from the Primary road behind a min 6m landscaping zone.' This is a good strategy for the aesthetics of an estate but not sure it is sufficient for a wildlife refuge and as green corridors they are really not sufficient, 15 to 20m would be more realistic. *p84, 7.2.1. New homes should have green energy heating as standard as it will not be possible to buy new gas boilers in just four years time (2025), this should be done before drives are constructed. *p86, not keen on drives behind houses, many people will not be prepared to pay a high price for a new home with shared rear parking. A 10m rear garden is very small, they ought to be larger. *p90, 7.3. and 7.3.1 there should be no nodal points on Mandale meadow. No spine road through Mandale meadow. *92, it's not a good idea to have car parking in or next to back gardens because of toxic pollution in private seating areas.
Trevor King				We can't control what happens on privately owned land but we want NO ROAD THROUGH MANDALE MEADOW/WOODS.
Val Young				No road through the meadow
	Story Homes	Chris Smith	Lichfields	Urban Strategy Studies 3.18 Pages 76-88 include images and sections for the various road types and landscaped areas within the development. Whilst the design principles are generally logical, the measurements and widths should be removed from the sections so that they remain sufficiently indicative for this stage of the design. 3.19 As set out above, it is clear that the roads will be designed in conjunction with Highway Designers & Transport Engineers so there is no need to specify the road widths. There is also no justification to apply a measurement to the size of the dwellings and gardens. There needs to be an acceptance that some details can only be established at the detailed design stage through the preparation of a planning application.

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Gill Sullivan				<p>Our Gov for may years have priorities traffic over clean safe air, the more road built, the more traffic and keep on building. Those stuck in traffic want more roads, but evidence shows any benefits are short lived. More and bidder roads might easy bottle necks in the short term, but in reality traffic grown when road capacity is increased. Acklam Stockton and Thirnaby do not need this traffic our congestion is bad enough. 1500 vehicles to go through mandale it will be a choke point alright in more was than one. Choking residents lungs and roads. from fumes tyres and brakes and years of housebuilding and all the pollutants. if you check the gazette it showed a piece about a problem with pollution on two parts of the a66 as MBC had not solved the problem, Defra had to step in in 2017 as a nigro dioxide put the a66 as having the highest reading in the country. MBC website at he time showed we had no problems with pollutions, clearly incorrect. Middlesbrough main polluter is traffic now, as the majority of industries have been sorted out. again MBC are committed to reducing pollution they have initiatives and are committed to reduction in traffic and to the reduction in traffic flow, show your commitment to the residents of acklam and wider Middlesbrough by been brave enough to take a second look at this massive traffic problem. Reduce our traffic and the traffic flow in thornaby and stockton.</p>

Question 9 – Please provide any comments you may have on Chapter 8.0 Masterplan Summary

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Name	Organisation (where applicable)	Agent	Organisation	Please provide any comments you may have on Chapter 8.0 Masterplan Summary
Aji Varghese		Aji Varghese		Good
Allison Gibbons				<p>An alternative road must be considered as the destruction of Mandale Meadow is not only criminal but will break environmental laws. Our green spaces are so limited in Middlesbrough. Our children have a right to protect these areas for their future. Wildlife conservation is surely to be of paramount importance</p> <p>Mandale road is a traffic jam every day and will be intolerable for existing residents. Middlesbrough Council please support your community on this.</p>
Amanda Stevenson				If this development must go ahead, please try to ensure there is minimal impact on the Medow. It doesn't need paths and roads, the Council should be protecting the diminishing number of green spaces left in the borough, not developing them.
Andrea Spooner				<p>The overall masterplan does not fit the area suggested, the local community use the existing area with all of the health benefits that provides. To build yet again on natural habitat eliminating the health benefit it provides is criminal. Health statistics in Middlesbrough are already extremely poor with lower than average life expectancies. To further increase air pollution in a deprived area where residents already suffer health conditions such as asthma , heart disease and strokes in my eyes is not acceptable whatever the cost. Middlesbrough Council should concentrate on regenerating the town centre and local area to make that a more attractive and vibrant area to attract people to the town to both live and socialise. This is what makes a town a city, not a town that you dare not walk in. Visit other cities , house prices closer to the city centre are more expensive rather than being no go areas , boarded up. The council need to have a good hard look at what Middlesbrough needs and more housing on an A road is not the way to go. Regenerate deprived areas, build a city, theatres, a good traffic system not a congested nightmare which is what this will become. Traffic on Mandale road is already unacceptably busy and if there is an accident on the A174 or A66 or A19 (yes those 3 busy roads already all circling Acklam) then most of Middlesbrough grinds to a halt. How do emergency vehicles then navigate that! Disgusted that these are your plans for a green open space and hope you listen to the people of Middlesbrough who pay their council tax and so should be heard.</p>
Andy Brown				No buildings of any kind on Mandale field & no new road!!

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angela barber				<p>AS a resident of this area, I fear my house value will lower so close to this new road.</p> <p>There will be No where to walk my dog</p> <p>There would be Little or no space for children to play safety in area</p> <p>An area to exercise my self and enjoy picnics with the family</p> <p>Local wild life will loose there home and endangered species plants and animals will loose there environment</p> <p>Why should be suffer ill health because of further pollution</p> <p>We pay council tax for green space- will our council tax lower-expect not</p> <p>Increased noise during the day and night- lack of sleep and depression result</p> <p>All you want to do is provide new access road to the estate- why cannot they come around down downside and Acklam road instead as always has happened. I feel the new road will increase traffic into Middlesbrough through our area.</p> <p>I do not want or need the road as I do not drive- I doubt it will be included on the bus route, so it does me no good.</p> <p>In fact everything about it is BAD</p>
Angela O'Byrne				Hands off our meadow!
Barbara Keville				It all looks good on paper but some of us have to live with it. I cannot see how all theses houses, roads ,school, hub, car parks paths etc can be built in such a small area.....another Ingleby Barwick.
Barry pinkney				No to country park
Ben Stephenson	Persimmon Homes Teesside			<p>Summary</p> <p>As set out above there are a number of areas of the Masterplan Document, particularly in respect of parking, which we feel should be clarified in order to ensure a justified, implementable guide is created to inform the future development of the site.</p> <p>The overall impact this Masterplan is to raise the design bar for applications on the site and whilst viability testing has already been undertaken for the allocation as part of the 2014 Local Plan adoption, the additional financial burdens identified in the draft Masterplan have not been tested cumulatively with all other requirements and the impact upon delivery is not known. It is for this very reason that the PPG makes it clear SPDs should not be used to add unnecessarily to the financial burdens on development.</p> <p>We would therefore recommend that viability testing is undertaken and published before the document is formally adopted.</p>
Bethany Thatcher				I think the existing access via Jack Simon Way needs to be looked at as a priority. As mentioned in the previous section, the entrance to the road is not wide enough to accommodate large amounts of traffic (particularly as the link road is not expected to be completed until 2027). The development is already very large and has just this one entrance via Jack Simon Way. The roundabout at the entrance to the estate needs to be re-shaped (or the lanes revised) to allow safer access.
Brad Raistrick				Hideous. Please spend the capital on something worth doing.
Brian Casson				<p>It is a sham to call this housing development a 'country park'. The country park already exists and will be cut in half by the spine road.</p> <p>I object to the building on green spaces in a town where we already have one of the smallest percentages of green spaces in the country, and I also object to the subterfuge and deception in attempting to describe a new road and housing development as a country park.</p>
Carol Newmarch				I object to the whole plan

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Catherine Howell (Boyle)				8.3.2 SWWE This road is required to relieve traffic caused by the development. If the development was not to take place then the road would not be required. Is a more sustainable option, given the changing parameters of the climate crisis, to provide the SWWE as only open to public transport, cyclists and pedestrians? What is the anticipated change in road usage by 2027 and beyond, and how has this been factored in?
Charles Rooney				The Masterplan is almost exactly the same as previous plan already consulted upon
Chris King				<p>'Strategic Highways/Transport The Local Plan identified the need for strategic highways improvements to enable the delivery of the housing allocations contained within it. This included the Stainton Way Westward Extension (SWWE), and the Longlands to Ladgate Link Road (LLLR). The realisation of these two schemes will provide the necessary mitigation to enable the housing to be delivered in full. All residential developments are expected to make a contribution towards the delivery of these schemes. The provision of the SWWE is also key to opening up the Stainsby site for development as it also acts as the main distributor road serving the housing.'</p> <p>It would be interesting to know what the estimated traffic flow would be in respect of entrance and exit to the project from the two main access points when completed and fully operational, clearly from my perspective from the Mandale side.</p>

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Christine King				<p>The natural greenspace of Mandale meadows and woods should be preserved. The natural habitats it provides and the flora and fauna that exist there cannot be replaced by the proposed so-called 'country park'. Having looked in detail at the plans for the country park, it will in no way be a 'country' park : its design shows a town park, with numerous paths and cycle paths cutting through small patches of small, mown green patches of grass. Any manufactured open spaces will not sustain the wildlife that exists now in natural meadow land. Newly planted small trees cannot compensate for the loss of semi-mature trees and the habitats they provide. In the current climate and the knowledge we now have about creating an imbalance in our ecosystems, it is unacceptable and unforgivable to destroy these natural habitats only to make way for housing and roads, when there are alternative solutions.</p> <p>The increased traffic caused by the proposed road will bring further air pollution and noise pollution. The council should be aiming to improve air quality and the quality of lives of the residents by reducing air and noise pollution. Air pollution can cause people to suffer from a range of respiratory problems and can exacerbate the conditions of people who already suffer from such conditions. The council have a responsibility for the welfare of its residents and the future residents of the town – our children. The death of the 9 year girl in London, whose death was found to be caused by air pollution, should move all councils into finding alternative solutions to new roads that would certainly create more air pollution.</p> <p>The speed limit on the proposed road will be exceeded by drivers (as all other speed limits on roads are) and the speed that traffic will travel at will not create a safe place for children to play and people to exercise their dogs.</p> <p>The planned 'quiet areas' will never provide the peace and tranquility and consequently the benefits to mental health that a natural, open greenspace like the meadows can. The current pandemic should have taught us all that these spaces are more important than ever for our health and well-being.</p> <p>The area of Mandale meadows is publicly owned; it belongs to the people of Middlesbrough. A deed of covenant, given in perpetuity, should mean that we will always be able to enjoy its beauty and the creatures that depend on it for their habitat should always be able to do so. The children of Middlesbrough should be able to continue to enjoy a natural greenspace with their children and thier grandchildren etc; we already have manmade parks like Albert Park and other similar parks in the town with mown greens and paths. Natural greenspace shouldn't be something we have to travel miles to experience and enjoy!</p> <p>We can't control what happens on privately owned land but we want NO ROAD THROUGH MANDALE MEADOW/WOODS.</p>
Christopher Bieda				I am happy with the Master plan except for the highlighted points on the road network. If these are addressed I would be happy to approve the plan and give my full support.
christopher Eddon				<p>i feel that all you are doing here is building a road to ease congestion then building the houses to pay for this road and then adding to the congestion by building these houses so its a never ending circle with the only added bonus of you creating more council tax on the back of more headaches to follow.</p> <p>so why dont you just scrap the whole idea if you cant afford it or think of better ideas to improve the traffic situation in the area maybe like a tram system.</p> <p>the river tees needs a new crossing but its already a bit stuck for space to use so why dont you build a tunnel under it instead, where it will come and go ill leave that to you because thats what you get paid for.</p>

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Claire Boyd				<p>Mandale meadow/woods is a very precious green space that should be preserved. The natural species that exist there cannot be replaced by the so called 'country park'; and change to the meadow will result in a change in habitat that cannot support the species present currently. There are alternative solutions that could be proposed in order to keep the meadow road free.</p> <p>Increased traffic will lead to way more traffic, noise and light pollution and lead to a potential increase in respiratory for those local to the meadow who bought traditional housing near to the meadow under the promise of a covenant to protect it. The local council has a responsibility to protect it's residents. after all, they were elected in by them.</p> <p>Any increase in traffic may lead to more deaths on the roads of wildlife and deem it unsafe for children to play or dogwalkers to frequent it; something that is enjoyed there currently.</p> <p>I played a huge part personally in the current pandemic as a frontline NHS worker and found peace and solace at the meadow, so vital for my mental health. I know others have very similar feelings. Natural green spaces are vital for future generations of Middlesbrough residents, not contrived, heavily managed areas such as a current parks.</p> <p>Please reverse this decision and keep Mandale Meadow (or 'Ottos field' as our family affectionately call it) and Woods untouched.</p>
Claire Di Cicco				You need to go back to the drawing board as this is not a MASTER plan. It's a plan that is going to destroy a valuable, much used greenspace!
CLAIRE KANE				The pictures are misleading it will be a black tarmac road for people to speed up and down destroying the habitats and wildlife - we already have foxes owls and bats around the area that are walking and flying around our 40 + year old estate because they have had their habitats destroyed by the new houses on Jack Simon Way estate
Claire Wilks				<p>Part of the Governments 25 Year Environment Plan, targets highlighted are: clean air, thriving plants and wildlife and to reduced flooding. Their policies focus on recovering nature and enhancing the beauty, heritage and engagement of the natural environment. This proposal to build a road through the natural and unspoilt landscapes, completely goes against what the governments plans are. Once habitats are lost, planting trees takes many, many years to develop and build up. The air pollution will increase from traffic which also will not meet government targets for cleaner air.</p> <p>The housing is not happening and the road is not needed to develop these plans any more. The Local Plan identified the need for 'strategic highways improvements' to enable housing to be developed. The houses are not being developed so the road is not needed. The idea that the road will support traffic build up on the A19 leading onto the A174, is delusional. There will be more bottle necks created along the proposed road as well as all of the already mentioned hazards such as air pollution, noise pollution and accident problems.</p> <p>As mentioned by the current prime minister at the time in their Environment Plan, 'Our natural environment is our most precious inheritance'. For the current and future residents of Middlesbrough, this area needs to be kept unspoilt, untouched and undeveloped a a to be enjoyed and explored freely and naturally.</p>

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Colin and Morag Bailey				<p>The masterplan fails to address all the concerns lodged with the council over a road through Mandale meadow. As the masterplan is at a very early stage of planning and can be discarded it is the option the council should go for, the road should not go through Mandale and the council has the evidence to justify a change in direction so should have no fear of a legal challenge. The council has signed up to the government Climate Emergency plans, One Planet Living and a Zero Carbon policy. Given that ONS data suggests a decline in Middlesbrough's population over the next decade, the number of houses planned for the new estate is excessive and all new housing needs can, and should be, built on the considerable amount of brownfield sites Middlesbrough has.</p> <p>p10. A loop road through the new estate will be perfectly adequate, an exit point on the A1130 is not needed. The quantity of housing needs to be reduced as the LP provides 1,342 dwellings over expected needs and most of these can be delivered on brown field sites.</p> <p>p10. Flood risk will be increased by too much building on farm land which currently acts as a soakaway for rainfall.</p> <p>p11, site access - A road is not needed at Mandale to exit onto the A1130, there should be a loop road only as was promised to the first purchasers of homes at Brookfield.</p> <p>p12. PROW and informal paths. The informal paths around Mandale meadow are greatly prized by locals because of their wildness and closeness to nature giving the opportunity to see flora and fauna up close. One such prized, ungravelled path, so enchanting, is known as 'the fairy path'. It cannot be seen on aerial maps.</p> <p>p13, trees and hedges. While it is laudable and desirable to plant more hedges and trees it is ridiculous to axe the existing woodland on Mandale meadow as doing so releases all their stored carbon, as is the case if the meadow land is churned up during construction. Furthermore, the trees on Mandale have taken 30 to 40 years to reach their current height and are now capable of absorbing gallons of water each per day as well as producing oxygen. Again, a chainsaw massacre of the Mandale woodland goes entirely against the Council's declaration to become carbon neutral and it isn't possible to replace semi mature trees with 30cm whips as they won't be capable of storing carbon, emit oxygen, act as an easement for traffic air and noise pollution or soak up gallons of water.</p> <p>p14. The existing woodland at Farley Copse (the semi mature woodland which runs across the back of the gardens on Farley Road) does not provide easement of traffic noise pollution and adding another road so close to residents back gardens will increase both noise and air pollution. If the Mandale woodland is axed down that would also create an increase in traffic air and noise pollution making residents lives intolerable.</p> <p>p14 sewerage system. has it been calculated whether the system could cope with 1670+ more houses?</p> <p>p15, summary. Mandale meadow should be designated a Conservation Area or even an SSSI because of the endangered wildlife which can be found there. Doing a full and proper wildlife survey after the plans are passed is too late.</p>
Dennis Mccabe				Looking at the masterplan, this has set the bench mark very high for any future housing developments in Middlesbrough
Edna Reddy				Page 6 of Summary, paragraph 38 says the scheme will "enhance and extend" existing habitats. If Mandale meadow is bisected by a main road carrying 3000+ vehicles this will not extend habitats but vastly reduce them as wildlife will be killed on the road and the noise and movement will deter deer, foxes etc. Children and dogs can no longer run free. The air and noise pollution will considerably degrade the habitat for wildlife and humans. How is this enhancement?
Emily quinnt				No road necessary. Do you want people and animals to get run over?
Francine Kenny				Yes to Country Park Yes to houses yes to road
Gill Mollard				This housing allocation is not necessary and should stop. Any 'country park elements' should remain within the site already under construction and NOT damage the area on Mandale Meadow, Newfield Meadow or the connecting green spaces by the addition of landscaping features, footpaths, cycle paths, play equipment or 'desire lines'.

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Helen McGregor				The area at Mandale meadows and woods would be better used as a totally natural area rather than having a road built through it. That is why I feel strongly that a loop road round the edge of the new estate should be built.
Hilary Morley		Hilary Morley	Resident	Residents don't want this... It will destroy a natural greenspace that is sorely needed and very much used
Ian Etherington				<p>I am happy overall with the masterplan as long as it is delivered as described. I would like to see an alternative route for the relief road however as I feel this would have a major effect on wildlife in the area.</p> <p>I would like to see motor traffic reduced as much as possible and active travel like cycling & walking encouraged, with better access provide both to the existing estate and to Thornaby.</p>
Ian Marron				In section 8.3.2 remove reference to Stainton Way western extension (SWWE) and describe alternative proposal of 2nd link onto B1363 adjacent Stainsby hall farm.
Ian Rogers				<p>I find the lack of detail regarding the existing habitats and the protected species that exist within them quite shocking. Several endangered species are likely to be killed off in the name of progress!</p> <p>There are already multiple housing projects taking place across the borough, more than enough to meet projected demands, it is time to reassess the 2014 Local Plan, during the pandemic more people are likely to work from home, reducing the need for yet more roads but also increasing the need for open space. We live in Acklam because we enjoy the natural open space and the existing wildlife, not the manufactured sterile vision with a major road running through it.</p> <p>I am appalled that the council removed a protective covenant from Mandale Meadow to allow the proposed spine road to be considered. The increase in traffic to Mandale interchange will increase congestion throughout Acklam and the noise and pollution levels around here will increase significantly.</p> <p>During lockdown thousand of people have re-engaged with nature by exercising in this natural space, great for both their physical and mental health, but this is to be taken away from us. Listen to your residents, build on the brownfield sites, like Gresham, Grove Hill and Middlehaven. All have been flattened ready for development but left because builders prefer greenfield sites!</p> <p>The Riverside Stadium opened on Middlehaven in 1995, other than Middlesbrough College and the odd office block, little else has been built there in the past 25 years. Meanwhile every bit of green space in the south of the town is disappearing under concrete. Please stop this destruction of our greenspaces and rebuild our town centre.</p>
Jacqueline Young				Whilst we cannot stop developers building houses on their own land we should not be giving away greenspaces for a road for access
jamie barber				frankly, none of this should go ahead. you're planning to destroy natural green space and animal habitats that are at a premium in Middlesbrough.

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janice slater				<p>8.3.2 Strategic highways/transport the local plan identified the need for strategic highways improvements to enable the delivery of housing allocations contained within it. This included the Stainsby Way Western Extension and the Ladgate Lane to Longlands Road. The realization of these two schemes will provide the necessary mitigation to enable the housing development to be delivered in full.</p> <p>As Paul Clarke planning officer has recently explained in the Nunthorpe Infrastructure Document on 19th May 2020 that the LLLR is no longer anticipated to deliver the improvements and alternative solutions are being consideredas it does not appear to be the necessary mitigation to the local housing development could you please reassess the need for the SWWE?... once a road is built it is there eternally ...but the necessity may not be an actuality. These two roads have been deemed to be the critical answers to the problems of the towns infrastructure... and yet suddenly one main aspect of such is deemed unsuitable.Have any Legal restraints been in place (as we are told is the situation with the SWWE) ? If so what and how are these decided and how and why have they been displaced? What has changed the requirements for the LLLR ? Why is it still stated in the plan as a necessity? are there any other potential out takes of this plan that are not deemed necessary or have not been explained to fully to either the Executive nor to us the public? I formally request answers to these pertinent questions ASAP, and definitely prior to any formal decisions being made on this plan.</p>
Jean Franklin				The only option for the new road is option 1 an internal loop road.
Jennifer A Alderson				Are MBC able to consult residents whose property back onto Mandale Meadow about the country park and its boundary regarding planting close to the property. I am hesitant around the time frame of the road and country park build. How long is it expected to take? Is this plan set in stone so that subsequent councils are able to either stop the plan for the park or change the plans in the future?
Jenny McCullagh				I think the whole thing is incredibly misleading. I'm not sure which members of the community this was developed with but pretty much everyone seems to be of the same opinion as me. Nobody wants this road here. Please take it elsewhere.
Jill Hadfield				No road through Mandale Meadows

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Joanne Mitchell				<p>The A19 has already been widened at some cost in terms of physical and mental health and the general well being of residents. Issues exist which require resolution regarding noise and air pollution and ought not to be made worse still by this imposition.</p> <p>The road would destroy the meadow.</p> <p>These sad plans they have to ruin one of the most beautiful parts of Middlesbrough not to mention the habitat of wildlife that will disappear without the meadow. Greenspaces are very precious and need to be kept as they are. Years of conservation and preservation work has been invested. People moved close to the meadow being given to understand it is protected, a greenbelt where no building or any development may ever take place. It may never have been a designated greenbelt but it was the subject of a Deed of Covenant, the land having been gifted to the people of Middlesbrough in perpetuity. A road is being planned through the meadow to service a housing estate.</p> <p>At the end of the day destroying a pristine meadow which is rich in biodiversity and has never been 'treated' with pesticides and herbicides, is nothing short of ecocide and vandalism and makes a laughing stock of the council's One Planet Living and green strategies.</p> <p>There was always another option, the 'developers' could create an estate loop road similar to the one at Trimdon.</p> <p>The council has stated they won't even pay local residents a penny in compensation for taking our public amenity from us and subjecting us to increased air and noise pollution, damaging our health.</p> <p>the disruption, destruction, the irreparable damage, and the blatant disregard for people who have lived here and who have been involved during many years in the care and preservation, and who will have their lives destroyed, just beggars belief. Residents already suffer the constant excessive traffic noise...and the pollution inflicted upon them. To think that there are those with the power who lack the empathy and the understanding, or thought and consideration for people who use the meadow and those who live close by is unbelievable.</p> <p>The residents in the Ayresome ward would no longer be able to access Mandale meadow from Mandale Road. It will be impossible for people in the Ayresome ward, including those of us who live a 2 minute walk from the current entrance to Mandale meadow because it would involve walking straight across two lanes of the proposed roundabout. The road on OUR meadow stopping us accessing OUR meadow is the issue. All this nonsense about "you are getting extra greenspace" is an insult when you are planning something which prevents us from accessing OUR green space.</p> <p>The remarkable thing about this is some of the language used in the document to describe the proposed plan: "To create a diverse ecosystem" etc when one already exists that will be destroyed by the building of so many houses. The plan to create this "diverse ecosystem" on land which has been polluted by herbicides, pesticides and pollution from the building process while destroying a pristine meadow which is genuinely a diverse ecosystem containing Red and Orange endangered species marked on this map of Mandale meadow. They know this because the map and the legend were included in the Greenstuff response to the 2018 LP.</p> <p>The area is home to rare species flora and fauna, which have taken decades to fully establish, all of this would be destroyed.</p> <p>The faux manufactured country park, cannot, and will never, as if by magic, replace this.</p> <p>Mandale Meadow is well used and loved by many, an accessible place in which we are able to walk and relax.</p> <p>Residents currently suffer excessive noise and air pollution levels 24/7 owing to the proximity of the A19...how can it possibly be considered in any way acceptable to bring yet another road even closer to homes.</p> <p>When the A19 was widened residents received compensation for the loss in value of their homes..but despite the proposed road being routed even closer we are told we would not receive any such award for all of our losses. ..the loss of peace, and of mental and physical health. ..</p> <p>These of course can never be compensated for. It is not what we want.</p> <p>The notice delivered to each resident shows a basic outline of the area concerned, part of which is Mandale Meadow at the top of the 'plan'. The notice gives the impression that the entire area will be Country Park, along with which would come 1700 houses, not forgetting the so called Relief Road, to 'relieve' congestion on the A19 and serve the 1700 new homes. The plan actually shows the road through Mandale Meadow which would discharge traffic onto Mandale Road, at a junction adjacent to residents homes where the current car park is now situated, actually at the foot of their gardens.</p>

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John and Janet Roebuck				<p>Masterplan summary</p> <p>a) Stainsby s106 and infrastructure requirements</p> <ul style="list-style-type: none"> • When will these costs and contributions be ascertained? • What viability modelling has been undertaken to demonstrate that the masterplan is a viable proposition <p>b) Education</p> <ul style="list-style-type: none"> • What modelling has been undertaken on traffic flows to the school from the north and south access points? • What scenario planning / modelling has been undertaken in relation to a requirement for a larger school? • What additional number of pupils may be needed to be catered for? • What is the impact on traffic flows at the peak times in morning and afternoon? <p>c) Strategic Highways and transport</p> <ul style="list-style-type: none"> • Can the Council confirm its position on the Longlands to Ladgate Link road? • What assumptions have been made in relation to the cost of the link road? • Is the FORE Initial appraisal document correct in estimating the capital cost of the SWWE at £50-£100 million • How is the Stainsby scheme viable at this level of cost for the SWWE. • How much are developers expected to contribute? • What funding has been committed by TVCA and Middlesbrough Council to fund the SWWE?
John Baines				I fully support the idea of a country park but without a the new vehicle road which will destroy much needed green space and increase traffic congestion on Mandale road.
John campbell				This proposal if approved will only provide another rat run from Martin to the A19 which will not only harm and remove something that we all love and visit but will be detrimental to those who already live their and future residents and the broader community.
John David Duffy				My summary is that the council are looking to provide a document to hold developers to account and to hopefully provide a development that the town will be proud of. Yes we all would love the road to miss mandale meadow, but unfortunately this cant happen. People need to look at the bigger picture. Loss of small element of green space to provide an additional 40+ ha.
Jonathan Hills				<p>How will the creation of the park affect the residents currently living in the boundary?</p> <p>Will they still be dependent on private landscaping companies or green area maintenance?</p>
Judith Harbron				No development please think of the people who live in the area and use the space, not much grass land left we need to protect. You need to get out and see how well used the land is, it is been a lifeline to a lot of people especially during the pandemic. The plant life and wildlife need to be considered we need this space for our well-being.
Judith Maunder	Friends of Mandale, Greenstuff, resident very local to Mandale Meadow			No Road through Mandale. p91. the green corridors are too narrow

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Julia Povey				<p>The natural greenspace of Mandale meadows and woods should be preserved. The natural habitats it provides and the flora and fauna that exist there cannot be replaced by the proposed so-called 'country park'. Having looked in detail at the plans for the country park, it will in no way be a 'country' park : its design shows a town park, with numerous paths and cycle paths cutting through small patches of small, mown green patches of grass. Any manufactured open spaces will not sustain the wildlife that exists now in natural meadow land. Newly planted small trees cannot compensate for the loss of semi-mature trees and the habitats they provide. In the current climate and the knowledge we now have about creating an imbalance in our ecosystems, it is unacceptable and unforgivable to destroy these natural habitats only to make way for housing and roads, when there are alternative solutions.</p> <p>The increased traffic caused by the proposed road will bring further air pollution and noise pollution. The council should be aiming to improve air quality and the quality of lives of the residents by reducing air and noise pollution. Air pollution can cause people to suffer from a range of respiratory problems and can exacerbate the conditions of people who already suffer from such conditions. The council have a responsibility for the welfare of its residents and the future residents of the town – our children. The death of the 9 year girl in London, whose death was found to be caused by air pollution, should move all councils into finding alternative solutions to new roads that would certainly create more air pollution.</p> <p>The speed limit on the proposed road will be exceeded by drivers (as all other speed limits on roads are) and the speed that traffic will travel at will not create a safe place for children to play and people to exercise their dogs.</p> <p>The planned 'quiet areas' will never provide the peace and tranquility and consequently the benefits to mental health that a natural, open greenspace like the meadows can. The current pandemic should have taught us all that these spaces are more important than ever for our health and well-being.</p> <p>The area of Mandale meadows is publicly owned; it belongs to the people of Middlesbrough. A deed of covenant, given in perpetuity, should mean that we will always be able to enjoy its beauty and the creatures that depend on it for their habitat should always be able to do so. The children of Middlesbrough should be able to continue to enjoy a natural greenspace with their children and thier grandchildren etc; we already have manmade parks like Albert Park and other similar parks in the town with mown greens and paths. Natural greenspace shouldn't be something we have to travel miles to experience and enjoy!</p> <p>We can't control what happens on privately owned land but we want NO ROAD THROUGH MANDALE MEADOW/WOODS.</p>
Julie Riley				NO ROAD THROUGH MANDALE MEADOW
Kabaser Ajaib	Retailing	Kabaser Ajaib	Retailing	Leave it naturally
Keith Russell				No road through Mandale Meadow
Keith Young				<p>Destroying natural habitats and trees and dispersing the wildlife is unforgiveable. It is impossible to grow a man made country park, the council should be expanding the natural greenspace we have at Mandale. I am also against more traffic which brings noise and pollution, its inexcusable to expect residents to suffer this pollution and inconvenience for the Jewel in the Crown country park estate we need to be looking after the welfare and well-being of all residents. Look at the case of Ella Kissi Debrah's death, 9 yrs old killed by the pollution from the roads. No road through Mandale, you can do what you like on the private land.</p>
Lauren Teate				<p>It does not seem very positive that only the road has a definite timing put on it. The Stainton Way Extension will simply cause more traffic to build up through Acklam and cause more problems on Mandale Roundabout. It will become a rat run for people trying to avoid the A174 and A19. During this pandemic, more people are working from home and less people are using the roads. We do not need this spine road at all.</p>

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Leah Davey				<p>This housing allocation is not necessary and should stop.</p> <p>Any 'country park elements' should remain within the site already under construction and NOT damage the area on Mandale Meadow, Newfield Meadow or the connecting green spaces by the addition of landscaping features, footpaths, cycle paths, play equipment or 'desire lines'.</p> <p>This site allocation will cause SIGNIFICANT detrimental impacts to the existing wildlife site at this location. It will destroy the character and openness of the agricultural landscape and loss of visual amenity. Mandale meadow is frequently used and much loved by resident, who were NOT properly consulted prior to the 2014 local plan publication.</p> <p>This housing development is wholly unnecessary given the large supply of deliverable sites elsewhere on brownfield. This remains one of the last irreplaceable areas of major open space. It is also adjacent to an existing green wedge. Development adjoining green wedges can have a significant detrimental impact.</p> <p>Serious thought needs to be given to the validity of this entire scheme. Times have changed and residents are not happy. The new local plan should be designed given up-to-date circumstances, evidences and policies, including the revised National Planning Policy Framework and recent five year housing land supply assessment, in the light of a post Covid Middlesbrough.</p>
Leigh Martin Sayers				Public transport?
Liam Harris				Please see sheet 1
Linda Barker				<p>Please take into full consideration the representation you have received from CPRE and the open letter from Greenstuff Middlesbrough. The current road at Jack Simon should loop through the current housing. Further development should be halted at this site and other greenfield sites in Coulby Newham and Nunthorpe. This would then not impact on infrastructure and Highways England would not disapprove.</p> <p>This housing allocation is not necessary and should stop.</p> <p>Any 'country park elements' should remain within the site already under construction and NOT damage the area on Mandale Meadow, Newfield Meadow or the connecting green spaces by the addition of landscaping features, footpaths, cycle paths, play equipment or 'desire lines'.</p> <p>This site allocation will cause SIGNIFICANT detrimental impacts to the existing wildlife site at this location. It will destroy the character and openness of the agricultural landscape and loss of visual amenity. Mandale meadow is frequently used and much loved by resident, who were NOT properly consulted prior to the 2014 local plan publication.</p> <p>This housing development is wholly unnecessary given the large supply of deliverable sites elsewhere on brownfield. This remains one of the last irreplaceable areas of major open space. It is also adjacent to an existing green wedge. Development adjoining green wedges can have a significant detrimental impact.</p> <p>Serious thought needs to be given to the validity of this entire scheme. Times have changed and residents are not happy. The new local plan should be designed given up-to-date circumstances, evidences and policies, including the revised National Planning Policy Framework and recent five year housing land supply assessment, in the light of a post Covid Middlesbrough.</p>

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Linda Jowers				No road through mandale medow, no country park
Lynn Blagg				Leave site as it is to enable residents and others to use it in its natural state for exercise and pleasure and avoid destruction of wildlife habitats and ageing flora and fauna and avoid excess traffic in the area.
M. E. Bailey				The masterplan fails to address all the concerns lodged with the council over a road through Mandale meadow. As the masterplan is at a very early stage of planning and can be discarded it is the option the council should go for, the road should not go through Mandale and the council has the evidence to justify a change in direction so should have no fear of a legal challenge. The council has signed up to the government Climate Emergency plans, One Planet Living and a Zero Carbon policy. Given that ONS data suggests a decline in Middlesbrough's population over the next decade, the number of houses planned for the new estate is excessive and all new housing needs can, and should be, built on the considerable amount of brownfield sites Middlesbrough has.

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				<p>p10. A loop road through the new estate will be perfectly adequate, an exit point on the A1130 is not needed. The quantity of housing needs to be reduced as the LP provides 1,342 dwellings over expected needs and most of these can be delivered on brown field sites.</p> <p>p10. Flood risk will be increased by too much building on farm land which currently acts as a soakaway for rainfall.</p> <p>p11, site access - A road is not needed at Mandale to exit onto the A1130, there should be a loop road only as was promised to the first purchasers of homes at Brookfield.</p> <p>p12. PROW and informal paths. The informal paths around Mandale meadow are greatly prized by locals because of their wildness and closeness to nature giving the opportunity to see flora and fauna up close. One such prized, ungravelled path, so enchanting, is known as 'the fairy path'. It cannot be seen on aerial maps.</p> <p>p13, trees and hedges. While it is laudable and desirable to plant more hedges and trees it is ridiculous to axe the existing woodland on Mandale meadow as doing so releases all their stored carbon, as is the case if the meadow land is churned up during construction. Furthermore, the trees on Mandale have taken 30 to 40 years to reach their current height and are now capable of absorbing gallons of water each per day as well as producing oxygen. Again, a chainsaw massacre of the Mandale woodland goes entirely against the Council's declaration to become carbon neutral and it isn't possible to replace semi mature trees with 30cm whips as they won't be capable of storing carbon, emit oxygen, act as an easement for traffic air and noise pollution or soak up gallons of water.</p> <p>p14. The existing woodland at Farley Copse (the semi mature woodland which runs across the back of the gardens on Farley Road) does not provide easement of traffic noise pollution and adding another road so close to residents back gardens will increase both noise and air pollution. If the Mandale woodland is axed down that would also create an increase in traffic air and noise pollution making residents lives intolerable.</p> <p>p14 sewerage system. has it been calculated whether the system could cope with 1670+ more houses?</p> <p>p15, summary. Mandale meadow should be designated a Conservation Area or even an SSSI because of the endangered wildlife which can be found there. Doing a full and proper wildlife survey after the plans are passed is too late.</p>
Malcolm Chilvers				<p>Like many people I have neither the time nor inclination to be able to read and absorb the information contained in all these chapters unlike yourselves who are being paid to do it.</p> <p>Basically you are presenting the public with a document filled to the brim with legalese, buzzwords and technical terms which to most is just waffle unless they have training in town & country planning in order to push this development through.</p> <p>Whilst pretending to listen to the people, you already had a secondary plan ready for when you knew there would be an outcry over the first set of plans.</p> <p>I wholeheartedly disagree with these plans and would consider in the current economic climate that it should be scrapped or at the very least put on hold until a) traffic movement is monitored post covid and b) all brownfield sites are being built upon - not planned, built.</p>
Malcolm Watson				please see comments in attached file
Martin Hammond				This is an exercise in greenwash. Teesside has abundant brownfield land which could be redeveloped, in locations closer to the town centre with more potential for cycling and public transport e.g. Grove Hill. This will be another car-centred commuter suburb which will leave a much-needed green corridor fragmented, hemmed in by housing and subject to so much recreational pressure that its environmental value will be greatly diminished.

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Matthew Shippey				Don't want this. I walk in mandale meadow. Middlesbrough is lacking in green space and you want to build a road through to facilitate new housing. Middlesbrough Council you are a disgrace.
Matthew Waterfield				The exit road will destroy the Mandale Meadow. The community that use that area each and every day are having an area ripped from them and replaced with a road through it. The exit road needs to be placed elsewhere, not ripping through established land, next to a housing estate and leading on to one of the busiest roundabouts off the A19. There are various exit points available without effecting the lives of a community who have lived there for years and without destroying a heavily used area.
Michael Miller	Natural England			<p>Natural England offers the following general advice:</p> <p>Protected Species Natural England has produced standing advice to help planning authorities understand the impact of particular developments on protected species. We advise you to refer to this advice. Natural England will only provide bespoke advice on protected species where they form part of a SSSI or in exceptional circumstances.</p> <p>Priority habitats and Species are of particular importance for nature conservation and included in the England Biodiversity List published under section 41 of the Natural Environment and Rural Communities Act 2006. Most priority habitats will be mapped either as Sites of Special Scientific Interest, on the Magic website or as Local Wildlife Sites. List of priority habitats and species can be found here . Natural England does not routinely hold species data, such data should be collected when impacts on priority habitats or species are considered likely. Consideration should also be given to the potential environmental value of brownfield sites, often found in urban areas and former industrial land, further information including links to the open mosaic habitats inventory can be found here.</p> <p>Ancient woodland and veteran trees You should consider any impacts on ancient woodland and veteran trees in line with paragraph 118 of the NPPF. Natural England maintains the Ancient Woodland Inventory which can help identify ancient woodland. Natural England and the Forest Commission have produced standing advice for planning authorities in relation to ancient woodland and veteran trees. It should be taken into account by planning authorities when determining relevant planning applications. Natural England will only provide bespoke advice on ancient woodland/veteran trees where they form part of a SSSI or in exceptional circumstances.</p> <p>Environmental enhancement Development provides opportunities to secure a net gain for nature and local communities, as outlined in paragraphs 9, 109 and 152 of the NPPF. We advise you to follow the mitigation hierarchy as set out in paragraph 118 of the NPPF and firstly consider what existing environmental features on and around the site can be retained or enhanced or what new features could be incorporated into the development proposal. Where onsite measures are not possible, you may wish to consider off site measures, including sites for biodiversity offsetting. Opportunities for enhancement might include:</p> <ul style="list-style-type: none"> • Providing a new footpath through the new development to link into existing rights of way. • Restoring a neglected hedgerow. • Creating a new pond as an attractive feature on the site. • Planting trees characteristic to the local area to make a positive contribution to the local landscape. • Using native plants in landscaping schemes for better nectar and seed sources for bees and birds. • Incorporating swift boxes or bat boxes into the design of new buildings. • Designing lighting to encourage wildlife. • Adding a green roof to new buildings. <p>You could also consider how the proposed development can contribute to the wider environment and help implement elements of any</p>

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				<p>Landscape, Green Infrastructure or Biodiversity Strategy in place in your area. For example:</p> <ul style="list-style-type: none"> • Links to existing greenspace and/or opportunities to enhance and improve access. • Identifying opportunities for new greenspace and managing existing (and new) public spaces to be more wildlife friendly (e.g. by sowing wild flower strips) • Planting additional street trees. • Identifying any improvements to the existing public right of way network or using the opportunity of new development to extend the network to create missing links. • Restoring neglected environmental features (e.g. coppicing a prominent hedge that is in poor condition or clearing away an eyesore). <p>Access and Recreation Natural England encourages any proposal to incorporate measures to help improve people's access to the natural environment. Measures such as reinstating existing footpaths together with the creation of new footpaths and bridleways should be considered. Links to other green networks and, where appropriate, urban fringe areas should also be explored to help promote the creation of wider green infrastructure. Relevant aspects of local authority green infrastructure strategies should be delivered where appropriate.</p> <p>Rights of Way, Access land, Coastal access and National Trails Paragraph 75 of the NPPF highlights the important of public rights of way and access. Development should consider potential impacts on access land, common land, rights of way and coastal access routes in the vicinity of the development. Consideration should also be given to the potential impacts on the any nearby National Trails. The National Trails website www.nationaltrail.co.uk provides information including contact details for the National Trail Officer. Appropriate mitigation measures should be incorporated for any adverse impacts.</p> <p>Biodiversity duty Your authority has a duty to have regard to conserving biodiversity as part of your decision making. Conserving biodiversity can also include restoration or enhancement to a population or habitat.</p>
Michael Starford				<p>8.3.2 Highways; I would observe that as a lateral thinking exercise, if a through road is imperative then our European Neighbours logic would be to construct a split carriageway borrowing junction from the farm originating practically near the start of A19(S) slip roads and use the existing farm crossing for one carriage way, and the other following the A19 line. I cannot comment on the civil engineering of this nor the politics, but if it were acceptable and practical, the impact on existing junctions is lessened.</p> <p>Cycleway access to the Park area from Mandale road should still be incorporated and the line of this can suit the space.</p>
Michelle Hedger				<p>No road through Mandale Meadow. Use the loop road option. If a road needs to be there make it a narrower road of 20mph. My children cannot play near the huge 30mph road planned. They can play now in the meadow. They will be hit by a car in this 'country park'. Please don't put all the footpaths in. We like to follow the natural winding dirt tracks. There is only lip service paid to these. We don't want artificial paths. It's beautiful, muddy and filled with nature.</p>

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Mike Conyard				Do not build on this land. This is much needed green space. There are no parks in this area and nowhere for people to walk, kids to play and people to live their lives. I find it disgusting that you propose to build on this beautiful piece of land. Also, who wants to live right next to the A19? These house will be worth literally nothing.. The land is also a buffer zone from the A19 to the residents of Heythrop Drive and beyond.
Miller Homes and Avant Homes	Miller Homes and Avant Homes	Mr Neil Morton	Savills	<p>8.1- Development Quantum – our clients welcome the masterplan’s recognition that densities will generally be higher in the south of the site, reflecting the style of the recently constructed development.</p> <p>8.2 – Masterplan Summary – the plan on p.96 attempts to present in layers the information provided by the masterplan. The key denotes 58 items which results in a plan which is not legible and incredibly difficult to interpret by users of the document, including the local community and applicants. This perhaps best illustrates our principal concern that the masterplan has provided a level of detail which unnecessarily goes well beyond its remit as design guidance.</p>
Mrs D M Hornby				<p>Environment</p> <p>Masterplan Development summary page 3 “climate change resilience”. Surely mitigation and reversal of climate change should be of far more importance than resilience?</p> <p>Planting trees and bushes, yes , of course. Dense bushes all along the road edge are an absolute 'must'. With 'upper storeys' of trees to grow and mature to lessen the effects of pollution, both air and noise.</p> <p>Trees are not required in the open grass field as this would change the character of that environment. Ground nesting birds, insects breeding and the creatures that eat them need open space.</p> <p>Pockets and stands of trees, scattered along the becks, link areas together to provide corridors and shelter for larger wildlife, and roosts for bats, owls and other species.</p> <p>In the housing developments, every fence must have a 'hedgehog highway', to facilitate easy access for small wildlife to forage, thus lessening the devastating effects of loss of habitat. And promote hedges rather than fences or walls.</p>
Nichola Davey				<p>This housing allocation is not necessary and should stop.</p> <p>Any 'country park elements' should remain within the site already under construction and NOT damage the area on Mandale Meadow, Newfield Meadow or the connecting green spaces by the addition of landscaping features, footpaths, cycle paths, play equipment or 'desire lines'.</p>

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Nicola Smith				<p>We DO NOT WANT IT!!!!</p> <p>WHY can't you build on areas where you have flatted hundreds of houses but not rebuilt? Keith Road or Marton Burn Road??? This is the only green space we have in Acklam and hundereds of residents rely on this being walking distance. Pre covid the traffic on mandale Road, Acklam Rd and levick Cresent is bad enough at peak times without further traffic. You have Acklam Grange Secondary school round the corner from the meadow with 1200+ students that also use these roads. This is already a very busy junction without diverting more traffic to it. What about adults and children's mental health?? Has the past year taught you nothing?? I think having a open greenspace with all the wildlife and plenty of room to exercise and get much needed fresh air and been an absolute saviour to hundereds of local residents. This project has not considered anything but money. Looking at the plans there will be very little green left and it will be surrounding a road and housing estate. Absolutely nothing like we have now. This is NOT ACCEPTABLE FROM ANYONE I KNOW WHO LIVES IN ACKLAM 😞😞😞😞😞😞😞😞😞</p>
Peter Davey				<p>This housing allocation is not necessary and should stop.</p> <p>Any 'country park elements' should remain within the site already under construction and NOT damage the area on Mandale Meadow, Newfield Meadow or the connecting green spaces by the addition of landscaping features, footpaths, cycle paths, play equipment or 'desire lines'.</p>
Poppy Bowyer				<p>Will adjacent roads and potentially the A19 be disrupted or unusable for much of the decade long build? Will this create further traffic management problems? Has this been considered?</p> <p>What are the potential issues with having the school and hub built and in use before other stages of the development?</p> <p>Are the costs for the highway and transport schemes and estate maintenance an additional annual cost for the residents?</p> <p>Have considerations been made for changes to design further into the build? Could this alter the overall integrity, cohesion of the design and development? And would conflicts surrounding potential slipping standards and false advertising cause issues for buyers with this specific vision and expectation?</p>
Rob Atkin				No road through Mandale.
Robert Teate				<p>I strongly disagree with the comments stating that the council have no authority over the building of housing on private land. It is the council who give planning permission with any building on any land.</p> <p>There was no consiltation back in 2014 for the now called Stainsby area. At the time it was called Brookfield leading to confusion due to building in Brookfield at the time.</p>
Sofia Torre				<p>The vision for the allocated housing site at Stainsby is a sustainable, mixed and balanced community set within an outstanding and engaging landscape, that includes the creation of a new country park, providing over 1500 new homes. 1500 homes with 2 cars each that is 3000 cars, then say that its 3 per home that is 4500 cars running around in your estate, are you utterly completely mad, look at Teesside Park, it can't handle another 1500 cars never mind 3000 if they all took there cars out.</p>
Sonia Bowler				No road through Mandale Meadow!

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Stephanie Atkin				<p>Masterplan is a misnomer. Country park is a smokescreen. No road through Mandale Meadow. Protect and preserve. 'If you build it, they will come' is more aptly applied to describe traffic volume. We are creating the problem we are seeking to solve. Brochure images have limited appeal compared to the natural beauty that is already there. It is also blindingly unrealistic. Where are the cars???</p> <p>Loop the road. Build on brownfield. Cut the profits and protect what needs protecting. No road through Mandale Meadow.</p>
Stephen Connors				Thoughts previously mentioned.
Steve Gorman				Overall I love the whole concept, I agree the road entrance is going to cause an issue for many people, but I cannot think of an easier solution to that. Building a link on the A19 is not going to happen, so from Mandale road that looks to be the only viable entrance. Its ok having one at the other end of the site where the new house are, however that will not get the traffic issues there as there is not much beyond that point really only Hemlington and Coulby Newham and Brookfield.
Steven Brown				<p>The principles of the development I fully understand My concerns are the way it has been thrown upon the residents of Stainsby Hall Farm I have continually voiced my opinion by all methods and to say that I have had no response from the council or the builders is an understatement.</p> <p>My question is to all concerned, how would you feel having a major building development built right on your doorstep and no one concerned has had the decency to have a meeting with us regarding our concerns. Yes there was local exhibitions of proposed development but not with us as the only houses that are directly effected by the development.</p>
stewart jones				I think another master plan was a waste of money this scheme has already undergone enhanced consultation
Thomas Maunder				The masterplan fails to address all the concerns lodged with the council over a road through Mandale meadow. As the masterplan is at a very early stage of planning and can be discarded it is the option the council should go for, the road should not go through Mandale and the council has the evidence to justify a change in direction so should have no fear of a legal challenge. The council has signed up to the government Climate Emergency plans, One Planet Living and a Zero Carbon policy. Given that ONS data suggests a decline in Middlesbrough's population over the next decade, the number of houses planned for the new estate is excessive and all new housing needs can, and should be, built on the considerable amount of brownfield sites Middlesbrough has.
Tim Allen				I'm just a little confused Re country park emphasis. Seems mainly houses, roads and some grass and trees around such houses n roads? Will it really just be similar to how Ingleby Berwick is set out?

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Trevor King				<p>The natural greenspace of Mandale meadows and woods should be preserved. The natural habitats it provides and the flora and fauna that exist there cannot be replaced by the proposed so-called 'country park'. Having looked in detail at the plans for the country park, it will in no way be a 'country' park : its design shows a town park, with numerous paths and cycle paths cutting through small patches of small, mown green patches of grass. Any manufactured open spaces will not sustain the wildlife that exists now in natural meadow land. Newly planted small trees cannot compensate for the loss of semi-mature trees and the habitats they provide. In the current climate and the knowledge we now have about creating an imbalance in our ecosystems, it is unacceptable and unforgivable to destroy these natural habitats only to make way for housing and roads, when there are alternative solutions.</p> <p>The increased traffic caused by the proposed road will bring further air pollution and noise pollution. The council should be aiming to improve air quality and the quality of lives of the residents by reducing air and noise pollution. Air pollution can cause people to suffer from a range of respiratory problems and can exacerbate the conditions of people who already suffer from such conditions. The council have a responsibility for the welfare of its residents and the future residents of the town – our children. The death of the 9 year girl in London, whose death was found to be caused by air pollution, should move all councils into finding alternative solutions to new roads that would certainly create more air pollution.</p> <p>The speed limit on the proposed road will be exceeded by drivers (as all other speed limits on roads are) and the speed that traffic will travel at will not create a safe place for children to play and people to exercise their dogs.</p> <p>The planned 'quiet areas' will never provide the peace and tranquility and consequently the benefits to mental health that a natural, open greenspace like the meadows can. The current pandemic should have taught us all that these spaces are more important than ever for our health and well-being.</p> <p>The area of Mandale meadows is publicly owned; it belongs to the people of Middlesbrough. A deed of covenant, given in perpetuity, should mean that we will always be able to enjoy its beauty and the creatures that depend on it for their habitat should always be able to do so. The children of Middlesbrough should be able to continue to enjoy a natural greenspace with their children and thier grandchildren etc; we already have manmade parks like Albert Park and other similar parks in the town with mown greens and paths. Natural greenspace shouldn't be something we have to travel miles to experience and enjoy!</p> <p>We can't control what happens on privately owned land but we want NO ROAD THROUGH MANDALE MEADOW/WOODS.</p>
Val Young				No road through the meadow
	Story Homes	Chris Smith	Lichfields	<p>Masterplan Summary</p> <p>3.21 Page 96 includes a Design and Principles Summary and a plan which perhaps best articulates the concerns and comments on the document. This plan includes 58 layers of information and as a result it is unfortunately ineligible. Story Homes (and the wider Developer Group) are concerned that there could be a requirement to accord with principles on a plan which are not possible to interpret.</p> <p>3.22 In contrast, there is a plan on page 48 which sets out a level of information appropriate for the purpose of this Masterplan. It sets out the key principles for the Landscape Strategy and is much easier to understand. The plan on page 48 is clear about the principles for the Country Park and other shared areas of the site but, importantly (and subject to removing the LAPs as discussed above), it would leave the development cells to allow flexibility for each developer to apply creativity to the design process.</p>

Question 10 – Please provide any additional comments you may have regards the draft development guidance

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Alan Liddle	Stainton and Thornton Parish Council			A full traffic appraisal/assessment needs to be undertaken before any further sites are considered for development over and above those already included in the 2014 adopted Local Plan. The economic viability of building road versus revenue gained from council tax generated from the increase in housing needs to be proven.
Amanda Stevenson				I would like to say that I oppose the development. I also think the consultation should have taken place after the lockdown restrictions have been lifted. a lot of people, especially older people do not have online access. Carrying out the consultation now makes people think that the Council has deliberately tried to reduce the number of objections to the very unpopular scheme. It certainly doesn't build any bridges with the people of Acklam.
Amy Lord				I sincerely hope the road through Mandale Meadow does not happen. Despite comments from the Mayor and Deputy Mayor on social media during the consultation, which stated that the road would be going ahead regardless, it seems naive to ignore the changes in environmental policy since the Local Plan was approved in 2014. This plan also states measures such as a traffic impact study of the spine road's link to Mandale roundabout have not been carried out, therefore, how can the Council know that this road is an acceptable solution? Adding 12k cars a day on this route will cause traffic problems, pollution and result in the loss of biodiversity. Please seek another solution.
Andy Brown				No buildings of any kind on Mandale field & no new road!!
Andy Emerson				A road on Mandale meadow will destroy a wildlife corridor on what should be green belt. There would be easy access for illegal campers against whom the council has no immediate defence as measures to remove them take more than a week to implement as has been proven recently.
angela barber				AS a resident of this area, I fear my house value will lower so close to this new road. There will be No where to walk my dog There would be Little or no space for children to play safety in area An area to exercise my self and enjoy picnics with the family Local wild life will loose there home and endangered species plants and animals will loose there environment Why should be suffer ill health because of further pollution We pay council tax for green space- will our council tax lower-expect not Increased noise during the day and night- lack of sleep and depression result All you want to do is provide new access road to the estate- why cannot they come around down downside and Acklam road instead as always has happened. I feel the new road will increase traffic into Middlesbrough through our area. I do not want or need the road as I do not drive- I doubt it will be included on the bus route, so it does me no good. In fact everything about it is BAD
Angela O'Byrne				Hands off our meadow!
Anthony O'Malley				I am in favour of the overall scheme but would like the council to consider my points raised for amendment.

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Barry Jobson	South Tees Green Party			<p>However, should the scheme go ahead, we have noted a number of positives and negatives in the proposals, and also have additional comments, which we have noted below.</p> <p>Positives</p> <ul style="list-style-type: none"> · Commuter cycle route · Additional ponds and swales incorporated into the site, to compliment those existing, and the proposed SUDS. This will provide extra wetland habitat, and potential for water vole expansion from Bluebell Beck. · Previously proposed housing on eastern side of Bluebell Beck have now been removed · New woodland [including carr], grassland, and wetland habitats – including woodland understory [often overlooked] · New foot/cycle paths · Pedestrian-friendly crossing points · Formal and informal play areas · 20mph speed limits on tertiary roads, and 10mph in ‘home zones’ welcomed. <p>Negatives</p> <ul style="list-style-type: none"> · Loss of open space · Loss of open views from existing housing and green spaces · Loss of trees and grassland habitats created in the 1980s and 1990s that are only now maturing. · Loss of marsh habitat [location of original 1980s Mandale Pond – filled in in 1990s when A19 was widened] · Increased traffic noise for existing houses off Mandale Road. <p>Further concerns</p> <ul style="list-style-type: none"> · Consultation documents show that Middlesbrough Council will be responsible for maintenance of the country park; however, there is no indication as to who will own the land that is currently farmland. Will this be transferred to the council, or will this remain in private hands? <p>Additional Comments</p> <ul style="list-style-type: none"> · Wet areas on Mandale Meadow could be opportunities for additional wetland creation. Historical maps show a pond along the eastern boundary that was lost to development. New ponds could make-up for this loss. · Secondary roads should also be 20mph, with traffic calming planned in, to prevent these become rat runs when traffic builds up at peak times. · New tree planting within the country park should reflect locally native species, as not all British native trees are locally native. · Seeds for grassland, woodland understory, and wetlands should be sourced from local biodiverse sites within the Tees Valley. Scattering green hay, harvested from these local sites, will contribute to their management, and ensure that plants are adapted to local conditions. · Grassland seeds should be sown on sub-soil, as this will ensure species rich grassland is not lost to more vigorous species that enjoy nutrient-rich soil. This will also ensure that grassland is slow growing, and as such will save the council money by requiring mowing less-often than grassland sown on top-soil. · Scrub should include species that were found nearby historically, but are no longer present, such as gorse and broom, which are referenced in field names on tithe maps from the 1800s.
Barry pinkney				Just leave well alone you are trying to rid us of all green belts
Beverley cooper				Plan cannot be accessed on the site
Carol Newmarc h				<p>I have known and used this land since 1960 It is loved and used by the local community and will be ruined by the proposals</p> <p>This is not wise planning in this time of climate crisis.</p> <p>I support all the members of Greenstuff in their arguments to save this land.</p>

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carole elizabeth mitchell				The environmental and ecological impact of the proposed route will have a huge negative impact on the area. We do not need a road or paths to be able to enjoy the green space of Mandale Meadow, and nature will thrive much better without the construction proposed. In addition the additional traffic routed to Mandale roundabout will cause bottlenecks at already very busy junctions.
Carolyn Dodds				All in all the proposed plans look fine, together with a comprehensive breakdown covering the various elements. I trust the developers will adhere to the final outcome of this consultation. I would also like confirmation that all the fields/meadows to the east of Saphwood and Bluebell Beck will remain as at present in their natural state. Apart obviously where the road is proposed on Mandale Meadow. I would like to see some form of screening near the road to make it as intrusive as possible for the residents of Ayresome Ward. I would suggest that an application is made to change the speed limit to 20 mph when the road enters Mandale Meadow.
Catherine Howell (Boyle)				I have spent nearly 15 years working to support the environmental sustainability of Middlesbrough, my home town, and implementing aspects of the One Planet Living framework on behalf of Middlesbrough Council. I have grave concerns that the climate crisis is not adequately accounted for in this proposal and that decisions are being made without anticipation for the challenges that we face. I do not believe that the SWWE is an appropriate solution and instead, as a town, we should be striving for a groundbreaking approach that favours cycling and pedestrians more favourably. I believe that any major road development should loop through the housing and not across an area of green space that is much used, much loved and very valued by the local community. Development of the road will have a severely detrimental effect on local residents - on their physical and mental wellbeing and their enjoyment of their homes and gardens. Additionally, I do not believe that the existing wildlife value of the site has been adequately explored and documented. I would urge MBC to take an innovative approach to this development and set a precedent for local authorities nationally by working in anticipation of climate crisis and acting accordingly.
Charles Rooney				If the road is necessary and already approved why accept comments?
Chris Brown				We reside at 118 Hesleden Av, Acklam and we are in favour of the proposed project to develop the land from Low Lane - Mandale Road. We have looked at the maserplan documents and look forward to the country park been developed .

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Christian wood				<p>Rather than comment on specific parts of the master plan main document, I'd prefer to make broader comments about the whole proposal and my relationship to it. I've lived in the immediate area of the proposal for the last 27 years. 13 of those years were spent on Endsleigh Drive, just north of the SWWE, and currently in Stainton, just south of the SWWE.</p> <p>While I was in opposition of the original plan for Mandale Meadow which included a housing development, I am completely in favour of the new master plan. As both a family man and a small business owner, the benefits of the new country park and relief road are unending.</p> <p>As a small general builder, I spend many hours on the roads local to the SWWE. Regardless of the new houses to be developed immediately surrounding the proposed road, it's been clear for several years that a new route has been needed. While I understand the view of those objecting to the new exit onto Mandale roundabout, my own believes are that this will actually reduce congestion in that area so long as that the roundabout is upgraded and traffic lights are introduced there.</p> <p>As a family man, I have and do spend many hours walking with my children and dogs along the acklam green belt and Mandale Meadow. The meadow in-particular is a stunning area and I'm relieved to see that there are no houses to be built there. That said, the new road will take very little of the field and I believe the benefits far outweigh the costs. In addition, the creation of the new Country Park will permit far improved access to the green belt and see it finally reach its potential as an excellent green space for thousands of families.</p> <p>My only concern for the proposal is one of access. It's clear that the park will become an area of frequent use by local families. As a result, we need to ensure that local families can safely and easily walk to the park. I really hope that enough safe pathways are introduced linking ALL surrounding areas to the new park so that my children and those of other families can walk to it rather than be driven to it. As the situation stands now, that's just not possible from where I live in Stainton. Should the situation not be improved, I'll be driving my children to this fantastic park rather than taking the risk of allowing them to walk there.</p> <p>Thank you</p>
Christine King				<p>Mandale meadow was gifted to the people of Middlesbrough. A deed of covenant, left in perpetuity. It should remain ours as a natural green space forever. Future generations should benefit from this place as a natural green space, not a manufactured one, that can never bring the same benefits as a natural beautiful area. And once it is lost, it is lost forever!</p> <p>We can't control what happens on privately owned land but NO ROAD TSHOULD EVER BE BUILT HROUGH MANDALE MEADOW/WOODS.</p>
Christine Ward				<p>Whilst at first glance, the headline for this development seems positive, it will involve the desecration of an existing green space enjoyed by many for years. I grew up in Acklam and the area under threat is one of the few natural spaces in that area - increasingly already encroached on by housing infill since I moved away. It must be protected.</p>

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Christopher Bieda				<p>In conclusion I would like to recommend the following for the plan.</p> <p>a) the full adoption of the site upto and including the Low Lane Roundabout as per the original 2014 plan.</p> <p>b) The installation of a MAX ROAD SPEED OF 20MPH in keeping with the residential area and in keeping with the majority of Acklam residential roads (Trimdon avenue etc)</p> <p>c) The installation of traffic calming measures between the Low Lane Roundabout and the proposed first roundabout from this point to control vehicle speed in a high residential zone.</p> <p>d) Controlled crossing points (lights or Zebra Crossing) between the above mentioned roundabouts to aid pedestrians.</p> <p>e) Cycle lanes installed between the above mentioned points.</p> <p>Many thanks for your time and understanding. I am available to discuss further if required.</p>
christopher Eddon				<p>you cant have it all your own way and sometimes you have to listen to what people in the boro want, i have to ask where do you live and does this development affect you at all and would you think differently about it if it did.</p> <p>i also think that building a 7 storey office block in the town centre, thats supposedly going to create 1800 jobs is only going to increase more traffic congestion. so another headache to be had in the future if this gets the go ahead.</p> <p>i think you need to realise that this town can only accommodate a certain amount before it becomes full up so you need to address that situation first before adding to its problems.</p>
Claire Boyd				<p>Mandale was a gift to the residents of Middlesbrough who use it daily for their recreational and personal needs. This should bequeathed to our future generations. Once it has gone.....it is gone forever.</p> <p>Please leave it alone.</p> <p>NO ROAD THROUGH MANDALE MEADOW.</p>
Claire Di Cicco				<p>Regenerate the town and Middlehaven first. Regeneration done correctly makes the area desirable to live in.</p>
CLAIRE KANE				<p>We do not want this to happen to our beautiful area I regularly walk on the land with my dogs for my mental health, I live in the area i do as i like the peace and quiet and the lovely natural environment I live in for my own peaceful retreat and mental health I also have children which I want to be safe away from busy roads and then one is going to be built on my door step</p>

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Claire Luke	Subject Leader for English			<p>I would like to express my disappointment and vehement opposition to this proposal.</p> <p>I live in the Kader Area (Kader Farm Road) and moved here chiefly for the safe and green space to bring up my family.I have been here 25 years- pay full council tax and raised a wonderful family here: I had hoped my grandchildren would be able to benefit from the beautiful beck, woodland and meadows in our surrounding vicinity.</p> <p>There are obvious environmental concerns that come with this dreadful proposal - apart from the excess pollution (when gov targets zero emissions) there is an abundance of wildlife in this area, whose habitat will be destroyed.</p> <p>Moreover, as I type, Middlesbrough has one of the worst COVID infection rates in the country. A densely populated town NEEDS green spaces, this area during lockdowns has been an absolute haven for walkers, runners,cyclists and the meadow sees families playing games. In an area that is desperately in need of promoting a healthy , active lifestyle why would you even consider wiping out this wonderful natural space.</p> <p>I urge you to reconsider</p>
craig Llewellyn				<p>Why bother sending out letters for a consultation when according to Antony High and Andy Preston it's a done deal so letter sending just wasted more government money. Lots of wildlife some of which are endangered will be displaced or killed off in the area. Around other developments there's lots of dead hedgehogs on the roads due to displacement. Also all the paths your talking about putting in are unrequired, what about drainage and flooding.</p>
Damian McKinley				<p>Only Option 1 Loop Road</p>
Deborah Evans				<p>I have lived in the Acklam area for 60 years. I played on the green belt between Mandale and Malvern Drive as a child. My children have grown up playing down there. It is a beautiful place, a little bit of heaven in town. The flowers are amazing, the frogs and newts in the pond, the deer and foxes and the birds.</p> <p>We have always felt so lucky to have such an amazing place on our doorstep. Even with the arrival of the A19 and then the widening of the A19, the beauty and the wildlife has remained.</p> <p>Residents litter pick and look after the area regularly, we all feel the need to look after it and preserve it.</p> <p>There are dozens of dog walkers there everyday, kids play there and during lockdown it's been a place to visit for everyone's sanity.</p> <p>Do not destroy this place but be proud of it. Roads and houses are everywhere but a place that's naturally wild and full of wildlife is rare and special.</p> <p>Just leave it as it is with it's natural beauty and woods and stream.</p> <p>Where will we all go? Please be proud of this beautiful area and respect what the residents want and deserve. It's ours, let it stay as we want it.</p>
Dennis Mccabe				<p>Looking at this proposal, if feel it is a fantastic improvement from the two we were shown three years ago.</p> <p>This shows the council has listened to the comments of the people, i am very happy with what i see on the plans and fully endorse this development as long as the road is moved as close as possible to Mandale Rd.</p>
Derek Phillips				<p>The road should not be built through the meadow. This green space should not be compromised by the building of this road it should be preserved for the use and benefit of current and future generations.</p>

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Dylan Currey				I will provide more general comments. The current alignment for the spine road would cause even more congestion in Thornaby which, while not under the purview of Mbro Council, still should be considered.
Edna Reddy				In the design code Gradon says it's "about people". The people who already live here do not want their environment degraded by the spine road ruining Mandale meadow for the people and wildlife already existing in this precious open space. We reject the air and noise pollution, the disturbance of all the animals, birds and plants. The congestion at the Mandale roundabout is already bad, 3000+more vehicles will create jams especially when A19 accidents will redirect heavy traffic across the meadow. How can the car park be "enhanced" as "access for dog walkers" when the area will not be safe to let children and dogs run free? At present this green open space provides vital help for our mental health and is in continual use, this space as it is now is why we came to live here. It has already been reduced once, removing the pond..Leave our home environment alone!
Emily quintt				Don't do it. Can't we have any nature round here anymore?
Francine Kenny				Yes to houses yes to road
Gill Mollard				This site allocation will cause SIGNIFICANT detrimental impacts to the existing wildlife site at this location. It will destroy the character and openness of the agricultural landscape and loss of visual amenity. Mandale meadow is frequently used and much loved by resident, who were NOT properly consulted prior to the 2014 local plan publication. This housing development is wholly unnecessary given the large supply of deliverable sites elsewhere on brownfield. This remains one of the last irreplaceable areas of major open space. It is also adjacent to an existing green wedge. Development adjoining green wedges can have a significant detrimental impact. Serious thought needs to be given to the validity of this entire scheme. Times have changed and residents are not happy. The new local plan should be designed given up-to-date circumstances, evidences and policies, including the revised National Planning Policy Framework and recent five year housing land supply assessment, in the light of a post Covid Middlesbrough.
Graeme Brooks				In summary of my question regarding the additional service charge i would just like the council to be extremely clear when they comment on this new proposal the exact costs that the new home owners will have to pay if indeed the service chargers will be required.

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Graeme Brooks				<p>Having read the supporting information:</p> <ul style="list-style-type: none"> - Stainsby Country Park & Masterplan background information (page 10) - Stainsby Country Park & Masterplan frequently asked questions (How will the green space be managed?) <p>I am still a little unclear as to whether the council will be adopting the green space or not.</p> <p>Background information it states:</p> <p>Should the Stainsby Country Park & Masterplan not be adopted by the Council as Development Guidance the following risks have been identified:</p> <ul style="list-style-type: none"> a) the alignment of the road through the private land could be developed with a route that the Council will have no control over; b) developers could look to maximise the density and volume of housing; c) the Council will have little control over the quality of the housing delivered; d) level of green space provided for the creation of the Country Park may be reduced to accommodate a larger built environment; e) quality of the local centre may be affected and not be provided in the ideal position on the site; f) typically on developments like Stainsby, each developer will appoint its own agent to manage the green space provided, leading to a number of different agents managing the Country Park; and, g) the Masterplan is developed following a full public consultation and takes into account the desires of the community, without it they are not heard by developers and will not be reflected in the development. <p>Am I correct in thinking this means that as of yet no decision has yet been made on who will maintain the green areas yet should the housing development go ahead?</p> <p>Also in FAQ</p> <p>How will the green space be managed? Usually developers will look to appoint private companies to manage and maintain public green space created by new developments. For Stainsby, the Council will seek to take on the ownership and management of the Country Park to ensure it is maintained and protected moving forward.</p> <p>Again this reads to me that as of yet no decision has been made on who will both own and maintain the land?</p> <p>Surely these decisions need to be made before anything is given the go ahead as the potential costs to the house owners could be rather large and one of my main concerns is that the prospective house owners are not made aware of these additional costs that would come their way through addition service charges. The standard fee may well only be £69 a year to start with but cost could very well escalate that the house owners would be liable for.</p>

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Helena Gordon				<p>I believe that by allowing a road and housing to carve up the natural space, Middlesbrough Council have demonstrated that it has put the increase of council taxable housing first. Any consideration towards promoting the area as a green space is laughable. Due to the road it will not be an area young families will be gathering and playing and the noise levels and subsequent pollution will make it a no go area except for the poor people who buy those houses.</p> <p>It was always the intent of the Council to build the road but to dress it up under the guise of a woodland park beggers belief.</p> <p>Until you address the infrastructure no housing should be built as it inevitably creates more problems. The Marton crawl became the Acklam crawl and the roundabout the new road leads onto to give access to the A19 is already problematic during rush hours so this new road leading directly onto it is bad planning. The decision has already been made so this "consultation" is a total sham. It will be done and the Council is just dressing it up by planting a few trees and calling it a woodland park. We are all familiar with artists impressions of how wonderful the area will be and the reality is never the same. Honesty is preferable to being lied to.</p> <p>I feel you may as well build the road as you have already decided to, but dont take us, your tax paying public for granted. We are all tired of the lies and deception we have been served and will have our say at the next opportunity. Ben Houchen is fighting to create a green environmental area to attract jobs and business to our area and the Council does this? Its high time you all sing from the same Hymn sheet and prove to everyone that we are moving forward but as usual in Middlesbrough, greed comes first.</p> <p>I object to this debacle and feel sorry for the people who live sandwiched between the A19 and this new road.. Country park it will NOT be.</p>
Henry Cumbers	Historic England			<p>Historic England is the Government's statutory adviser on all matters relating to the historic environment in England. We are a non-departmental public body established under the National Heritage Act 1983 and sponsored by the Department for Culture, Media and Sport (DCMS). We champion and protect England's historic places, providing expert advice to local planning authorities, developers, owners and communities to help ensure our historic environment is properly understood, enjoyed and cared for.</p> <p>General comments</p> <p>Historic England welcomes the preparation of a masterplan for the delivery of a strategic site at Stainsby Park which will support the proper planning of the site allocated under policy H21 of the Housing Local Plan (2014).</p> <p>Paragraph 185 of the NPPF requires plans to set out a positive strategy for the historic environment. It is important that the plan seeks to both conserve and where appropriate take opportunities to enhance heritage assets and their setting.</p> <p>We are concerned by the current absence of any reference to the historic environment within the draft masterplan document. Whilst we acknowledge that there are relatively few designated heritage assets within proximity of the site, the site lies within very close proximity of the Scheduled Monument of Stainsby Medieval Village (List UID 1016352). The site may also be of archaeological interest.</p>
Ian Marron				Please remove reference to SWWE and link to Mandale road.

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Ian Rogers				The pandemic has changed the way we work and live, as well as increasing our love and respect for the natural environment. Building on mature meadows, destroying habitats and endangered species goes against everything that a responsible council should be doing.
Ina Tullo				This road is l'll thought out and will be a dangerous addition to the traffic system. The Meadow will no longer be fulfilling its purpose.
J Duffy				I just wish to say I don't agree with the plans you have for Stainsby, it should be left alone, people are happy with it as it is and it was also a lifesaver during lockdown when people and dogs enjoyed the greenspace which was essential for everyone's wellbeing and mental health and still is essential for peoples wellbeing. It is a lovely long walk right from Low Lane to Mandale Road with no busy roads. The precious green space is going to be spoilt, overcrowded and congested with houses becoming a concrete jungle and a dangerous busy road also close to the A19. Dog walkers will no longer be able to let their dogs off the lead for a bit of safe freedom on one of the only precious safe green open spaces left as I have been able to do for over 45 years and people with young children will not be able to have them running carefree because of the new road that is planned. The wildlife will be disrupted as well, there are deer and sparrowhawks and other wildlife that go in the present woodland. The next thing that will happen is you will build a shopping development to cater for these houses and take up even more valuable green space, it will never end.
Jacqueline Jones				I would like to see the area to be considered a Nature reserve with appropriate access .cylce routes for families
Jacqueline Young				At least its a draft, go back to the drawing board and find another option for the road
jamie barber				frankly, none of this should go ahead. you're planning to destroy natural green space and animal habitats that are at a premium in Middlesbrough.
Jamie Killington				As a local resident I am very concerned about the future development. Acklam has very little green space as it is; with avenue of Trees being the only outlier, (Mill Hill also potentially being fenced/sold off in the future). In a time where people need outdoor space for physical and mental health I feel the decision to build on Mandale meadows is pretty tone deaf. I understand money needs to be made as funding is very low, but this could have many knock on affects.
Janet Jackson	South Tees Hospitals NHS Trust (Radiographer)			This area means a lot to the local people, more so as a result of the Covid pandemic, as it has provided a very accessible place to escape, exercise, socialise and destress during what has been a very difficult time. I have visited this space in sections or walking the length of the green belt, starting from Hemlington Lake through to the Tees Barrage. At certain points, there are no houses visible and very little noise from the A19 to be heard and it provides a natural country space within the centre of population. I feel that this space needs care taking of it and that the road through the Mandale Meadow would spoil and negatively impact on this area for the local users, the many activities which are possible, the wildlife and the overall health of the town. I appreciate there may be a need for additional road infrastructure but not through the green belt and Mandale Meadow. This is a decision which needs to be made with care because if the wrong decision is taken now, the damage will be irreparable.

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Janet Johnson				I have no suggestions for alternate route but strongly object to the spine road planned route. It will cause too much congestion and the road is so close to my home the noise will be unbearable. My main complaint that I expressed before is the land it will destroy is so beautiful and home to many creatures some protected and lots of plants. This piece of land has been a safe haven for some many in this area during covid and to lose access to this amazing space would be criminal. When I bought my house I bought it because I was assured that this land could never be built on!
janice slater				I just wish to reiterate my request for further information relating to all of my questions and points that i have requested information and clarification on. please could these be sent to me at janice.slater@hotmail.co.uk as soon as possible and prior to any decision being made with relation to Mandale and or the SWWE road. thank you Janice Slater
Jess Brown				To profess that Middlesbrough is going green then build a road right through a green space is just madness. Think about developments before granting permission. If the infrastructure isn't there then don't try and shoehorn it through the few green spaces we have left.
Jill Gristwood				My husband has early onset Alzheimer's disease. He still retains some independence by walking our dog on Mandale Meadow. Clearly he will not be able to do this unassisted if such dramatic changes take place. We live on Preen Drive which has traffic calming measures, and trust me, these do not stop cars and motorbikes speeding down our road. Therefore we have no confidence in the assurance that the road from the housing development will have both traffic calming and speed restrictions (although I've seen the speed limit will both 20MPH & 30MPH!) I cannot be reassured that these measures will work. Subsequently our dog walks, with the dog off the lead, will not happen. Families with young children won't feel confident that children can run around freely. There will be a large number of cars heading down this rd towards the A19 at all times of the day. Is there any reassurance that in 5 or 10 years time the council won't change their minds about building houses on the green area? Perhaps the road is the first part of a plan to eventually build homes on the meadow as most of its regular users will be unable to use this green area of Acklam.
Jill Hadfield				No road through Mandale Meadows

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Jim Smith	Forestry Commision			<p>Thank you for seeking the Forestry Commission’s advice about the impacts that this application may have on woodland. As a non-statutory consultee, the Forestry Commission is pleased to provide you with the following information that may be helpful when you consider the application.</p> <p>Please ensure that all felling is clearly outlined in the planning documentation. Any trees subject to felling not identified in the planning documentation could be subject to the need for a felling licence from the Forestry Commission.</p> <p>There is an existing Forestry Commission England Woodland Grant (EWGS 27585) Woodland Creation Scheme within the boundary of the red line and is still in obligation, please contact the Forestry Commission for more information on this. The scheme is also in receipt of Farm Woodland Payments.</p> <p>There are several priority habitat woodlands within the boundary map provided which would be good to retain and manage in accordance with the UK Forestry Standard https://www.gov.uk/government/publications/the-uk-forestry-standard This guidance sets out the UK government’s approach to sustainable forestry, including standards and requirements, regulations and monitoring, and reporting.</p> <p>We recommend a management plan(see https://www.gov.uk/guidance/create-a-woodland-management-plan) is developed for ongoing management of any new woodland sites, and that management is considered in relation to neighbouring or other existing woodland in the local landscape. The Forestry Commission can offer assistance in the production of both creation and management plans.</p> <p>The Forestry Commission would strongly encourage the applicant to consider climate change when developing their proposed development. The predicted changes in temperature along with introduced plant pests and diseases mean that we there is a need to create and manage woodlands that are more resilient to these threats.</p> <p>Woodland adaption for resilience can be achieved through:</p> <ul style="list-style-type: none"> • Planting a wider range of tree species. • Using seed from a wider range of origins and provenances, including planting native trees outside their natural range. • Encouraging natural regeneration where it is likely to be successful, to encourage evolutionary adaptation and as the climate changes. • Protecting from damaging animals. <p>Further information can be found in the Forestry Commissions guide to “Responding to the climate emergency with new trees and woodlands”.</p> <p>https://assets.publishing.service.gov.uk/government/uploads/system/uploads/attachment_data/file/892714/Responding_to_the_climate_emergency_with_new_trees_and_woodlands.pdf</p> <p>Can the applicant please confirm that no ancient woodlands will be affected by the proposed development? One of the most important features of ancient woodlands is the quality and inherent biodiversity of the soil, they being relatively undisturbed physically or chemically. This applies both to Ancient Semi Natural Woodland (ASNW) and Plantations on Ancient Woodland Sites (PAWS). Direct impacts of development that could result in the loss or deterioration of ancient woodland or ancient and veteran trees include:</p> <ul style="list-style-type: none"> • damaging or destroying all or part of them (including their soils, ground flora or fungi) • damaging roots and understory (all the vegetation under the taller trees) • damaging or compacting soil around the tree roots • polluting the ground around them • changing the water table or drainage of woodland or individual trees • damaging archaeological features or heritage assets <p>It is therefore essential that any ancient woodland identified is considered appropriately to avoid the above impacts. Page 3</p> <p>Unfortunately, we have not been able to visit the site to develop our comments, although from looking at our mapping system and the Countryside Stewardship targeting areas, it would appear that there are several opportunities to develop and improve riparian woodland that could potentially benefit flood risk and potentially water quality particularly in the area surrounding Blue Bell Brook.</p>

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Joan Hornby				I will email.
Joan Pollak				<p>I did not see your letter about the Stainsby masterplan when it arrived, as it was only addressed to my husband so I did not open it . I am disappointed at the time frame allowed for the public consultation of only 4 weeks, given the implications of the plan on the local community. As we are in the middle of a lockdown situation in the middle of a pandemic, a much longer time frame of at least 4 months should have been given. Certainly it would have been helpful if it could have been discussed with our neighbours and fellow residents affected by this and at public meetings with your department with maps, plans and illustrations like we had at previous meetings, which would have been possible if we were allowed to meet up. As the letter was unhelpful and the map on the reverse lacking any detail, it has taken me a long time to access the information and I have been unable to access any on line response form.</p> <p>. I feel that generally we need to reassess future needs following the likely changes brought about by the pandemic. It is now likely that many more people will continue working from home which will have a considerable effect on commuting and the need for offices. There may be much less demand for travel to town centres with the growth in internet banking and shopping on line. People have appreciated their local environment and outside space more and are wanting larger gardens and offices at home. This will have an effect on what sort of development is now desirable and given the cost of the pandemic, what will be affordable. I look forward to hearing about the outcome of this consultation and seeing amended plans.</p>
Joanne Coe				<p>If the current plans go ahead, the whole of Mandale meadow will be destroyed. It's a place of natural beauty; a place that is home to a myriad of wildlife, some of which are endangered; it is a place of quiet and calm; a place that has helped people's mental health; a place that has stood for years and should be allowed to stand untouched for many more. Should the current plans go ahead, the meadow will be destroyed. The noise pollution will make Living nearby unbearable; air pollution will increase and consequently ill health of residents will increase; children will have nowhere to play; mental health and well-being will suffer; youths will abuse the so called country park - as we all know antisocial behaviour in a clamor in out of control at the moment. Traffic will be unbearable ; the sheer volume will cause congestion at exit point, which will back up on surrounding roads. People's commute times will increase, stress will increase. I will not feel safe in my garden with a road running nearby and a county park that will attract people that I don't want out the back of My house. This truly is a dreadful idea for all. There's an alternative that should be fully investigated - look at the loop road. I would like to see evidence / investigations into this matter.</p>

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Joanne Mitchell				<p>The A19 has already been widened at some cost in terms of physical and mental health and the general well being of residents. Issues exist which require resolution regarding noise and air pollution and ought not to be made worse still by this imposition.</p> <p>The road would destroy the meadow.</p> <p>These sad plans they have to ruin one of the most beautiful parts of Middlesbrough not to mention the habitat of wildlife that will disappear without the meadow. Greenspaces are very precious and need to be kept as they are. Years of conservation and preservation work has been invested. People moved close to the meadow being given to understand it is protected, a greenbelt where no building or any development may ever take place. It may never have been a designated greenbelt but it was the subject of a Deed of Covenant, the land having been gifted to the people of Middlesbrough in perpetuity. A road is being planned through the meadow to service a housing estate.</p> <p>At the end of the day destroying a pristine meadow which is rich in biodiversity and has never been 'treated' with pesticides and herbicides, is nothing short of ecocide and vandalism and makes a laughing stock of the council's One Planet Living and green strategies.</p> <p>There was always another option, the 'developers' could create an estate loop road similar to the one at Trimdon.</p> <p>The council has stated they won't even pay local residents a penny in compensation for taking our public amenity from us and subjecting us to increased air and noise pollution, damaging our health.</p> <p>the disruption, destruction, the irreparable damage, and the blatant disregard for people who have lived here and who have been involved during many years in the care and preservation, and who will have their lives destroyed, just beggars belief. Residents already suffer the constant excessive traffic noise...and the pollution inflicted upon them. To think that there are those with the power who lack the empathy and the understanding, or thought and consideration for people who use the meadow and those who live close by is unbelievable.</p> <p>The residents in the Ayresome ward would no longer be able to access Mandale meadow from Mandale Road.It will impossible for people in the Ayresome ward, including those of us who live a 2 minute walk from the current entrance to Mandale meadow is because it would involve walking straight across two lanes of the proposed roundabout. The road on OUR meadow stopping us accessing OUR meadow is the issue. All this nonsense about "you are getting extra greenspace" is an insult when you are planning something which prevents us from accessing OUR green space.</p> <p>The remarkable thing about this is some of the language used in the document to describe the proposed plan: "To create a diverse ecosystem" etc when one already exists that will be destroyed by the building of so many houses. The plan to create this "diverse ecosystem" on land which has been polluted by herbicides, pesticides and pollution from the building process while destroying a pristine meadow which is genuinely a diverse ecosystem containing Red and Orange endangered species marked on this map of Mandale meadow. They know this because the map and the legend were included in the Greenstuff response to the 2018 LP.</p> <p>The area is home to rare species flora and fauna, which have taken decades to fully establish, all of this would be destroyed.</p> <p>The faux manufactured country park, cannot, and will never, as if by magic, replace this.</p> <p>Mandale Meadow is well used and loved by many, an accessible place in which we are able to walk and relax.</p> <p>Residents currently suffer excessive noise and air pollution levels 24/7 owing to the proximity of the A19...how can it possibly be considered in any way acceptable to bring yet another road even closer to homes.</p> <p>When the A19 was widened residents received compensation for the loss in value of their homes..but despite the proposed road being routed even closer we are told we would not receive any such award for all of our losses. ..the loss of peace, and of mental and physical health. .. These of course can never be compensated for. It is not what we want.</p> <p>The notice delivered to each resident shows a basic outline of the area concerned, part of which is Mandale Meadow at the top of the 'plan'. The notice gives the impression that the entire area will be Country Park, along with which would come 1700 houses, not forgetting the so called Relief Road, to 'relieve' congestion on the A19 and serve the 1700 new homes. The plan actually shows the road through Mandale Meadow which would discharge traffic onto Mandale Road, at a junction adjacent to residents homes where the current car park is now situated, actually at the foot of their gardens.</p>
John				<p>Will this be monitored by camera and have a fair level of security?i don't want another albert Park where you have major issues with drugs and crime.</p> <p>Its a great idea in principle but I don't want another no go area ruined by drug misuse and anti social behaviour.theres plenty of issues with that at trimdon shops and the park which the police can't control.</p> <p>Its a great idea but I fear the few will spoil it for the majority of good people.</p> <p>People speeding around on off road bikes and kids littering empty cans and bottles all over every weekend will be problematic</p>

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John and Janet Roebuck				<p>Our overall conclusions and response to the Council.</p> <p>1. We do not support the Country park and masterplan as put forward by the Council for the following reasons:</p> <p>a. The Council has not followed the requirements of the NPPF, which:</p> <p>i. requires the preparation and review of all policies to be underpinned by relevant and up to date evidence and</p> <p>ii. places a duty on local planning authorities to co-operate with each other, on strategic matters that cross administrative boundaries</p> <ol style="list-style-type: none"> 1. Policy H21 was adopted in November 2014. 2. Its requirement for a spine road was based on evidence taken from a report produced in October 2013, which had used a base year of 2005 for traffic journeys 3. That same report required a further study to determine the extent of mitigation that would occur if the SWWE was undertaken 4. Council has subsequently modified Policy H21 in relation to the protection of Mandale meadow from housing. However, it has not revisited the basis of the October 2013 report which is outdated, as will be shown below 5. Council has failed to address improvements to the road schemes associated with the A19/A174 junction, that occurred in 2016 6. Council has failed to provide evidence of co-operation with Stockton Council in relation to the impact of their Local Plan policies on the Strategic Road network, key route networks and the junctions at A1130 and A174 7. Council has failed to take account of road schemes that have not materialised, that have an impact on the Stainsby development. Again, these are referenced below 8. Council has failed to review other relevant housing policies at Coulby Newham and Nunthorpe Grange, to determine whether they are still relevant, in light of the changing demographic nature of Middlesbrough, based on both the 2014 household projections, for assessing housing needs and the latest available 2018-based ONS population projections, for social and economic implications <p>b. The lack of honesty, openness and transparency associated with the basis of the consultation process, given the interventions and mixed messages from the Mayor, Deputy Mayor and senior officers of the Council, mid-way through the consultation period</p> <p>i. It is evident that responses to the consultation will not be treated objectively, given the views of senior officers, in the Council, that the spine road must proceed</p> <p>ii. It is apparent that, for consultation responses to be treated fairly, an independent review team needs to be appointed to assess the responses. Clearly, Council officers have demonstrated their bias in relation to the country park and masterplan</p> <p>c. Insufficient evidence to substantiate the assertions made by the Council for the Country park and masterplan, e.g.</p> <p>i. Supporting evidence is required in relation to net gains in biodiversity, climate change resilience and ecosystem services to the wider environment</p> <p>ii. No reference is made to volumes of traffic likely to use the spine road, assessment of air pollution and the means by which national emissions targets will be achieved</p> <p>iii. Given the scale of the masterplan, consideration of financial issues is sadly lacking and has been brushed over in supporting documentation, particularly the FORE repor</p> <p>d. The Council has failed to review its base data, contained in the ARUP report of October 2013, associated with the projected populations, households, jobs, workers</p>

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				<p>and trips produced, to ensure that the original assumptions are still sound.</p> <p>i. Relevant, up to date evidence, from the Office of National Statistics (ONS) indicates that the ARUP population and household figures have subsequently been proven to be significantly over-stated. This means that the assessment of trips produced will equally be overstated.</p> <p>Demographic projections used by the ARUP transport study in March 2014 have not materialised.</p> <p>Population Households Year ARUP Model ONS 2018- based Difference ARUP Model ONS 2018- based Difference 2020 142,478 140,423 -2055 60,051 57,078 -2973 2030 146,513 139,361 -7152 63,652 57,330 -6322 2035 147,943 139,070 -8873 65,349 57,809 -7540</p> <p>The 2018 based ONS projections show that the population and household numbers for Middlesbrough in 2035, are far less than those used by ARUP for its 2020 projections. Hence, decisions based on the assumptions arising from the ARUP model, should now be disregarded and a new traffic model re-run, reflecting the demographic and economic conditions as they stand now and projected forward.</p> <p>The Tees Valley Local Industrial Strategy Evidence Base, provides a good basis for developing a new model. This is referenced in the Middlesbrough Local Implementation Plan, (LIP) which also makes the point that:</p> <p>"an integrated transport network...will reduce dependence on the private car, thereby improving accessibility and releasing capacity on the road network, without the need for expensive (and often unaffordable) highway improvement schemes"</p> <p>Demographic projections used by the ARUP transport study in March 2014 have not materialised.</p> <p>Population Households Year ARUP Model ONS 2018- based Difference ARUP Model ONS 2018- based Difference 2020 142,478 140,423 -2055 60,051 57,078 -2973 2030 146,513 139,361 -7152 63,652 57,330 -6322 2035 147,943 139,070 -8873 65,349 57,809 -7540</p> <p>The 2018 based ONS projections show that the population and household numbers for Middlesbrough in 2035, are far less than those used by ARUP for its 2020 projections. Hence, decisions based on the assumptions arising from the ARUP model, should now be disregarded and a new traffic model re-run, reflecting the demographic and economic conditions as they stand now and projected forward.</p> <p>The Tees Valley Local Industrial Strategy Evidence Base, provides a good basis for developing a new model. This is referenced in the Middlesbrough Local Implementation Plan, (LIP) which also makes the point that:</p> <p>"an integrated transport network...will reduce dependence on the private car, thereby improving accessibility and releasing capacity on the road network, without the need for expensive (and often unaffordable) highway improvement schemes"</p> <p>e. The ARUP report indicated that both the Longlands to Prissick link and the SWWE improvement schemes needed to be tested to identify the scale of mitigation that is afforded by the infrastructure.</p> <p>i. There is no evidence, provided publicly, to demonstrate that the tests have been undertaken. Council is requested to provide the necessary evidence by a re-run using the new strategic transport model.</p>

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				<p>ii. Council should make its position clear on the Longlands to Prissick link and assess the implications of not proceeding with this link</p> <ol style="list-style-type: none"> 1. If this road does not go ahead, what is the alternative mitigation that is being planned to enable the development to go ahead in full. 2. Alternatively, how many dwellings will not be able to be provided, if no alternative mitigation is generated? <p>f. The Council has not produced a detailed, robust business case to assess the relative merits of the 4 options for the road supporting the Stainsby development, as required by the Transport Appraisal Guidance, referenced in the FORE report.</p> <ol style="list-style-type: none"> i. Capital costs associated with the 4 options range from £10-£25m, for Option 1, through to £100m+ for Option 4 ii. No indication has been given to the number of dwellings that can be supported by a loop road in Option 1. iii. No assessment of affordability has been provided for any of the options or any funding streams iv. It is essential that the Council has the full objective assessment provided by a detailed business case, before committing to a masterplan that could be undeliverable v. The business case should assess all four options in detail vi. There is no reference or commitment from the TVCA Road Implementation Plan 2020, which deals with Key Route Networks across Teesside, to support funding of the SWWE. Assertions in the FORE initial option assessment report, EAST assessment, are therefore invalid, in terms of its funding source. vii. Given the scale of investment needed in relation to the Council's preferred option, there is no specific reference in the Middlesbrough LIP to this scheme <p>g. The Council has a duty to co-operate with other councils, where there are common issues. There is no evidence of consultation with Stockton Council, given that one of the major reasons for the SWWE, is to address capacity and congestion issues on the A19 and the respective junctions at the A174 and A1130, both of which are impacted by traffic from both Middlesbrough and Stockton</p> <ol style="list-style-type: none"> i. The ARUP report, from 2013, referenced work associated with the Portrack Relief Road and the Parkway extension, both schemes within Stockton, that would assist in providing additional capacity to support the growth peaks in south west and west Middlesbrough. ii. Clearly, liaison on these schemes, together with the impact of any further housing and commercial development that could impact on the A174 / A19 or A1130 / A19 junctions, should be assessed and built into a new transport assessment <p>h. The Council should review the need for new housing developments in the south of the town, in light of the issues associated with road schemes at SWWE, Longlands to Prissick link and Nunthorpe park and Ride.</p> <ol style="list-style-type: none"> i. The Council already has a 10-year supply of housing based on its latest 5 year housing land supply. ii. Reduction in the development of housing sites in south Middlesbrough, would reduce the pressure on the A19 / A174 junction. Schemes at Coulby Newham (1000 dwellings) and Nunthorpe (250 dwellings) could be de-allocated from the new Local Plan, without any significant detriment, as a result of substitution of new urban living sites nearer to the centre of Middlesbrough being prioritised. <p>i. The Executive report of 22nd December, makes a number of claims in relation to:</p> <ol style="list-style-type: none"> i. the catastrophic impact on the town's overall housing growth plans <ol style="list-style-type: none"> 1. 643 dwellings have already been approved for the Stainsby site. 2. The Council itself has reduced the scale of housing development by 100 dwellings as a result of its decision on Mandale meadow. This leaves around 900 dwellings for future development, equivalent to 1/10th of the total potential housing allocations presented to the LPMWG on 22nd December 3. The Council has not quantified the number of dwellings that would be lost as a result of a Loop road option being developed ii. 5-year housing land supply <ol style="list-style-type: none"> 1. The table below shows the current position reported by the Council <p>Deliverable supply of Housing</p>

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				<p>Ref Number of dwellings A Five-year requirement 2020/21 to 2024/5, including 5% buffer 1344 B Deliverable supply 2020/21 to 2024/5 2686 C Deliverable supply over and above the five-year requirement (C=B-A) 1342 D Average annual requirement 2020/21 to 2024/5 (D=A/5) 269 E Deliverable supply expressed as a number of years (E=C/D+5) 9.99 years</p> <p>The only sites identified in the 5-year housing land supply, associated with Stainsby, are the sites at Brookland Park and Brookfield Woods. Taken together, these amount to 164 dwellings. If these were not to go ahead, the impact on the 5-year housing land supply would be to reduce the deliverable supply to 2522. Expressed in years, this would fall to 9.37 years, from the 9.99 years</p> <p>iii. achievement of the Medium-Term Financial Plan</p> <ol style="list-style-type: none"> 1. The table below shows the delivery of new dwellings against the 2014 Local Plan expectations <p>Housing growth compared to 2014 local plan assumptions</p> <table border="1"> <thead> <tr> <th>Year</th> <th>Local Plan assumption</th> <th>Actual delivery</th> <th>Difference</th> </tr> </thead> <tbody> <tr> <td>2014-15</td> <td>300</td> <td>678</td> <td>+378</td> </tr> <tr> <td>2015-16</td> <td>300</td> <td>557</td> <td>+257</td> </tr> <tr> <td>2016-17</td> <td>300</td> <td>536</td> <td>+236</td> </tr> <tr> <td>2017-18</td> <td>410</td> <td>408</td> <td>-2</td> </tr> <tr> <td>2018-19</td> <td>410</td> <td>592</td> <td>+182</td> </tr> <tr> <td>Total</td> <td>1720</td> <td>2771</td> <td>+1051</td> </tr> </tbody> </table> <ol style="list-style-type: none"> 2. The scale of actual delivery achieved (61% increase) against the Local Plan assumption implies that the growth in Council Tax cumulatively over the 5-year period will be sufficient to meet the MTFP. 3. For 2019-20, the Council has also indicated that it achieved actual delivery of 655 dwellings against its average of 410 dwellings per annum, again, further evidence that MTFP targets, associated with Council Tax growth and New Homes premiums are being achieved 4. The Council anticipates delivering around 500 dwellings per year for the next 5 years <p>As such, Council Tax revenues and New Homes bonus will be achieved to meet MTFP requirements. Taken together, there is no basis for the assertions provided in the report. .</p> <ol style="list-style-type: none"> 2. In light of the above points, we support the option of a loop road to serve the Stainsby development. In our opinion: <ol style="list-style-type: none"> a. The Council should have undertaken a masterplan for the entire site of 1670 dwellings, prior to the start of the development b. The initial stretch of the Jack Simon Way is not fit for purpose as a spine road, given that the access has not been planned to deal with the volume of traffic that a spine road may attract c. Introducing a spine road, along what is an estate access road, will be detrimental to the public health of residents living along Jack Simon Way d. There is insufficient evidence to support a spine road, given the over inflated population and household figures used in the Arup report 	Year	Local Plan assumption	Actual delivery	Difference	2014-15	300	678	+378	2015-16	300	557	+257	2016-17	300	536	+236	2017-18	410	408	-2	2018-19	410	592	+182	Total	1720	2771	+1051
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				<p>e. The Council should protect Mandale Meadow and Bluebell Beck and avoid inflicting harm on the natural environment</p> <p>f. The Council has failed to take account of the option to reduce uncommitted new housing developments in the south of Middlesbrough in light of the infrastructure issues associated with the Longlands to Prissick link and the Nunthorpe park and ride schemes.</p> <p>g. In funding terms, the loop road is the most cost effective option, if costs contained in the FORE report are to be believed, and is likely to meet viability tests for developers, albeit, allowing for a marginal reduction in the overall number of dwellings to be developed on the site</p> <p>We are happy to discuss the points in more detail if required.</p>
John Baines				I do not support the introduction of a new spine road as it will increase traffic congestion on an already busy Mandale road.It would destroy the peace and beauty of the green space in particular Mandale meadow.I would fully support the creation of a country park in the area without a spine road for vehicles.Build a cycle path along the same route and divert the cars onto the A19 North and south.
John campbell				Please rethink this proposal - there are alternatives

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John David Duffy				<p>Please don't listen to the few. Facts need to be presented, not the fake news that has been passed through peoples doors, during a pandemic may i add. I have also seen posters that i thought was designed by middlesbrough council, but upon closer look, the logo has been amended to Middlesbrough Moving Backwards. I think this is clearly misleading and smacks of fake news.</p> <p>Please also don't think that only those who have provided comments through this process are those who are the only ones interested. You will through this process only really hear the view of the campaign group. This is the minority of NIMBY views. I have seen via social media, outside of the campaign pages, a positive perception of the scheme. This will attract people not only from Middlesbrough, but hopefully people from wider afield, making Middlesbrough a place people want to come visit and live.</p> <p>Well done.....</p>
john moses				I have made the comments that I feel are needed about this proposed development in the initial comments section.
John Reid				<p>One other point which has come to light is the septic tank overflow line from Stainsby Hall Farm house & other residents (including my property The Cottage) at present exits into a gully North of all properties which appears to be in the middle of a proposed housing development.</p> <p>In November 2015 we had an issue over the style between our property & The Weightmans property whereby an elderly gentleman had fallen over the second style on the Weightmans land and then again on our mutual style between our properties.</p> <p>As you may recall at our meeting at that time at the said style, I offered this same solution up to you then whereby you informed me you were already looking into the history of this track with a view to extending the lane to accommodate a bridle way as well.</p> <p>After bringing this to your attention three times now I am therefore at a loss as to why, after 3 years, this was not incorporated into the plan for the forthcoming country park.</p>
John White				I confirm that I support this plan.
Jonathan Hills				<p>How will the creation of the park affect the residents currently living in the boundary?</p> <p>Will they still be dependent on private landscaping companies or green area maintenance?</p>
Judith Harbron				No development please think of the people who live in the area and use the space, not much grass land left we need to protect. You need to get out and see how well used the land is, it is been a lifeline to a lot of people especially during the pandemic. The plant life and wildlife need to be considered we need this space for our well-being.
Judith Maunder	Friends of Mandale, Greenstuff,			No road through Mandale. The features for the estate are acceptable for new residents but it shouldn't mean Ayresome ward residents have to suffer for it. No road through Mandale.

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	resident very local to Mandale Meadow			
Julia Povey				Mandale meadows was gifted to the people of Middlesbrough. It should remain ours as a natural green space forever. Future generations should benefit from this as a natural green space, not a manufactured one that can never bring the same benefits as a natural beautiful area. And once it is lost, it is lost forever. NO ROAD THROUGH MANDALE MEADOW/WOODS
Julie Riley				NO ROAD THROUGH MANDALE More than likely this is a decision already made, the area should not have to put up with a road so close to people's houses, the noise and pollution is not right for people's health and wellbeing. The council supported the last campaign at Newfield and needs to put help into supporting this one, the area is a greenbelt with lovely walks and plenty of wildlife it will be an absolute disgrace to allow this to be taken away
Kabaser Ajaib				I don't give consent for stainsby Park scheme(through road mandale). I believe there is a lot of wildlife that are close to being endangered of their natural habitat. I also have a child with a disability who loves the stainsby meadows and enjoys its natural land. We will be seeking further action if this continues as it would cause life changing in my sons timetable of exercise and enjoyment which would cause his great distress and self harm.
Karen Watson				You are just proving what people already know ,You don't care about what we have , in town is a dump empty spaces no houses (where houses used to be)instead your killing wild animals hedgehogs ect that are on a great decline thanks to people like you, who pretend you want to do the best for this planet, you could'nt care less! how much money have you all made in your back pocket putting a slip rd in, which we do not need through a much used meadow by walkers, visitors ect
Kate Bowe				Please reconsider this disastrous plan and leave the residents with some green space in which to walk.
Keith Russell				No road through Mandale Meadow
Keith Young				No road through Mandale, you can do what you like on the private land

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Lauren Teate				<p>Horses should not be allowed to use this site along Bluebell Beck. Many people do not know how to behave around horses and I fear this will be yet another problem for local wildlife and dog walkers. Allowing horses, cyclists and hundreds, if not thousands, of visitors access to the site on 3.5m paths that cut through every bit of habitat is going to completely ruin the place for the local residents and for the wildlife. The paths should be kept to the housing estate side of the beck and not encroach onto Bluebell Beck at all.</p> <p>If this must happen, there must be places for wildlife to be left undisturbed that are inaccessible to dogs, horses and people. In addition there must be places for dogs to be walked off lead where they cannot run onto roads, housing estates or come into conflict with children, cyclists and horses. As mentioned previously, dog owners and dog walkers are being pushed out of greenspaces and this creating more problems in the local areas. Dogs are an integral part of our culture and cannot be ignored and must be considered in the local plan.</p> <p>Wildlife should be at the heart of the Country Park, not people. There has been barely any mention of mitigation for the disturbance of wildlife at the site and I am disappointed that no surveys or studies have been done by any local wildlife group/organisation/charity. I, myself, have done many surveys along Bluebell Beck and have evidence of red listed species using this area for breeding. I have brought this up several times and it seems to have been completely ignored. Please find attached the documents that list the species residing/breeding on Mandale Meadow.</p>
Leah Davey				<p>This site allocation will cause SIGNIFICANT detrimental impacts to the existing wildlife site at this location. It will destroy the character and openness of the agricultural landscape and loss of visual amenity. Mandale meadow is frequently used and much loved by residents, who were NOT properly consulted prior to the 2014 local plan publication.</p> <p>This housing development is wholly unnecessary given the large supply of deliverable sites elsewhere on brownfield. This remains one of the last irreplaceable areas of major open space. It is also adjacent to an existing green wedge. Development adjoining green wedges can have a significant detrimental impact.</p> <p>Serious thought needs to be given to the validity of this entire scheme. Times have changed and residents are not happy. The new local plan should be designed given up-to-date circumstances, evidences and policies, including the revised National Planning Policy Framework and recent five year housing land supply assessment, in the light of a post Covid Middlesbrough.</p>
Liam Harris				This proposal should never have gone past an initial idea, the council don't have the knowledge or funding to make this work long term.

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Linda Barker				<p>Please take into full consideration the representation you have received from CPRE and the open letter from Greenstuff Middlesbrough.</p> <p>The current road at Jack Simon should loop through the current housing. Further development should be halted at this site and other greenfield sites in Coulby Newham and Nunthorpe. This would then not impact on infrastructure and Highways England would not disapprove.</p> <p>This housing allocation is not necessary and should stop.</p> <p>Any 'country park elements' should remain within the site already under construction and NOT damage the area on Mandale Meadow, Newfield Meadow or the connecting green spaces by the addition of landscaping features, footpaths, cycle paths, play equipment or 'desire lines'.</p> <p>This site allocation will cause SIGNIFICANT detrimental impacts to the existing wildlife site at this location. It will destroy the character and openness of the agricultural landscape and loss of visual amenity. Mandale meadow is frequently used and much loved by resident, who were NOT properly consulted prior to the 2014 local plan publication.</p> <p>This housing development is wholly unnecessary given the large supply of deliverable sites elsewhere on brownfield. This remains one of the last irreplaceable areas of major open space. It is also adjacent to an existing green wedge. Development adjoining green wedges can have a significant detrimental impact.</p> <p>Serious thought needs to be given to the validity of this entire scheme. Times have changed and residents are not happy. The new local plan should be designed given up-to-date circumstances, evidences and policies, including the revised National Planning Policy Framework and recent five year housing land supply assessment, in the light of a post Covid Middlesbrough.</p>
Linda Hyde				I am concerned about the amount of traffic that will be generated. The additional houses will make it difficult for the residents. The level of pollution will be increased greatly and has been shown that pollution could cause an increase in the illnesses of older people and children. The increase of children going to schools in the area particularly Acklam Grange will cause more problems with traffic on Heythrop Drive during morning and evening school times. It is already difficult to leave drives and access to Mandale Road during these times.
Linda Hyde				We have lost too many green spaces and this will have a detrimental effect on people's health. There has already been studies showing that traffic air pollution is dangerous.
Linthorpe /Acklam Resident				FORGET ABOUT IT
Lorraine Meynell				There will be too much traffic and for those of us who live nearby it will be a nightmare trying to get in and out if Mandale Road
Lynn Blagg				Leave site as it is to enable residents and others to use it in its natural state for exercise and pleasure and avoid destruction of wildlife habitats and ageing flora and fauna and avoid excess traffic in the area.
Lynne woodward				Need to implement Option 1 loop road only to protect habitat and fauna
Malcolm Chilvers				I think I have already said it.

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Malcolm Watson				<p>I genuinely believe that if local people thought that they could really believe there was any chance of the finished development looking anything like the pictures in the Masterplan, and if the road was a local road with a 30mph limit and traffic calming (or, better still, 20mph) designed to discourage through-traffic rather than being a "relief road" rat-run then there would not have been the amount of resistance that there has been.</p> <p>My concerns are as follows: The "Country Park" that has had so much publicity is badly defined. Part of the problem arises from the mixture of terminology that is being used. We have "Country Park", "Open space", "green corridor", "green space", "green public space" etc but without better definition there is no reason to think that developers will abide by the vision.</p> <p>Developers appear to have little respect for Councils' plans (ref recent St Davids decision) so it is difficult to see this consultation as more than a box-ticking exercise - it would be nice find that I am wrong ! At the end of the day it appears that the details in the Masterplan are just aspirations and are largely outside the Council's control. Phrases in the document such as "Significantly greater housing numbers suggested by developers leading to greater densities are likely to be resisted unless there are as yet unknown viability concerns" do not inspire confidence.</p> <p>I have nothing against a road per-se. But I do have strong objections to the creation of a rat-run. The idea that the solution to congestion on a 3-lane dual carriageway is to build a road through a housing development frankly beggars belief. I assume that there has been a traffic management/flow report/projection done for that roundabout and for the short stretch to the Mandale/A19 roundabout as part of the Stainsby design but I cannot see how it can be made to work.</p> <p>Further details to all of these points are contained in the attached Word document</p>
Margaret Elliot				Good idea to increase the already existing green space into an enhanced country park but will it actually happen? The developments at Whinney Banks and Coulby Newham have been left with much lacking in the promises made.
Margaret Jackson				<p>It would seem that additional housing is going to be built, though this may no longer be suited to the needs of the population in the future. To service this housing my preferred option, which does not endanger the wild meadow with all its benefits for mental and physical wellbeing and the climate would be:</p> <ul style="list-style-type: none"> • Option 1: Internal Loop – this would be an extension to/from the existing termination point of Jack Simon Way, but would not provide a connection to the A1130 to the north, thereby avoiding the need to cross Blue Bell Beck.
Martin deacy				Option 1 please
Martin deacy				There are major problems with this construction . The general public do NOT want to his road and subsequent houses as the green belt is too precious to lose . Think about the mental health of Middlesbrough residents and leave this on the drawing board.

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Martyn ROSE				<p>Please read my earlier comments.</p> <p>This entire form is also unnecessary complicated which presumably is to ease the passing of the plan.</p> <p>A public facing form should not require the mentality of a solicitor in order to complete.</p>
Matt Ridge	Homes England			<p>I would firstly like to thank you for the opportunity to comment on the consultation relating to the Stainsby Country Park and Masterplan.</p> <p>Homes England is the government's housing accelerator. We have the appetite, influence, expertise and resources to drive positive market change. By releasing more land to developers who want to make a difference, we're making possible the new homes England needs, helping to improve neighbourhoods and grow communities.</p> <p>Homes England does not wish to make any representations on the subject project at this time. We will however continue to engage with you and any further requests as part of the future decision-making process for the project, as appropriate.</p>
Matthew Rowe				I'm in full support of the master plan proposals and everything is fine in my opinion, I would like the plans to remain unchanged after the consultation.
Matthew Shippey				We already have a beautiful green space. The country park plan is just a sham to get the road in. People are not fools. What little green space we have left needs protecting for future generations. I'm not against new housing developments, I'm against building roads through our limited green space.
Michael Starford				<p>Experience with Covid has reinforced the message that "space" and connecting with nature are essential ingredients of well being. The Mandale Meadow and woodland is an established recreation area. We are concerned that in general the land within the Middlesbrough boundary is largely developed (or has been). It is more difficult to see where Stockton and Middlesbrough ends as they gradually merge</p> <p>There is not much left of sizeable and accessible natural areas for public recreation.</p> <p>My main reservation is the impact of the through road on existing traffic and the line it takes (with the additional space of the Cycleway) through the Mandale Meadow. It splits the area into two wedges. Unless calmed or screened it would be a hazard to pedestrians and dogs.</p> <p>We would also wish that there is a good blending of the "man made" among the natural features within the park.</p>

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Michelle Bennett				<p>As I live on a main road not too far away from this proposed road and housing development I can tell you how much stress this is going to cause on the local area with the amount of traffic, it's bad enough that more houses will be built on the site that St David's school used to be but adding more to the other side of Acklam Road will be a disaster.</p> <p>Have any of the committee actually spent time in this area to know how busy the roads are as it stands now?</p> <p>This is not to mention the fact that a large part of our local green space will be taken away from us, not everyone has a car to drive to the countryside, our young and elderly shouldn't have to suffer watching these last places of beauty be ruined with more tarmac and buildings.</p> <p>Green space should not mean grass verges on either side of a road or trees inbetween houses.</p> <p>Middlesbrough has had plenty of houses ripped down without anything ever been built in its place, why can't housing developments start there where we already know the roads won't be a problem?</p> <p>I think what you are planning is deviating and totally wrong, I urge you do change the plans</p>
Michelle Hedger				<p>No road through Mandale Meadow. Use the loop road option. If a road needs to be there make it a narrower road of 20mph. My children cannot play near the huge 30mph road planned. They can play now in the meadow. They will be hit by a car in this 'country park'. Please don't put all the footpaths in. We like to follow the natural winding dirt tracks. There is only lip service paid to these. We don't want artificial paths. It's beautiful, muddy and filled with nature.</p>
Mike Conyard				<p>Do not build on this land. This is much needed green space. There are no parks in this area and nowhere for people to walk, kids to play and people to live their lives. I find it disgusting that you propose to build on this beautiful piece of land. Also, who wants to live right next to the A19? These house will be worth literally nothing..</p> <p>The land is also a buffer zone from the A19 to the residents of Heythrop Drive and beyond.</p>

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Miller Homes and Avant Homes	Miller Homes and Avant Homes	Mr Neil Morton	Savills	<p>Thank you for the opportunity to provide these representations to the masterplan. Overall, whilst the Council's preparation of design guidance for this strategic site is very much welcomed, and our clients are committed to delivering a well-designed and high quality mixed community, we continue to have reservations about the significant level of detail and prescriptiveness provided by the document. This has been the continued theme of represents and this position is maintained. It is noted that this is the final consultation on the masterplan prior to its reporting to Executive and anticipated adoption. Should any points of difference remain, then these will need to be examined as part of the planning application process.</p> <p>In this regard, our clients are very pleased to confirm that they are committed to delivering the next phase of the Stainsby development associated with the remaining southern ownership at the very earliest opportunity, which once constructed, would see around half of this key strategic site developed. Following the closure of this consultation, our clients intend to commence pre-application discussions with the Council immediately and submit a major planning application around mid-2021 which would follow the anticipated adoption of the masterplan. This is a hugely exciting development for the town and we are committed to working positively and proactively with the Council and local community to rapidly advance the planning process. In this regard we very much look forward to continuing to work with the Council and its partners over the coming months ahead.</p>
Mr Craig Trainor				<p>I wish to object in the strongest terms for this development.</p> <p>We have too many fast food restaurants within a two mile radius so why on earth do we need another one. And they is plenty of supermarkets and convenience store close by to this development.</p> <p>This will cause disruption to the homes built nearby and cause more traffic problems to this area. Imagine the slip road being tailed back due to the queue for McDonald's just look at what happened recently in Redcar. We dont want or do not need this development spoiling this land.</p> <p>I and most residents object strongly and would like my opinion to be heard and look forward to your response.</p> <p>KEEP ACKLAM GREEN AND SAFE.</p>
Mr Neville stubbs				<p>Don't use mandel roundabout as a connection point to Middlesbrough or the A19! This route is already far to busy as it currently is and MUST not be used for the county park.</p> <p>Please provide the community with alternative plans to connect Stainsby County Park to the A19</p>
Mr.D.Brough				<p>I am Extremely concerned at the through road exiting onto the small roundabout on the Acklam R/d Levick Crescent small roundabout. As there is already a considerable problem with extensive traffic in this area it would seem a rather silly idea to add a to the considerable traffic flow problems already experienced in this area personally this plan needs to be taken back to the drawing board and be seriously looking at alternatives thanking you in advance.</p>

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Mrs Dorothy Gale				Please can the road be as far away from the houses on Heythrop Drive and Farley Drive as possible. Thank you .
Natalie Turley				Build on brown sites rather than what little green sites we have left. Our community stopped your greedy council from robbing us of Newfield meadow. We're willing to do the same for mandale. This online consultation is a load of rubbish too. A lot of our residents are elderly and simply do not have online access so they cannot have their say. It seems to me like an underhanded tactic to get this pushed through on the quiet. Shame on you
National Grid		Matt Verlander MRTPI	Avison Young	<p>National Grid has appointed Avison Young to review and respond to local planning authority Development Plan Document consultations on its behalf. We are instructed by our client to submit the following representation with regard to the current consultation on the above document.</p> <p>About National Grid</p> <p>National Grid Electricity Transmission plc (NGET) owns and maintains the electricity transmission system in England and Wales. The energy is then distributed to the electricity distribution network operators, so it can reach homes and businesses.</p> <p>National Grid Gas plc (NGG) owns and operates the high-pressure gas transmission system across the UK. In the UK, gas leaves the transmission system and enters the UK's four gas distribution networks where pressure is reduced for public use.</p> <p>National Grid Ventures (NGV) is separate from National Grid's core regulated businesses. NGV develop, operate and invest in energy projects, technologies, and partnerships to help accelerate the development of a clean energy future for consumers across the UK, Europe and the United States.</p> <p>Response</p> <p>We have reviewed the above document and can confirm that National Grid has no comments to make in response to this consultation.</p> <p>Further Advice</p> <p>National Grid is happy to provide advice and guidance to the Council concerning their networks.</p> <p>Please see attached information outlining further guidance on development close to National Grid assets.</p>

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Nichola Davey				<p>This housing allocation is not necessary and should stop.</p> <p>Any 'country park elements' should remain within the site already under construction and NOT damage the area on Mandale Meadow, Newfield Meadow or the connecting green spaces by the addition of landscaping features, footpaths, cycle paths, play equipment or 'desire lines'.</p> <p>This site allocation will cause SIGNIFICANT detrimental impacts to the existing wildlife site at this location. It will destroy the character and openness of the agricultural landscape and loss of visual amenity. Mandale meadow is frequently used and much loved by residents, who were NOT properly consulted prior to the 2014 local plan publication.</p> <p>This housing development is wholly unnecessary given the large supply of deliverable sites elsewhere on brownfield. This remains one of the last irreplaceable areas of major open space. It is also adjacent to an existing green wedge. Development adjoining green wedges can have a significant detrimental impact.</p> <p>Serious thought needs to be given to the validity of this entire scheme. Times have changed and residents are not happy. The new local plan should be designed given up-to-date circumstances, evidences and policies, including the revised National Planning Policy Framework and recent five year housing land supply assessment, in the light of a post Covid Middlesbrough.</p>
Nick Luke				<p>I would like to express my disappointment and vehement opposition to this proposal. I live in the Kader Area (Kader Farm Road) and moved here chiefly for the safe and green space to bring up my family. I have been here 25 years- pay full council tax and have indeed raised a wonderful family. I had hoped my grandchildren would be able to benefit from the beautiful beck, woodland and meadows in our surrounding vicinity. That hope is not dead yet.</p> <p>There are obvious environmental concerns that come with this dreadful proposal - apart from the excess pollution (when gov targets zero emissions) there is an abundance of wildlife in this area, whose habitat will be destroyed.</p> <p>Moreover, as I type, Middlesbrough has one of the worst COVID infection rates in the country. A densely populated town NEEDS green space! During the pandemic the Stainsby beck and woodland have been an absolute haven for walkers, runners and cyclists. In an area that is desperately in need of promoting a healthy, active lifestyle, why would you even consider spoiling this wonderful natural resource.</p> <p>Please discard the idea: preserve the Stainsby meadow for future generations. Think of the locality, the town, Teesside and the wider green agenda.</p>

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Nicola Smith				<p>We DO NOT WANT IT!!!!</p> <p>WHY can't you build on areas where you have flatted hundreds of houses but not rebuilt? Keith Road or Marton Burn Road??? This is the only green space we have in Acklam and hundereds of residents rely on this being walking distance. Pre covid the traffic on mandale Road, Acklam Rd and levick Cresent is bad enough at peak times without further traffic. You have Acklam Grange Secondary school round the corner from the meadow with 1200+ students that also use these roads. This is already a very busy junction without diverting more traffic to it. What about adults and children's mental health?? Has the past year taught you nothing?? I think having a open greenspace with all the wildlife and plenty of room to exercise and get much needed fresh air and been an absolute saviour to hundereds of local residents. This project has not considered anything but money. Looking at the plans there will be very little green left and it will be surrounding a road and housing estate. Absolutely nothing like we have now. This is NOT ACCEPTABLE FROM ANYONE I KNOW WHO LIVES IN ACKLAM 😞😞😞😞😞😞😞😞</p>
Nicolas Mitchell				<p>I would prefer we didn't have a new road from Ladgate Lane to the Mandale. In Coulby Newham, there is a huge housing development with one way in and out, so the Council cannot say this has not already been done.</p> <p>But this is about more than just a road and my first concern is to protect the semi-wild common land and not turn it into a concreted network of walkways and cycle ways and this is my biggest concern from the proposed development.</p>
Norma Meehan				<p>I would really like to express how I feel about the proposals for Mandale Meadow - (I realise that the Council now refer to this area as Stainsby Country Park). I find it unbelievable that the Council wish to destroy this beautiful area.</p> <p>This land was covenanted to the people of Middlesbrough and I have recently discovered that the Council have removed the covenant. I understand that the original owner sold the land to the Council - however, this was on the understanding that the Council were Trustees. Surely the Council realise what the responsibility of a Trustee is?? I feel so strongly about this and I know there are thousands of other people who feel the same - we should not have to fight for what is ours.</p> <p>So many people use that land to walk their pets, take children for walks and basically just enjoy nature. Surely we are all trying to 'save the planet' not destroy it. There are some Red listed species that live there such as the Dingy Skipper and the Argus butterflies - plus bee orchids also grow there.</p> <p>On one hand the Council want people to have plants and shrubs growing in alleys and are also promoting the planting of numerous trees in the area and yet on the other hand wish to destroy nature that being Mandale Meadow. Please will you reconsider building houses on this land and putting a road across it.</p>

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Peter Davey				<p>This site allocation will cause SIGNIFICANT detrimental impacts to the existing wildlife site at this location. It will destroy the character and openness of the agricultural landscape and loss of visual amenity. Mandale meadow is frequently used and much loved by resident, who were NOT properly consulted prior to the 2014 local plan publication.</p> <p>This housing development is wholly unnecessary given the large supply of deliverable sites elsewhere on brownfield. This remains one of the last irreplaceable areas of major open space. It is also adjacent to an existing green wedge. Development adjoining green wedges can have a significant detrimental impact.</p> <p>Serious thought needs to be given to the validity of this entire scheme. Times have changed and residents are not happy. The new local plan should be designed given up-to-date circumstances, evidences and policies, including the revised National Planning Policy Framework and recent five year housing land supply assessment, in the light of a post Covid Middlesbrough.</p>
Peter Smith				as said earlier widen the bridge and create slip roads on and of the a19 to the new estate
Poppy Bowyer				<p>A potentially beautiful investment, providing the end results meet the same standards presented.</p> <p>Some examples of personal changes - The amount of houses (1000) create high finishes and more bespoke feeling - maximise on price. The school size (700). Expand on the 'Country Park' idea and push it further - invest in cohesive impactful landscaping. Expand on improving, creating and sectioning off new and existing habitats from public. Expand on sustainability ideas for buildings and shared spaces (green roofs, larger verges and green zones).</p>
Richard Huskinson				<p>am writing to object to you attempting to build houses on our local meadow, again. It won't be long until we have no green space left. The meadow is teeming with beautiful wildlife, and to build on there, would just be wrong.</p>
Richard Huskinson				<p>I am writing to object to you attempting to build houses on our local meadow, again. It won't be long until we have no green space left. The meadow is teeming with beautiful wildlife, and to build on there, would just be wrong.</p>
Robert Teate				<p>The Council like all other areas seems obsessed with money. You have an opportunity to stop the building before it becomes another Ingelby Barwick. The private land can only be built on, with council consent and can be changed if needed.</p> <p>Why not rewild the rest of the site and promote eco tourism. That would be the future, not building more houses and roads.</p> <p>Have we not learned anything from this pandemic. People need breathing spaces for their health and mental wellbeing and with global warming we need fauna and flora not brick and tarmac.</p> <p>We must take the carbon output from such developments into consideration in the event of climate change and councils need to start leading the way. Not contributing to an already over populated area with yet more building and vehicles.</p>

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Sam Matthew man				<p>An ecological survey should be undertaken of the site and the greenbelt space, as I do not believe it has been done already. Also, as habitats where protected native species are living will be destroyed, how are they being protected or relocated? There are strict rules by the Government to not damage Shrew and Great Crested Newt habitats for example. The results should be published to the public and whatever the cost, limiting the effect on wildlife should be too priority.</p> <p>I would like to see the roads through the development all reduced to 20 mph. If it is a country park then cars should be minimised as much as possible. Also, speed management device, such as speed cameras, speed humps in the road and island break points should be included in the plans. Maybe cycle lanes built into all roads?</p>
Sarah currey				<p>The road should not go through mandale meadow. Once there is a road its only a matter of time before houses follow. Also the road will bring pollution, congestion and more traffic noise into an area already suffering. The country park is not necessary as it is already an area of natural beauty that I'm not convinced a developer can improve on and we all know that developers don't keep their promises so no doubt will fall by the wayside to more houses. So we'll end up with a road at the end of the garden and the outside space we use daily ruined.</p>
Sarah Henderson				<p>I am writing to lodge my objection to the stainsby country park, in particular to the road that is going to be built across existing green areas.</p> <p>Many of us in this area objecting to the building of houses on mandate meadow for exactly the same reasons we object to this. We have bought houses in this area for a reason, in our case to bring up our daughter surrounded by nature and green space that she can access in safety, a road along those routes would damage the habitat of many animals, make walks we usually take unsafe due to cars, increase traffic in the area (to no advantage to the current residents in terms of access).</p> <p>Also the plans stated grossly utilise existing green land to claim as a 'new' country park. The developers are simply using current green land to claim as their own (such as the marsh land and woods) and destroying parts of it for their own gain (as we are well aware access limits the amount of houses they can build). Meaning the public are quite simply being misled. It feels simply like you have given with one hand (protecting mandale meadow) and taken with another. That entire green land is one of the few that remains in the area.</p> <p>On top of this the amount of traffic it would bring to mandate roundabout (an already busy area with lots of accidents) would cause more problems to residents.</p>
Scott Elliot				<p>In general, the Country Park seems a wonderful scheme, but can it be guaranteed to actually happen? Similar things were proposed at Coulby Newham and Whitney Banks developments but have fallen a long way short of expectations and promises.</p>
Scott Pearson				<p>We do not want this road!!!!</p>

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Simon Sharp	Sutcliffe Play			<p>In the first instance may I add my support for the proposed new country park in Middlesbrough and wish all the stakeholders well during the consultation and beyond.</p> <p>By way of an introduction, my name is Simon Sharp and I work as an Area Sales Manager for Sutcliffe Play Ltd (www.sutcliffeplay.co.uk), an Employee-Owned, UK Manufacturer of play equipment based in West Yorkshire. We work with the majority of local authorities in the UK designing, manufacturing and installing play equipment and have decades of experience improving the lives of young people through play.</p> <p>The development of Stainsby Country Park is an impressive undertaking and will transform the lives of many and can only be a good thing. To this end, if I can help in any way, either by providing designs (free of charge) or quotations to help visualise the Park's play provision, please don't hesitate to contact me.</p> <p>Thank you for your time and I'd be thrilled to hear from you.</p>
Sofia Torre				<p>I know its going to happen because you don't know the meaning of consultation. So do what the hell you want. Other areas would have been better Port Clarence, by the transporter bridge those areas could have done better. But again its giving those who have a better environment to live in and those who don't just pollution to swallow</p>
Sonia Bowler				<p>I consider myself a local resident to the area that will be affected by this plan but I did not receive the letter from Middlesbrough Council that I was expecting nor did many residents at my end of Chalford Oaks. It is only through the hard work of local groups and volunteers that I found out about the full extent of the consultation and how to comment on it.</p> <p>The area of Mandale Road from the Coronation pub to the A19 already gets very congested and makes it difficult to access and exit our small estate at key times of the day. The intention to build this new road through Mandale Meadow and exit onto this already busy road will only make the situation worse.</p> <p>Having read all the documents, I fail to see how building a loop road that exits back onto Jack Simon Way and will have access from the roundabout there onto the A174 will cause any more problems than exiting the road through our meadow onto our much smaller roundabout.</p> <p>In addition a loop road system will avoid the potential flood zone of Bluebell Beck and also the no-build area of the 2 underground sewer pipes.</p> <p>In conclusion, I still say no road through Mandale Meadow.</p>
Stephanie Atkin				<p>Is this consultation genuine when we are told that no viable alternatives exist? At best, this brochure is a waste of public money. At its worst, it is nothing short of ecocide.</p> <p>No road through Mandale Meadow.</p>
Stephen Connors				<p>Middlesbrough council strives for a greener community yet destroys the green areas to gain more council tax funding.</p>

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Stephen Lee				I strongly object to the development plans for South Middlesbrough, these objections are based on the fact that we have a raft of land towards and around the centre of the town. This land which was cleared to provide "student housing" the development of which is acknowledged as no longer viable should be prioritised for building private housing. My second objection is based on the council's assumption that there is actually the need for additional housing in the town. Given that the current trend is for people to move away from urban settings, employment opportunities are declining - currently much of the output from the chemical industry is contracting and service industries and retailers are moving online and further out of town development will have an adverse effect on wildlife and the quality of life for current residents Stephen Lee
Steven Brown				<p>I as a resident of Stainsby Hall Farm have had to endure major building work for the the last few years. Even through the pandemic,continuous noise and dust. As a resident I am supposed to just put up with this, same council tax payments. We have had numerous power cuts due to poor workmanship by the developers workforce and proposed outages to connect the development</p> <p>I believe that this development will have an adverse effect on the value of my property and will not be as appealing for future sale. The residents living on the East side of the development are less effected due to the beck and existing tree line and retention of Saffron Wood, but all the green sections are planned on that side.</p> <p>Could we have some sort of tree line separation ? Privacy has been given to us in the form of a wooden fence ?</p>

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Name	Organisation (where applicable)	Agent	Organisation	Please provide any additional comments you may have regards the draft development guidance.
Steven Jackson				<p>Initially I would like to say how unfair I feel MBC have made this consultation. You have excluded a whole swathe of the population who are not computer literate (my wife has had to do this on my behalf as I am not used to using the computer, but how many others will not have anyone to assist them?!), and the documents are so lengthy that they are off putting – you need a qualification in planning to fully understand (I have therefore refrained from completing the consultation section by section and decided to express my concerns on this page). Furthermore, the information circulated by MBC to residents and posted in the community were difficult to understand and certainly gave no indication that a proposed road is to run through Mandale meadow, so how are people meant to make an informed decision if they are unable to clearly understand the plan? A Country Park sound lovely and inviting but how many people will realise that part of the process is in fact, destroying a beautiful, established piece of the natural world which is already enjoyed by wildlife and humans alike? Any suggestions the council and planners have made to turn this meadow into a 'Country Park' are not going to enhance the meadow, but totally destroy the tranquillity and beauty of this naturally beautiful meadow. I do not believe, especially due to lockdown, that you have allowed sufficient time and methods to complete a comprehensive consultation, which is extremely disappointing. I also think that we are building far too many houses on green spaces and that, if you are a forward thinking council who truly believes in One Planet Living then we should be looking at utilising the many brownfield sites, and challenging builders (and government) when they want to develop on our green spaces. I am totally heartbroken by the amount of green spaces Middlesbrough has lost recently and it looks like the only green spaces that will be left in Middlesbrough will be Ormesby Hall, Albert Park and Stewarts Park. TOTALLY HEARTBREAKING and I cannot reconcile this with the Council's green strategy and One Planet Living ethos! As a council, you should be protecting the interests of the people of Middlesbrough and green spaces should be high on the list of priorities, in my opinion, especially given the lack of green spaces for people to access in the recent lockdowns. I believe that as an area, Middlesbrough is already far too densely populated and rather than building more houses, we should be looking at re-greening areas. Again, as a progressive council, this is something I, as a resident of Middlesbrough, would expect you to be looking at, instead of building more houses. There is another question to be answered here too - are the houses really needed? Middlesbrough's population has not grown substantially yet the housebuilding in Middlesbrough has grown exponentially, so much so that builders are having to offer incentives / reduce house prices, to encourage buyers. In future, can exercises to ensure all new house/office/retail park building is necessary be undertaken, especially with changing working methods due to the current pandemic? Also, due to the changing work habits of people, is the road really necessary? Surely there will be less domestic traffic on the road if more people are making the permanent move to predominantly work from home? Our population density has been highlighted during the Covid pandemic, where medical professionals have sited this as one of the reasons our Covid figures remains one of the highest in the country.</p> <p>My wife and I moved to Linthorpe over 20 years ago because it was so green - unfortunately within this time, that 'greenness', the reason for us buying in Linthorpe, has gradually been eroded with what seems like every available green space being developed – flats have even been built on what had previously been someone's garden on Thornfield Road, despite Linthorpe apparently being a 'conservation area'! I am a keen gardener and do what I can to encourage wildlife into the garden but I have noticed less birds, especially the smaller ones such as blue tits, and less flying insects including butterflies and bees in recent years: signs of the decline in the local ecosystem, indicating just how much green space we are losing year on year, which is why the preservation of Mandale meadow is so much more important than installing yet another road, that will ultimately become as congested as any other road and therefore, nothing will have been achieved, other than the destruction of a precious green space.</p> <p>I appreciate the illustrations of the Country Park look idyllic but the reality as we know, will be nothing like this. For starters there will be heavy machinery during the installation of the road; this itself will ruin the existing established meadow and as we know, the meadow is home to a number of red and orange endangered species including marsh and bee orchids, sparrow hawk, common blue butterfly, elephant hawkmoth, woodpecker, emperor dragonfly, foxes, hedgehogs, Roe deer, water voles, newts, frogs, toads and no doubt many more - this ecosystem should be cherished and nurtured rather than allowing a road to destroy it amazing diversity. The meadow is also host to pollen rich grasses and native wildflowers suited to the clay soil and are exactly what bees and other pollinating insects need. There has already been an impact on the deer's at the meadow due to the installation of the traffic lights at Mandale roundabout - a baby deer recently got separated from its mother, again, heart-breaking and totally unnecessary, and shows just how little concern contractors have for the natural world around them (I am not blaming them, they are there to do a job but the fact remains that there is a huge negative impact on the natural world when construction work is undertaken). How can MBC justify this ecocide? I despair at some of the language used in the document to describe the plan: 'To create a diverse ecosystem' – a diverse ecosystem takes decades to establish and there is no need to create one as one already exists which the council are planning on destroying. This statement is unbelievable!</p> <p>How does this align with the council's One Plant Living and green strategies? Why promote these when you are prepared to destroy this beautiful meadow, with mature trees and an abundance of diverse wildlife, flora and fauna? Surely your One Plant Living and green strategy can incorporate and protect this precious piece of meadow and woodland by reinstating the Deed of Covenant which bequeathed this land to the people of the town in perpetuity? In fact, what happened to this? How were the council able to remove it, especially without informing the people of the town that it was going to happen?</p> <p>The 'relief road' will just become another road and traffic will be as heavy here as it is on every other road in Middlesbrough – so just how realistic is the 'country park' if it has HGVs, cars, vans, motorbikes charging through the centre of it?! You can dress it up any way you want, the reality is that this will become a main road and will result in an increase in pollutants, adding to the poor health of Middlesbrough's residents living within the vicinity of the A19/A66. And how do we expect wildlife and a healthy eco-system to be developed when there will be so much traffic and pollution? The plan just isn't realistic and I respectfully request that MBC finds another</p>

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				<p>route for this road and protects this remaining meadow for the future of the residents of Middlesbrough and our precious, and ever declining, eco system.</p> <p>If the 'country park' does go ahead again there are logistical questions to be answered, such as who is going to be responsible and pay for the upkeep this new country park, what standard is the park going to be maintained to, what guarantees will residents be given that further house building will not go ahead at a later date? Surely there will be a cost to the council to main the 'country park', which will no doubt fall by the wayside as council budgets continue to be cut by central government and this will not be a priority. No doubt eventually houses will replace any green space, sadly. It therefore makes sense to leave it as it is and allow the community who currently manage the meadow so brilliantly to continue to do just that?</p> <p>People are already complaining about the levels of noise that they have been disturbed by and this is just the installation of traffic lights at Monday roundabout.</p> <p>People already complain about the levels of noise that they have been disturbed by and this is just the installation of traffic lights at Mandela roundabout can you imagine what it's going to be like when there is heavy traffic running to the side of the rear of the property? How would you feel if this was being planned for the rear of your property? Local residents do NOT want this road – please please listen to what the people who already live this area want – they are the ones who will have to live with the consequences! However, my concerns come from any planned development on this beautiful, diverse and very much needed green space. Please MBC, stop this relief road, preserve our green spaces for us and generations to come.</p>
Steven larkman				The proposed spine road will lead to the destruction of treescapes that I've loved since I was a12 year old and enjoy weekly to this day I would have to see this happen.Don't do it!
Stewart Jones				<p>Can we please have a rewind and conduct the correct way of approaching planning proposals.</p> <p>The local ward which Mandale is located was not part of the community involvement process in 2013</p> <p>None of the consultees were told about the protected land status and the registered local wild life site</p>

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Stewart Jones				<p>To facilitate the development of the SWWE, the Council owned public open space required for the road corridor will need to be appropriated under S122 (1) of the Local government Act 1972. As such this will firstly require advertisement under S122 (2A) of the Local Government Act 1972 and then consideration of objections received prior to any appropriation being implemented by the Council. As the Council will not be looking to dispose of any land, the Public Open Space Disposal process under S123 of the Local Government Act 1972 will not be required.</p> <p>Can you confirm if this is correct To enable the Council to demonstrate that the decision has been made properly and in accordance with Wednesbury reasonableness, the proposal to appropriate must be the subject of a comprehensive report and decision record or minute. The minutes should show that the land is not currently appropriated for planning purposes and that the Council intends to formally appropriate the land under s.122 LGA 1972 or that the land is currently held for planning purposes and that the Council intends to appropriate the land for alternative planning purposes. If s.122 LGA 1972 is being utilised the minutes must record the resolution that the land is no longer required for the purpose for which it is currently appropriated.</p> <p>Advertisements in accordance with s.122(2A) or s.232(4) in respect of open space must give clear information and refer to the intended appropriation, the Council's minutes must then record that any objections received have been duly considered.</p> <p>Appropriations of land are an executive function under the Local Government Act 2000 and so any decision to appropriate land would need to taken by the Cabinet.</p> <p>I know there has been no SA completed for this area and as it contains a local wildlife site and is a habitat of the nationally protected water vole plus it being a site of interest and nature conservation.</p> <p>Will the council publish a comprehensive report for public scrutiny before the process of appropriation is started</p>

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Sunny ALi	Highways England			<p>The summary and conclusions offer the key findings of the review (as set out below),</p> <ul style="list-style-type: none"> • The Local Plan clearly sets out the development ambition for the site and the transport measures needed to support the development, particularly with a view to the need to consider the SRN. • The scale of housing being sought, with a view to the 1,670 dwelling allocation in the Local Plan, should be confirmed and assessed appropriately. • The sustainable transport provisions and the supporting land uses provide the opportunity to enhance sustainable movement. • The physical relationship between the site and the SRN will need exploration to ensure the principle of any element of the development. • In relation to the Stainton Way Western Extension (SWWE), further work and evidence needs to be developed to quantify impacts. While Highways England can provide an uninformed view in relation to the various options, a fully informed view would need to be established through appropriate transport modelling. At this stage, we do not consider that Highways England is in the position to suggest a preferred option from a SRN perspective. • In relation to any other transport interventions that may be required, appropriate transport modelling will need to give full consideration to the wider impacts at the SRN. • In regard timescales of the various elements of the development and (i) considering the impacts of the various stages and (ii) establishing sustainable movements form day one, further information in relation to the intended delivery programme would be welcome. <p>SEE ATTACHED FILE</p>

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The Countryside Charity North Yorkshire CPRENY	CPRENY			<p>Comment</p> <p>The North Yorkshire county branch of CPRE The Countryside Charity ('CPRENY') welcomes the opportunity to provide Middlesbrough Council ('MC' / 'The Council') with comments in response to the public consultation on the Stainsby Country Park and Masterplan. The document has been published on behalf of the Council. Whilst the site is located currently outside of CPRENY jurisdiction, a number of public requests have resulted in the county branch getting involved and providing this representation.</p> <p>It is understood that this consultation is the latest in a series of consultations on this site. Furthermore, this is not a planning application, but a masterplan produced by the Council on behalf of several landowners including themselves, with the intention of retaining 'control' of the development and ongoing management of the proposed greenspace and country park.</p> <p>Planning Context</p> <p>The National Planning Policy Framework ('NPPF') was updated on 19 February 2019 and sets out the government's planning policies for England and how these are expected to be applied. The NPPF is, therefore, a material consideration which should be taken into account when plan-making and determining applications.</p> <p>The planning system should contribute to achieving sustainable development. The NPPF aims to deliver sustainable development through the implementation of its policies. For plan-making this means that plans should positively seek opportunities to meet the development needs of their area and be sufficiently flexible to adapt to rapid change. Succinct and up to date plans should encourage a positive vision for the future of each area including housing and economic needs alongside social and environmental priorities. Plan-making is also about providing a 'platform for local people to shape their surroundings' (para. 15, NPPF).</p> <p>Regulation 10A of the Town and Country Planning (Local Planning) (England) Regulations 2012 places a legal requirement on Local Planning Authorities to review Local Plans at least every five years. Paragraph 33 of the NPPF sets out that 'reviews should be completed no later than five years from the adoption date of a plan and should take into account changing circumstances affecting the area, or any relevant changes in national policy.'</p> <p>It is understood that the Council are in the process of preparing a new Local Plan which will replace a number of existing adopted planning policy documents. The Local Plan was due to be submitted to the Secretary of State for examination in the summer of 2019, however, following the decision of the full Council in July 2019, the planning authority were directed to re-start the process of preparing a new Local Plan. The preferred options for which were due to be published for consultation in January 2012, however, there appears to be some slippage. As a result of the fact that the Council are at an early stage in plan preparation, no weight can be attributed to the new Local Plan in the planning process in accordance with guidance set out in the NPPF. Currently, therefore, the relevant Development Plan in force for Middlesbrough Council consists of a number of adopted documents, including:</p> <ul style="list-style-type: none"> • Housing Local Plan (adopted 2014); • Core Strategy Development Plan Document (DPD) (adopted 2008); • Regeneration DPD (adopted 2009); • Tees Valley Minerals and Waste Core Strategy DPD (adopted 2011); • Tees Valley Minerals and Waste Policies and Sites DPD (adopted 2011); • Middlesbrough Local Plan 1999 (saved policies) • Proposals Map. <p>The 130Ha site at Stainsby Hall Farm and Stainsby Hill Farm subject to this masterplan consultation was allocated for a mixed development of 1670 dwellings, employment land, local retail centre and a primary school, within the 2014 Housing Local Plan (Policy H21 - Brookfield) and is shown on the relevant policies map. The adopted policy sets a number of criteria which must be achieved in order for any proposed developments on the site to be considered appropriate including (amongst other matters): the provision of a link road through the development; provision of affordable housing and off-site contributions; a mix of dwelling types and sizes; pedestrian and cycle links through the development linking with existing residential areas adjacent to the site; a country park; landscape screening of the A19; appropriate green corridors adjacent to existing Saffwood Beck and Blue Bell Beck; and, enhancement of the existing Local Wildlife Site to the north of the site.</p> <p>It is further acknowledged that the site was retained in the publication version of the Local Plan which was due to be submitted to Government in 2019. As such, and given the Council are promoting this consultation, it is fully expected that the site allocation will be carried forward to a forthcoming version of the preferred option Local Plan due to be published for consultation in the future.</p>

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				<p>Considering recent events (the Covid-19 pandemic) and the changing emphasis of national government in respect of Climate Change, CPRENY fundamentally object to the principle of continuing the allocation of such a large green field location. Further, it is noted that despite it having been allocated in the 2014 Local Plan and withdrawn Publication Local Plan (October 2018) which was consulted on between 9 November and 21 December 2018 (having already been through the preferred options stage), many members of the public and local community interest groups are voicing their concerns about development at this proposed location, one of the few remaining areas of 'major open space' as shown on the local development framework core strategy key diagram. However, this entire area is shown as 'allocated' on the 2014 Housing Local Plan Allocations Plan and it is acknowledged that some of this area has been built out.</p> <p>The Council are continuing in their work towards a new Local Plan and published in January 2021 their most up to date Local Housing Needs Assessment, prepared on their behalf by Opinion Research Services ('ORS'), stating a 7,200 dwellings (400 per annum) local housing need. However, there is no mention within this document of the impact on housing need resulting from the Covid-19 pandemic or indeed changing live/work lifestyles as a result of the pandemic, which broadly supports the figure allocated in the Housing Local Plan ('HLP').</p> <p>Furthermore, the Council's most recent 'five year housing land supply assessment 2020-2025' published April 2020, highlights that the Council has a 9.99 year housing land supply across the district. Helpfully, this assessment sets out that a number of allocated sites for residential development in the adopted HLP, did not have planning permission on 1 April 2020. The NPPF requires that these sites should only be assessed as deliverable where there is clear evidence that housing will be delivered within five years. Whilst the Council own some of the land within the area subject to this response, other parts of the wider site are in the control of different landowners and the Council are still come way off receiving formal planning applications. As such, there is no clear evidence that this site will be delivered within five years, therefore, it was correctly excluded by the Council's policy officers in their assessment of whether the sites will be deliverable in the next five-year period. The Council has thus projected a deliverable supply from other allocated sites as being 590 dwellings. When that figure is considered in the round with other available sources of housing supply, the Council projects a total supply of 2,686 dwellings over the next five-year period. This exceeds the minimum housing requirement of 1,344 dwellings by 1,342 dwellings. Whilst the assessment acknowledges that there has been a slower than usual rate of construction because of COVID-19, it also does not take account of any revisions to projected housing numbers which may occur as a result of the pandemic, which may still be unknown.</p> <p>Given the likely impact the pandemic has had, CPRENY are of the opinion that whilst the Council are at such an early stage of plan-making, now is the time to commission an addendum to the ORS report to fully address this matter, to ensure that the correct level of housing is planned for which may prove that not all proposed green-space is required. Simply because an allocation was made in 2014, is not sufficient justification to continue with it, providing the evidence suggests otherwise. As set out above, the NPPF expects Local Plans to be reviewed regularly and be updated to reflect up to date evidence and be able to respond to rapid change. It would be perfectly reasonable for the Council to effectively 'de-allocate' the site in the emerging Local Plan should the Council have the appropriate evidence to justify this route.</p> <p>It is understood that the HLP was assessed via independent examination and the Plan found sound at that time, however, since 2014 the NPPF has been revised, the Government (and MC) have declared a Climate Emergency and we are living through a global pandemic which is having a catastrophic impact on lives and livelihoods. As such, CPRENY would urge the Council to take a more considered approach to developing on such a substantial site and listen to local residents who fervently oppose this allocation as they value the open agricultural views afforded to them from Mandale Meadows and from Bluebell and Saffwood Becks.</p> <p>The pandemic has enabled residents to enjoy the countryside for its own sake and brought into sharp appreciation how valued and beneficial open views, fresh air and green space is in terms of impacts on both physical and mental health. The entire housing allocation is adjacent to land allocated as 'Green Wedge' under saved policy E2 of the 1999 Middlesbrough Local Plan. Policy E3, was also saved, and deals specifically with development adjoining green wedges. The supporting text to both policies sets out that the Cleveland Structure Plan identified green wedges at the Stainsby Beck Valley including the Bluebell Beck Valley extension and goes on to state that 'development adjoining Green Wedges can have a significant impact upon the perceived character of the Green Wedge.'</p> <p>Whilst it is recognised that the Masterplan seeks to retain existing green corridors and create an essence of 'country park living,' CPRENY believe that the proposed development as set out in the masterplan would totally alter the character of the green wedges at this location, removing the openness and long-range agricultural views entirely. Furthermore, CPRENY consider that the proposal is wholly unnecessary given the large supply of deliverable sites elsewhere in the Council's area including existing brownfield sites, which the district has in abundance (according to their Brownfield Register). We, therefore, urge the Council to support regeneration schemes at Middlehaven, Grove Hill and Newport. Favouring a brownfield first approach is entirely in line with national policy and would enable these areas to be redeveloped with a green, low-carbon emphasis – bringing the benefits of biodiversity into the urban environment and appealing aesthetics through</p>

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				<p>careful design. At CPRE, both nationally and locally, we recognise the urgent need for more affordable homes and argue that the way to do this is to make best use of brownfield land before even considering development on the greenfield sites. A CPRE poll of adults across the country shows that two-thirds of adults think that protecting and enhancing our green spaces should be a higher priority after lockdown. This shows just how much communities would suffer if these local patches of green are lost.</p> <p>The Council declared a climate emergency in 2019 and according to a statement by the Mayor on the MC website, the Council has ‘ambitious targets to be carbon neutral by 2029 as an organisation,’ and goes on to state that the ‘goal is for the whole town to be carbon neutral ten years later, well ahead of the government’s 2050 target.’ The Council is currently preparing a Green Strategy which will set out how they intend to achieve this. CPRENY would argue that the provision of such a large housing allocation on one of the only remaining large green field sites in the district will not help achieve this due to the sheer scale and landtake involved in the proposals. CPRE believe that we should be encouraging agricultural practises to tackle climate change to enable nature recovery, provide food needed to sustain the country and provide clean air.</p> <p>The Masterplan subject to this consultation sets out that the site is 140Ha, which is an increase of 10Ha over and above that allocated in the HLP. Further, the masterplan sets out the positioning of the new primary road through the site connecting new development at the south of the site through to the existing farm area to the north. It goes on to say a further connection will be created through an enhanced roundabout entrance at Mandale Road – passing over Blue Bell Beck. CPRENY has significant concerns that this will cause significant detrimental impacts to the existing local wildlife site at this location. Whilst compensatory measures are a criterion of Policy H21 of the HLP, the level of construction work which will occur because of the development of the wider site and the timescales involved will likely mean habitats will be disturbed and are unlikely to recover. CPRENY has recently seen evidence of newts on site and urge the Council to consider the implications for protected species.</p> <p>It is further understood that the HLP was partly based upon the March 2014 Infrastructure Delivery Plan, which assumed the delivery of housing allocations in Brookfield, Hemlington Grange, Coulby Newham, Stainton, Nunthorpe Grande and Low Gill. The MC five-year housing land supply document sets out that no housing is planned at Coulby Newham or Nunthorpe Grange in the next five years in the same way as at Stainsby. As such, the new road is not required to deliver development.</p> <p>The Council should carefully consider how they wish to develop Middlesbrough in line with their climate declaration and ambition to ‘protect green spaces’. Up to date evidence allows the Council to deliver new policies and not be bound by those made in the past. Therefore, CPRENY urge the Council to ensure that evidence in relation to exiting housing land supply, the current static population in Middlesbrough and the fact that the ONS population figures indicate a reduction over the next 10 years of circa 1000 people from the district, alongside the effects of the pandemic, is considered when plan-making. Of the downwards projection from the ONS on population figures, they also predict a reduction in the number of children and working age adults and increases in older populations. This alongside the fact that the pandemic has had a significant effect on the economy with more people working from home and unlikely to return to pre-COVID levels of commuting, will have a significant impact on traffic flows, particularly at peak times. The Council should therefore consider all these elements when creating a revised infrastructure delivery plan which should in turn impact the Local Plan. It is therefore the opinion of CPRENY that the Stainton Way Western Extension would not be critical to the towns infrastructure and Highways England would not be concerned about capacity of existing roads.</p> <p>Given the Government’s recent commitments to tackling climate change and the Council’s own recent commitments to the ‘Climate Emergency’ and ‘One Planet Living’ and evidence which has come to light since 2014, the Council has the evidence to justify a change in direction so should not fear legal challenge as a reason to not halt the development in these areas.</p> <p>Paying specific attention to the design principles set out in the Masterplan document, CPRENY, welcomes the Council’s attention to the inclusion of biodiversity and provision of pedestrian and cycle routes across the whole site. The document proposed that 53% of the total site be retained as green space which is encouraged. The inclusion of a specific landscape strategy as central to the masterplan is considered good planning. The provision of a country park and facilities, green connectivity and green fingers across the site, the creation of SuDs, recreational and circular routes, public art within the development, trim trails and dog walking areas, formal sport pitches and play areas, new woodland habitat, a green commercial and community hub, food growing areas including public orchards and quiet areas for solitude and wildlife-watching are all assets to be welcomed and encouraged. The summary of landscape strategy plan (pg 48) shows proposed locations for all potential facilities and ‘areas’, however, CPRENY are aware that this is not a planning application and has not been put forward by developers as a specific proposal so are concerned that much of this could be considered aspirational and unviable. It is hoped that MC would seek to ensure that these concepts are delivered in actuality across the whole site which is not entirely within Council control through the use of legal agreements etc.</p>

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				<p>CPRENY would support the Council in transferring the concept of this proposal to any of the brownfield sites within Middlesbrough as a general concept to ensure the green regeneration of urban derelict sites. In essence, if these principles were adopted as standard by MC across all developments within the district, CPRENY would be supporting of them. It is hoped that the emerging Local Plan will reflect this concept in its design policies. In the case of Stainsby, the site location is simply not appropriate or necessary for development in our opinion despite the landscape first concept.</p> <p>It is considered that the parking sections within section 6 and 7 of the 'Urban Strategy' chapters should include reference to the provision of electric vehicle charge points. Not all houses will have garages with suitable charge points, therefore, given the Government's commitment to electric vehicles it is considered that thought should be given to how owners should charge vehicles, especially in those areas where frontage parking is being discouraged and landscape and boundary treatments are being proposed at a detailed level.</p> <p>The Masterplan concentrates on landscape, open space and infrastructure provision for the site and thus presumably leaves the details of housing to any future developer bar giving an indication of the types of housing the Council would consider appropriate in broad locations, i.e. higher density towards the central areas of each neighbourhood zone. CPRENY were disappointed that an indication of what the Council would expect to see delivered, as a minimum, has not been included in the Masterplan in terms of providing net-zero / low-carbon dwellings, for example, through the use of sustainable building materials, or the provision of individual or district-wide renewable energy infrastructure. Whilst this is perhaps a detail which could be left to a planning application, given the amount of detail the Council has prepared on green elements, it is thought that this would support the Council's ambition of reaching net zero by 2039.</p> <p>Conclusion CPRENY welcomes the opportunity to provide MC with a written representation on the Stainsby Country Park and Masterplan consultation.</p> <p>CPRENY are of the opinion that given the Council's 9.99 year supply of available housing land supply alongside specific changes to policies and circumstances at both national and local levels, the allocated site in HLP Policy 21 is no longer required and the Council would be justified in de-allocating this site from the emerging Local Plan.</p> <p>Given the Council are in the process of preparing a new Local Plan and are reviewing the evidence base, CPRENY urge the Council to take on board the arguments set out above and consider the impacts of the global pandemic and future population forecasts from the ONS on their estimated housing need prior to the publication of their Preferred Options Local Plan. Alongside this, the Councils commitment to the Climate Emergency should be central to any planning decisions. This approach is entirely in conformity with paragraph 33 of the NPPF which instructs Local Plan reviews to 'take into account changing circumstances affecting the area, or any relevant changes in national policy'.</p> <p>Further, it is considered that these change in circumstances render the proposed Stainton Way Western Extension road unnecessary.</p> <p>CPRENY would, however, support the Council's landscape-first approach to the design of the development and would support them in transferring this principle to a brownfield-first objective ensuring that all derelict sites are transformed by green-regeneration schemes.</p> <p>CPRENY support the numerous residents who have contacted us expressing concerns about the proposals at this location in terms of imposing a detrimental impact on the character and openness of the agricultural landscape and loss of visual amenity at this location, especially considering the frequency of use of these areas in light of the recent pandemic and endorse their objections.</p> <p>CPRENY reserve the right to comment further should additional information be consulted upon in support of the development in this location.</p>
Tim Allen				<p>I'm not against it overall. Would also be interested on timescales for the start and eventual final completion</p>
Tony slater	MJBON/A			<p>To Summarise,whilst accepting the new estate is on private land and therefore unlikely to be affected by the concerns of objectors.The land controlled by the council can indeed be changed.The road changes at the Mandale area should therefore be given serious consideration regarding the likely increase of pollution.</p>

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Town Clerk	Thornaby Town Council			. It is TTC's considered opinion that the Masterplan should therefore be reviewed. It is dated, shortsighted and exposes any altruistic claims by MBC to preserve and protect the natural environment for current and future generations as wholly disingenuous.
Trevor King				This land was left to the people of Middlesbrough. A deed of covenant, left in perpetuity. This meadow should be left as a natural green space for the residents of the town forever! NO ROAD THROUGH MANDALE MEADOW/WOODS
Val Young				No road through the meadow. Green spaces are needed now more than ever. The meadow is needed for the residents of Middlesbrough's mental health & well being. The habitats of the animals birds & insects need to be preserved. The road would put the remaining meadow at greater risk of abuse by off road bikers, fly tippers & the travelling community.
Vanessa collett				The stainton way extension plan is about to decimate an area of green space that it used by the community. It's a habitat of flora and fauna, and in today's world of looking after the environment and climate to do this is reprehensible. Children use the land to play, in an era where they are encourage to get out more for their well-being. Adults use the area to run, walk and cycle, it's an area we've come to use The local plan should consider brown field sites of which there are many, everywhere you look there more and more housing developments all encroaching on to green belt. Many of these developments are beyond the means of a substantial number of the towns residents. Perhaps the council should wait for the ones currently under construction to be completed and properties sold before they start elsewhere. 2000 properties and your looking at anything up to 8000 car journeys acklam is slowly turning in to one big car park. I don't believe this development is warranted, nor will it happen as described and mandate meadow should remain as it is as was the benefactor who gifted it to the town wanted. An area where people can come together to engage in leisure activities have better mental and physical health.
Zoe Tootell				I have great concerns about the road that is being proposed. It will change where I live to a safe quiet area to a noisy unsafe environment for my children and pets. It will Also seriously affect my house price.

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	Story Homes	Chris Smith	Lichfields	<p>Introduction</p> <p>1.1 This response has been prepared by Lichfields on behalf of our client Story Homes and it responds to the latest draft Stainsby Landscape Masterplan Design Code (20th January 2021) (“Masterplan”) which has been prepared by Gradon Architecture.</p> <p>2.0 Context</p> <p>2.1 As the Council will be aware, Story Homes have an interest in the site along with Miller Homes and Avant Homes. Story Homes and the wider Developer Group responded to an earlier draft Masterplan and has subsequently remained in discussions with the Council. Story Homes and the wider Developer Group also attended a video call with the Council on Monday 9 November 2020 where the updated draft Masterplan was presented. Story Homes and the wider Developer Group have since been afforded an opportunity to review and provide further comments on the Masterplan.</p> <p>2.2 Story Homes (and the wider Developer Group) are committed to delivering a high quality new community at Stainsby and welcomes the Masterplan to guide development and in particular its design. However, as set out previously, it is considered that the Masterplan goes beyond the role of a design code and is unnecessarily prescriptive. Given the engagement on various occasions as described above to help develop the Masterplan, there is a feeling that the Council are not actively engaging with the feedback which is being provided.</p> <p>The extent of the changes since the last draft is disappointing as they are minimal and the comments and suggestions submitted during the previous consultation appear to be have been ignored. Story Homes (and the wider Developer Group) remain committed to working with the Council and the comments to the latest draft are set out in the sections below.</p> <p>2.4 The feedback mainly relates to the latter part of the Masterplan which sets out design principles for housing densities and typologies; road hierarchy; parking strategy; character areas; landscape strategy, including country park; and indicative sections and vignettes. The document is a Landscape Masterplan Design Code and many of these elements go beyond this remit.</p> <p>2.5 It is considered that the level of the detail and requirements generally exceeds the purpose of the document. We have not sought to respond to each page although the sections below highlight key concerns.</p> <p>Summary</p> <p>4.1 In the sections above, the key concerns with the Masterplan have been highlighted but there is a general view amongst the Story Homes (and the wider Developer Group) that the document does not need to include any of the information beyond page 48.</p> <p>4.2 Page 48 presents a plan with clear design principles akin to this stage of the development proposals. As the document is a Landscape Masterplan, this would be a logical stopping point.</p> <p>4.3 The detail on the remaining pages is generally unnecessarily prescriptive without justification. Furthermore, it tends to be either indicative or caveated by the fact that the design will be finalised in consultation with the relevant department within the Council – which renders the detail unnecessary.</p> <p>4.4 Story Homes (and the wider Developer Group) trusts that the comments will be taken into account. It is evident that the Masterplan requires further attention and Story Homes remains committed to working with the Council on further iterations.</p>
	ML Retail Ltd	Matthew Sobic	Savills UK LTD	<p>Introduction</p> <p>We write in relation to the Stainsby Masterplan Consultation on behalf of our client ML Retail Development Limited.</p> <p>ML Retail Development Limited is the owner of a site located on Low Lane adjacent to the current Stainsby Masterplan area (‘the site’). The site is subject to a current application for retail development that is being considered by the Local Planning Authority under Reference 20/0510/OUT. The site boundary is shown on Plan Ref. 16-1170 – ‘Site Location Plan’.</p> <p>As part of pre-application discussions and recent discussions on the application, the Local Planning Authority has encouraged the applicant to submit representations on emerging policy documents.</p> <p>The representation is submitted to confirm that it would be appropriate to include the site within the wider Stainsby Masterplan as an opportunity to provide retail facilities that would serve both Stainsby and the wider residential areas that form part of Acklam, being the overall residential community in this part of Middlesborough that will be formed when the Stainsby allocation is developed. That area will be the area that retail facilities at the site would serve.</p>

Please provide any additional comments you may have regards the draft development guidance				
Name	Organisation (where applicable)	Agent	Organisation	Please provide any additional comments you may have regards the draft development guidance.
				<p>Background The site measures circa 1.76ha and is bound by the A1044. The extent of the application site is illustrated on enclosed Plan Ref. 16-1170 – ‘Site Location Plan’.</p> <p>The site at present comprises vacant overgrown land, entirely enclosed by the local highway network. The site is located within the western area of Middlesbrough. To the north of the site lies the existing residential areas of Acklam and Trimdon.</p> <p>The site is located to the east of a planned new residential neighbourhood allocation known in the Middlesbrough Housing Local Plan Housing Core Strategy and Housing Development Plan Document (‘HLP’) (adopted November 2014) as ‘the Brookfield allocation’ and is the area broadly subject to the Masterplan allocation. The allocation seeks to deliver over 1,670 dwellings.</p> <p>The planned residential development will be accessed from both the B1380 and A1130 creating a single carriageway link road through the development, known as the Stainton Way Westward Extension (‘SWWE’). The southern end of this distributor road connects to the application site. It follows that the site is already well connected to existing residential areas but with the completion of the SWWE, the site will also be well connected to the northern part of the development at the emerging Stainsby allocation.</p> <p>The Masterplan The Vision included at Section 1.3 of the Masterplan confirms that: ‘The vision for the allocated housing site at Stainsby is a sustainable, mixed and balanced community set within an outstanding and engaging landscape, that includes the creation of a new country park, providing over 1500 new homes. This iteration of the masterplan seeks to flesh these out and provide greater detail and clarity around these themes.’</p> <p>It is important to note that the Masterplan area for Stainsby does not include all of the land either within the Brookfield allocation as defined by the HLP. The Masterplan covers the section outlined in red in Figure 1 below, and not the residential development that has been delivered at the south east section of the defined Brookfield allocation, which has provided 343 homes under Permission Reference M/FP/0572/11/P. A ‘Central Hub’ is proposed within the Masterplan. Page 55 provides details and confirms that: ‘As proposed in Section 4.13 Country Park, a ‘Central Hub’ will be a highly desirable attribute to support the achievement of Country Park Status. The site as designated sits at the centre of the proposed masterplan, providing the most accessible location for this bridge and gateway between the Landscape and the Urban.</p> <p>This community focused area will seek to meet the requirements for the Country Park and any other commercial and educational needs for the site. These expanded needs will be subject to further development with Middlesbrough Council following detailed design assessments.</p> <p>An aspirational centre is therefore a high requirement focusing on naturalistic materials in the context of a country park, defining spaces that focus on the surrounding park and enhancing the setting. The adjacent precedents give a taste of high quality design and interfaces between indoor and outdoor spaces.’</p> <p>Representations Overall our client supports the principle of the Masterplan as a document to support with the delivery of significant residential development within Middlesbrough.</p> <p>However, and given the significance of the development and the levels of new homes that it seeks to deliver, it is considered that the Masterplan does not tie in with the wider community that will be created by the development. That point is very much established by the absence of the existing development that has already been delivered at the allocation – a substantial 343 homes.</p> <p>Further, the Masterplan does not sufficiently acknowledge how the Stainsby development forms part of the wider established and existing community in this western part of Middlesbrough and primarily the existing residential areas of Acklam, which fall within the Trimdon, Kader and Acklam Wards. It follows that it is considered the Masterplan needs to look beyond the current redline to establish the overall community that will be created by the housing-led development that will be delivered at Stainsby.</p>

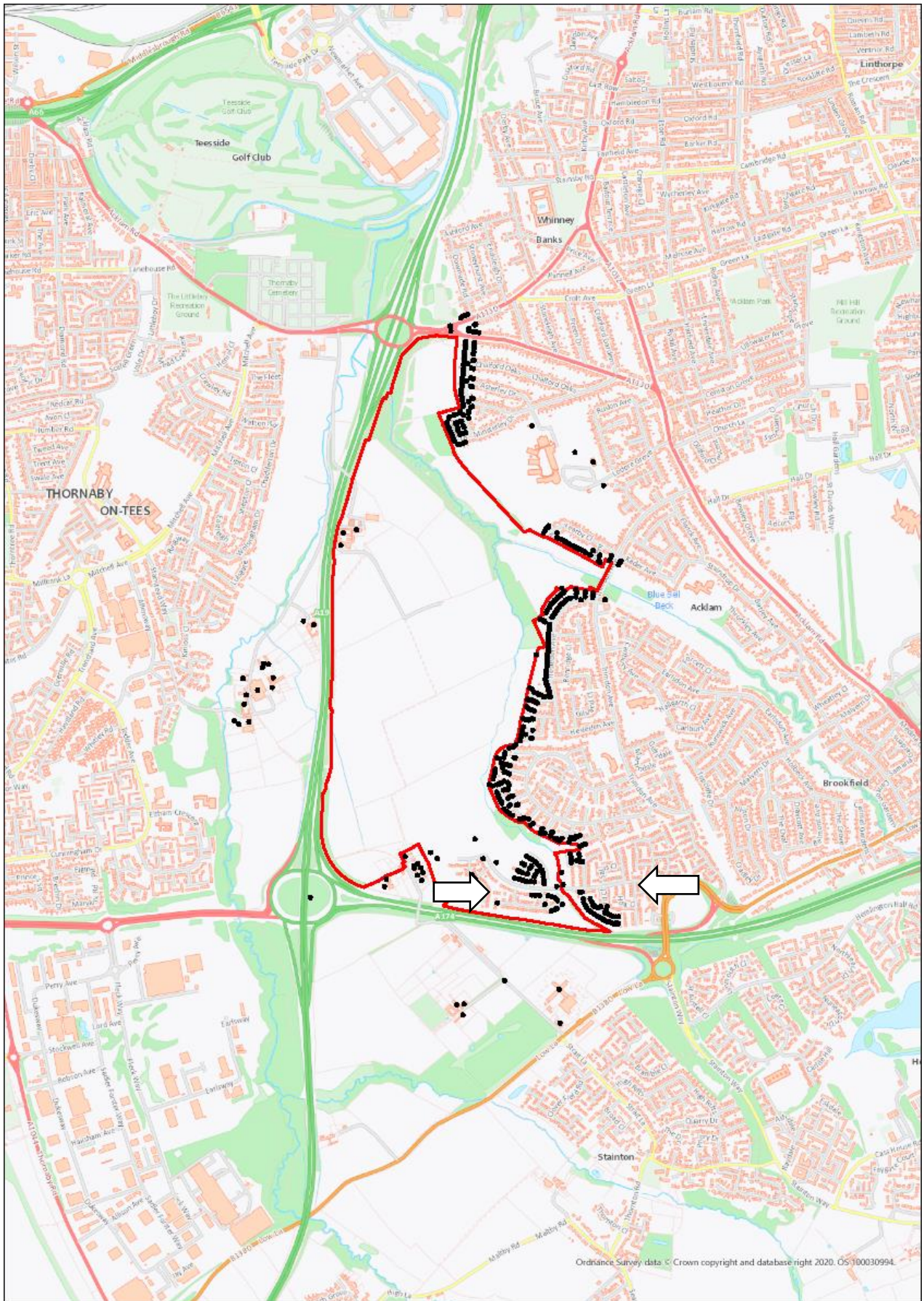
Please provide any additional comments you may have regards the draft development guidance				
Name	Organisation (where applicable)	Agent	Organisation	Please provide any additional comments you may have regards the draft development guidance.
				<p>Part of this includes the role that the site will play in providing retail facilities that will serve both the existing and established community in the Stainsby and Acklam areas and those further areas planned.</p> <p>As the Council is aware, there is significant market demand from occupiers who want to operate from the site in order to meet a provision for retail facilities that will serve the Stainsby and Acklam residential community in this western area of Middlesborough. Such provision will form a natural central point of the residential community at Stainsby and Acklam and is located in an area to ensure both commercial success and residents' needs can be met.</p> <p>The Trimdon, Acklam and Kader Wards that the proposed retail facilities at the site will serve are set out in the Council's Ward Profiles from 2015 to have a total residential population of 15,382 people, which amounts to approximately 11% of the total population of Middlesborough (forecast by the Council to be 138,412 at 2015). That residential population will only expand substantially with the delivery of approximately 1,670 dwellings as part of the development in Stainsby.</p> <p>The Council will be aware that there is no convenience retail provision of note in this area resulting in unsustainable shopping patterns arising as a consequence of an absence of provision. The site's development will address that significant deficiency in provision and ensure that sustainable shopping patterns can be delivered in this part of Middlesborough</p> <p>We understand that the reason that Stainsby has been selected for significant development, is that it will form a sustainable urban extension to the existing urban pattern of development to the east of it. It follows that the 4</p> <p>designation of the site for the provision of retail facilities that will serve the residential population in the three Wards identified above, which already comprise approximately 11% of the total population of Middlesborough, but do not have any material convenience goods retail provision serving that area, should be an important requirement of the Masterplan in ensuring that both the development at Stainsby is sustainable, but also the residential community that it will form part of in this part of Middlesborough is also a sustainable location.</p> <p>We note that a Local Centre is proposed as part of the Stainsby Masterplan. Our client's proposal is not that the development of retail facilities at its site would be at the expense of the Local Centre in the Masterplan, for the reasons that similar facilities in the existing residential areas in the above Wards – e.g. the facilities at The Oval, the parade of retail facilities on Trimdon Avenue, the retail facilities on Acklam Road. It is very much the case that the provision at our client's site will form part of a provision that will serve the residential community in the three Wards mentioned above and can therefore be complementary to the provision in this western section of Middlesborough where there is a significant deficiency in provision of the type of development that is proposed at the site.</p> <p>Conclusion</p> <p>It follows that to ensure the development at land on Stainsby connects well with the existing and established residential community in this part of Middlesborough and is therefore sustainable, the redline of the Masterplan should be expanded to include both the existing residential development that has been delivered at the allocation, and our client's site to ensure that a truly sustainable development is delivered, acknowledging the residential community that the site will form part of in this western section of Middlesborough and the provision that the development of retail facilities at the site will support in terms of serving the overall residential community and in turn supporting the delivery of a sustainable development and Masterplan for both the Stainsby part of this residential community and the overall wider residential community.</p> <p>Accordingly, our client's site should be designated as providing retail facilities that will support the sustainable delivery of the development on land at Stainsby and ensuring that that development can form part of the wider residential community that is already established and located in this part of Middlesborough. Adopting the approach outlined above now will ensure that when the Local Plan is reviewed, it can be concluded that it is sound as it will be positively prepared, justified, effective and consistent with national policy. There are no sound reasons to not support the amendments suggested as part of this representation given the sustainable benefits that arise in terms of delivering facilities that will serve the residential community that currently has an absence of provision of such facilities resulting in unsustainable shopping patterns.</p> <p>We trust that the redline will be amended on the Masterplan to be expanded to include our client's site and that the site will be designated for retail facilities in order to meet the requirements of the residential community and market demand of occupiers who are committed to serving those residential communities.</p>

Please provide any additional comments you may have regards the draft development guidance				
Name	Organisation (where applicable)	Agent	Organisation	Please provide any additional comments you may have regards the draft development guidance.
Dave Blackburn				This is just more of the same old council. Dogma. Residents are not listened to I was told things would change with the new mayor since before 1989 we have had the freedom to wander where we liked on our fields, but when this is finished MBC are seeing to it so that these freedoms are gone. I do not want more brick and concrete. Do not damage our field. This is going to cause a serious lack of privacy as the thousands of new residents will want to share out fields. MBC will routine the character of our lovely neighbourhood the reason people want to iive here.
Doris Hammond				That road will spoil acklams piece and quiet. The notice levels worry me if you pull old trees down to do this, it will make things worse for the resident. My heart bleeds when I think of the other residents especially those near mandale. No shield from road. with thousands of extra people and all their cars I feel there is going to be pollution problems. why do we have to have all that congestion when you have a cheaper alternative.
Gill Sullivan				Middlesbrough Council masterplan from 2014 needs a major overhaul, not tweeks for the following reasons. 1. In 2018 the north east required building figures fell as the government altered how housing was calculated. North East has dropped by 19%. 2. In 2019 thre was a climate change emergency declared. 3. In 2020 could hit us feilds over wed now. 4. In 2014 failed their residents in their duty to inform the public of massive changes to middesbroughs infrastructure and housing numbers. 5. As i understand this middlesbrough council have not carried out the required eco study, unfortunatley 750 jobs the council were hoping to bring to middlesbrough have fallen through so 750 homes are not needed. 6. there is a conflict of interest: Middlesbrough councils duties towards its residentswith their need for housing and hard cash. 7. MBC can gaurantee very little around this massive stainby build. The only control they have is...limit by design and limit by infrastructure. 8. by what laws exctly can MBC push through a road at this speed, through open pubic green space when 900 residents said no building. they clearly didnt want a road either. Road have to be built. 9. MBC have made such a mess of the consultation since 2014. MBC set the tone by both failing in their duty to inform and by their actions to obstruct deflect and generally make it difficult to get relevant and public information out to the public. 10. MBC intend to railroad their residents and do whats in the best interest of both themselves and the developers.

Appendix 1 – Stainsby Masterplan Correspondence

Letter/Emails sent	Number
Adjacent neighbouring properties - Letters	731
Local Plan Database - Letters	812
Local Plan Database - Email	603
Local Plan Database Agents - Email	38
Ward Councillors -Email	48

Adjacent neighbouring properties were determined using the same method as for a major planning application. The attached plan, shown by dots annotated to the map below, indicate the residents that have been sent a letter as part of the consultation. Further to this there are some addresses, primarily new-build homes which have not yet been added to the Gazetteer that, although occupied, may not appear to be annotated on the map (see arrows on the map), these addresses have also been sent a letter; this includes all addresses within the boundary and those just outside of the boundary to the south.



PUBLIC NOTICE

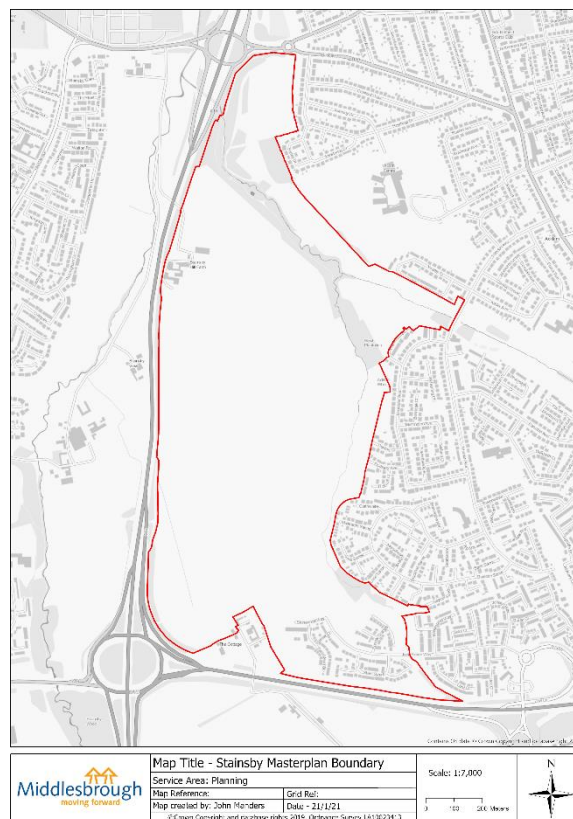
STAINSBY COUNTRY PARK AND MASTERPLAN PUBLIC CONSULTATION

Middlesbrough Council has prepared the Stainsby Country Park and Masterplan (see site plan below). The Council is undertaking a four-week period of public consultation on the Masterplan, and is seeking representations from local residents and other stakeholders. The consultation will provide an opportunity to comment on the Masterplan before it is finalised and adopted by the Council.

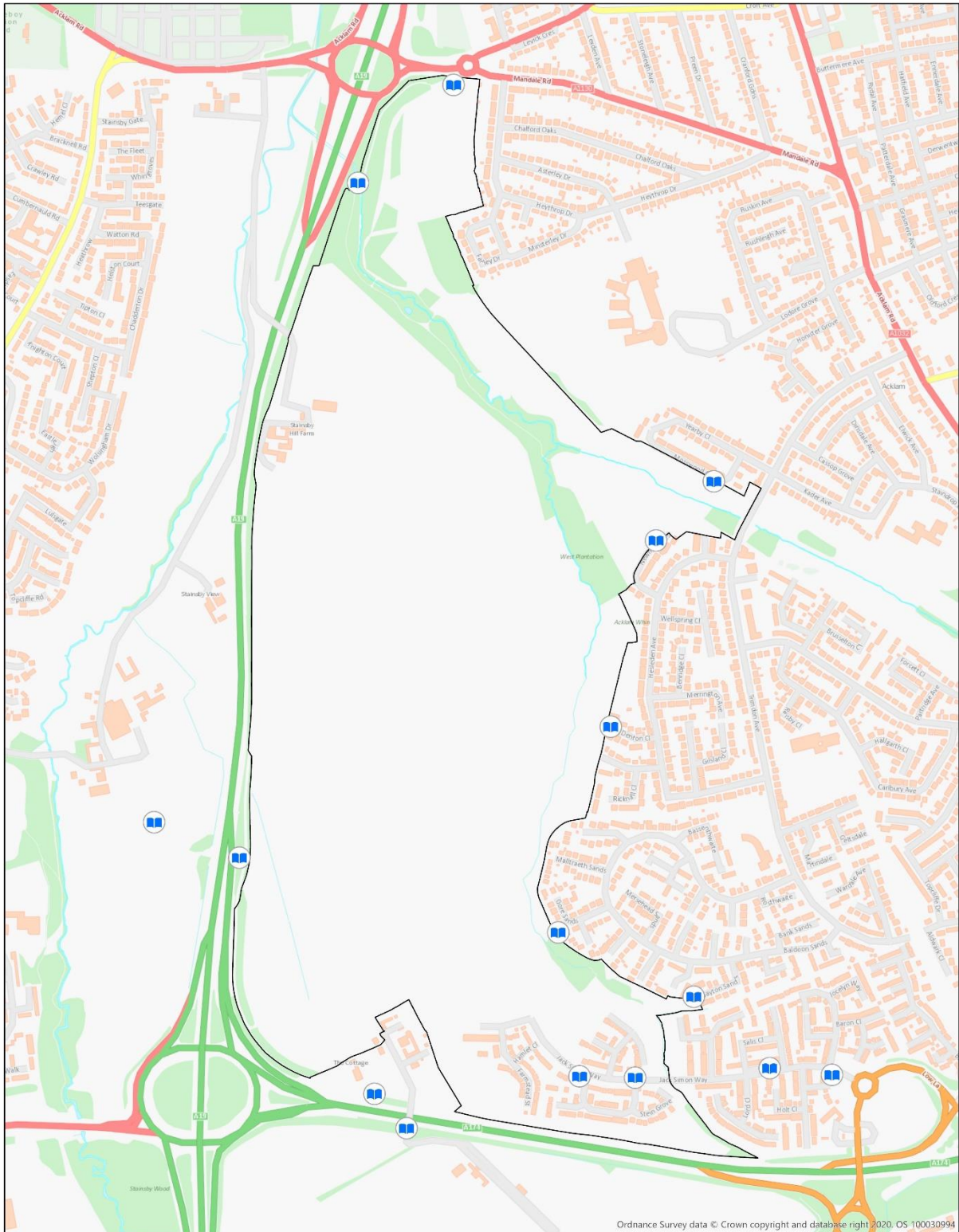
The consultation period will run from **9:00am on Monday 1st February 2021 until 5:00pm on Monday 1st March 2021.**

Online representations can be submitted using the following link www.middlesbrough.gov.uk/stainsby. Representations can also be submitted via email to Stainsby@middlesbrough.gov.uk or via post to: Planning Policy, Middlesbrough Council, PO Box 504, First Floor, Civic Centre, TS1 9FY.

If you wish to discuss any matters concerning the Masterplan, the consultation process or the next steps, please contact 01642 729068/729282 or email Stainsby@middlesbrough.gov.uk



Appendix 3 – Site Notice Locations



Ordnance Survey data © Crown copyright and database right 2020 OS 100030994

	Title - Stainsby Site Notice Locations		Scale: 1:7,000 	
	Service Area: Planning			
	Map Reference:	Grid Ref:		
	Map created by: John Manders	Date - 21/1/21		
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STAINSBY
COUNTRY PARK & MASTERPLAN
DESIGN CODE

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Creative Solutions for the Built & Natural Environment

1.0

INTRODUCTION



1.0 INTRODUCTION

1.1 Foreword

The Stainsby Site allocation is a major opportunity to create a vision for high quality residential development in a growing Middlesbrough, that meets the needs of homeowners and enhances the natural environment; creating a unique quality of place.

If Middlesbrough is to continue to be moulded as a vibrant, modern and growing city, competing against the wider north east, then the development of high quality homes is of paramount importance. Although the population of England has increased by about 10% in recent years, for the first time in generations there is population growth in Middlesbrough, with people attracted to the opportunities presented by the growing local economy. The majority of this growth has been achieved towards the south of the borough, with housing sites performing very well in terms of completion numbers, and sale values.

Stabilising the population is essential if Middlesbrough is to consolidate its status as the city-centre of the Tees Valley. Stabilising the population will help to support and improve viability of local services such as schools and shops. The objective will be achieved through a combination of providing housing in locations, and of the type, that people want, and through improvements to the local environment and investment in facilities and infrastructure.

A mixed and balanced community at Stainsby is the ultimate aim with the opportunity to provide an outstanding 'country park' setting for this community not to be missed. Middlesbrough Council, Ward members, local people and Housing Developers, alike have contributed to the future vision for the site; with the emphasis firmly on creating a high quality and sustainable place to live.

This masterplan and design code is intended to set that vision, and to test future developments against its high standards.



1.0 INTRODUCTION

1.2 Purpose of the Design Code

The development of Stainsby as a new extension to the urban area of Middlesbrough has attracted significant interest from stakeholders who wish to see sustainable development. The purpose of this masterplan and design code therefore is to set out specific principles that should be adhered to in the pursuit of 'quality of place.'

Middlesbrough Council wish to utilise this document, adopted by the Council, as a tool to assess forthcoming planning applications. The document will therefore ensure that clear guidance is set out to developers on the scale, nature and type of development expected in relation to Middlesbrough's adopted Local Plan Policies.

Where specific requirements are set out, those 'hooks' will be identified in order to demonstrate that the masterplan is in line with Middlesbrough's Planning Policy, and where these would test any application.

The detailed guidance in this document is intended to ensure that a high quality development is created at design stage and retained throughout the approval and construction process to delivery.

It should be remembered however that this document is not intended to be prescriptive but provides a framework to guide development and sets out key design objectives and principles that all developments will be expected to embrace. The design solutions and plans are but one interpretation of what can be achieved. Developers will be expected to identify their own solutions to deliver the principles set out in this document.

To this end this document recognises that developers will not be restricted to specific house types, designs or materials rather it is intended to promote innovation and best practice, particularly in layout, street scene and the green infrastructure that is synonymous with a high quality of place.

The document intends to assess and build upon the significant body of work already undertaken at previous masterplan stages in relation to current objectives, identifying any outstanding issues and strategic objectives.

This design code and indicative masterplan is based on a thorough desktop assessment of the existing site, context and character, identifying landscape and visual constraints and opportunities as well as receptors such as Air, Water & Flooding, Heritage, Ecology, Public Rights Of Way and Transport Corridors, and Noise.

It is expected that the desktop assessment will be further developed, in line with best practice place making and design guidelines, during the detailed design stage.

The document will create a site specific best practice development vision for the site, with objectives and design principles that address wider environmental objectives and outstanding issues

Lastly the document will demonstrate an indicative Outline Masterplan & design code which meets the identified objectives and design principles and illustrates the development vision and spatial layout including;

- housing densities and typologies,
- road hierarchy,
- parking strategy,
- character areas,
- landscape strategy, including country park,
- indicative sections and vignettes etc



1.0 INTRODUCTION

1.3 The Vision

The vision for the allocated housing site at Stainsby is a sustainable, mixed and balanced community set within an outstanding and engaging landscape, that includes the creation of a new country park, providing over 1500 new homes. This iteration of the masterplan seeks to flesh these out and provide greater detail and clarity around these themes.

Central to this vision is the very simple blurring between country park and residential development. This 'start with the park' philosophy has driven the development of this masterplan where new residents, existing residents and visitors will always have the 'feel' of living in a park. This huge new provision of functional green space will greatly expand recreational activities in the local area.

1.3.1 Homes, Streets and Parking

A mix of standard developer housing types is anticipated, which will vary across the site from a high density around the central commercial hub area and along primary road frontages. Lower density development will dissipate towards the open countryside, alongside areas of the country park and particularly towards Bluebell Beck minimising the impact on this important habitat.

The character of the residential area will be homes within a park and therefore the impact of roads will be reduced to change driver behaviour, lower speeds and shared spaces that promote sustainable transport and active travel. The principal road will serve the development, alongside, cycle routes, with secondary and tertiary roads serving streets, lined with trees to provide legibility to the country park.

Parking is intended to be unobtrusive rather than dominating with ample spaces for cars, but avoiding bland parking courts, and visitor parking integrated with urban landscaping. Electric vehicle charging provision will be encouraged throughout.

1.3.2 Landscape, Nature and a New Country Park

Starting with the park sets the tone for the development to provide a crucial new green space and set of green infrastructures that permeates the development. The jewel in the crown of this, is the new Country Park. The purpose of this is to 'envelope' and pervade the residential area, providing setting and character.

The Country park would extend and enhance existing habitats, water courses and recreational areas including that of Bluebell Beck with the key concern that the green open space network should permeate the development at every turn.

New habitats created would include structural native and community woodland and glades, species rich grassland, and wetlands incorporating sustainable drainage systems to manage surface water, as well as more formal areas of planting such as orchards and growing areas and natural play spaces.

In total the open space within the masterplan area will account for at least 53% of the total area.

1.3.3 Community and Connectivity

At the heart of the development and connected to the Country Park is a new community and commercial Hub. This includes a new primary school, local centre, community centre and associated formal sports facilities. Permeating the development will be a network of safe formal and non-formal cycle routes and multi-use paths, including a commuting route and footpaths that link the country park, bluebell beck, and the commercial hub with residential streets and adjacent areas of Middlesbrough.

1.3.4 Summary

- *The site is allocated for a high quality mixed and balanced community with an opportunity for increased public space*
- *A residential development within a new Country Park setting*
- *A mix of housing types, and densities creating distinct character*
- *Access for sustainable transport as well as new roads to serve the development*
- *Opportunities for a central commercial and community hub serving as the heart of the development*
- *Enhanced connectivity to and from the wider area via a network of paths*



Chicago Botanic Garden Learning Campus, MYK-D © Kate Joyce



Kurve 7, Stu/D/O © Stu/D/O, Ketsiree Wongwan



Kromhout Barracks / Karres en Brands © Jannes Linders

1.0 INTRODUCTION

1.4 Planning Policy Context and the Existing Site

The existing Stainsby site was allocated as a Housing Site within the adopted 2014 Local Plan with a specific policy attached governing the expected outcomes via Policy H21. This policy is the golden thread that runs through the philosophy of the masterplan with key elements of delivering housing within a mixed, balanced community and a country park.

Key to driving the final quality of the masterplan design will be adherence to Local and National Planning Policy. There are a number of elements to ensuring that policy will be met by the proposed planning applications, mirroring the level of design within the master plan.

These relate to the Middlesbrough Local Plan, Infrastructure Delivery Plan as well as the Middlesbrough Urban Design Guide. The Stainsby Masterplan is intended as a guide to developers on behalf of the local authority to provide certainty over design.

Due to the nature of the site as greenfield and with reasonably certain development costs, lack of abnormalities such as remediation the site is considered to be able to support the proposed housing numbers and infrastructure requirements within Policy H21.

The current planning policy context for the site in relation to its 'golden thread' of H21 is intended to set the bar intentionally high to achieve a strong quality of place. This combined with key elements of the Core Strategy and the Middlesbrough Urban Design Guide form material considerations in the masterplan design.

Housing Local Plan Policy H21

130 ha of land are allocated at Stainsby Hall Farm and Stainsby Hill Farm for a mixed and balanced residential community.

The following uses are considered appropriate:

- Residential – 1670 dwellings of which a minimum of 1125 to be completed within the Plan period up to 2029.
- Employment (B1 use) 2ha.
- Local retail centre – to be provided when need arises.
- Primary school – to be provided when needs arises.

The Council will require the development to deliver a high quality scheme that:

- a. *Creates residential development in neighbourhoods of identifiable character which provide variety and diversity in layout and design.*
- b. *Provides a mix of dwelling types and sizes, including three and four bedroom detached and semi-detached dwellings.*
- c. *15% of dwellings to be affordable provided as 5% of the dwellings on site and a 10% off-site affordable housing contribution.*
- d. *Provides the school and local centre in a central location which maximises accessibility for future residents, timing to be agreed subject to need.*
- e. *Is accessed from both the B1380 and A1130 creating a link road through the development.*
- f. *Provides any necessary off-site improvements to transport infrastructure to ensure that traffic generated by the development does not have a detrimental impact on the highway network.*
- g. *Provides pedestrian and cycleway links throughout the development to improve connectivity including links to the residential areas and community facilities located to the east of the site and to the public right of way to the south west of the site.*

- h. *Incorporates a country park along the western and south western parts of the sites including significant areas of woodland and structural landscaping to provide screening from the A19.*
- i. *Incorporates green corridors adjacent to Saffwood Beck and Blue Bell Beck, including a local park west of Saffwood Beck with pedestrian links to open space at Stotfold Walk.*
- j. *Where crossings over the becks are required this shall be by clear span crossings rather than culverting.*
- k. *Enhances the Local Wildlife Site in the north of the site and provides compensatory provision for any loss of habitat required for highway access.*
- l. *Maximises the use of SUDS, water efficiency measures and landscape buffers as appropriate to protect Saffwood and Blue Bell Beck from urban run-off and sedimentation.*
- m. *Maintains an access route for farm vehicles from the farmstead at Stainsby Hill Farm to the farmland south of Stainsby Grange Equestrian Centre. A noise assessment will be required to take account of traffic noise from the A19 and A174.*

1.0 INTRODUCTION

1.4 Planning Policy Context and the Existing Site continued..

Middlesbrough Core Strategy Design - CS5

CS5, requires that *“all development proposals will be required to demonstrate a high quality of design in terms of layout, form and contribution to the character and appearance of the area”*.

Policy CS5 further seeks the *“enhancement of the best characteristics of Middlesbrough’s built environment to create a positive identity for the town and improve the quality of life of its population”*.

Middlesbrough Urban Design Guide

This document will be used when determining most planning applications and failure to comply with the guidance set out in this document will be resisted.

A successful development should: -

- a) *relate well to the surrounding geography and history of the land, and contribute to the wider environmental benefits for the town;*
- b) *be of a density that is appropriate to the locality;*
- c) *respond to the natural patterns of movement in and around the site and encourage connectivity;*
- d) *respect and exploit already established important local and strategic views and vistas;*
- e) *relate to the scale (height and massing) of the surrounding buildings;*
- f) *consider the diversity and pattern of the already established surrounding urban grain, block and plot size;*
- g) *consider the current building line;*
- h) *ensure satisfactory measures are incorporated in the design and location of the development to minimise the effects of noise from any existing or potential sources nearby;*
- i) *create a site with identity;*
- j) *act as a visual focus, or complement the open spaces that already exist;*

Legibility, clear definition of public/private realm

Buildings and the spaces between them should work together to create key recognisable events and places that are easy to navigate through. Spaces are most successful when it is easy to identify who is meant to use them.

Development can be designed to ensure that places are easily understood by:-

- a) *creating or redefining a clear hierarchy of routes, streets and spaces;*
- b) *locating activity and mixes of uses along key transport corridors;*
- c) *relating building heights to streets and spaces to reinforce their relative importance;*
- d) *supporting a hierarchy of open spaces whose function and importance can easily be understood through their design;*
- e) *relating development to the visual connections between it and the surroundings;*
- f) *protecting and enhancing existing locally distinctive landmarks;*
- g) *creating new landmark buildings and spaces where appropriate e.g. gateway sites;*
- h) *incorporating memorable public art at landmark locations;*
- i) *reinforcing visual connections along routes with appropriate landscaping, lighting and signage;*
- j) *designing developments so that their intended functions are clear, and the access to entrances are visible and appropriately located; and,*
- k) *forming easily recognisable and clear delineation between private and public land through the use of boundary treatments where appropriate.*

The ten One Planet Living principles:

-  **Health and happiness**
-  **Equity and local economy**
-  **Culture and community**
-  **Land and nature**
-  **Sustainable water**
-  **Local and sustainable food**
-  **Travel and transport**
-  **Materials and products**
-  **Zero waste**
-  **Zero carbon energy**



**ONE
PLANET
LIVING**

1.0 INTRODUCTION

1.4 Planning Policy Context and the Existing Site continued..

Further to the above elements of Local Planning Policy and also Design Guides, the Stainsby Masterplan has been designed to ‘**One Planet Living Principles**’ wherever possible.

The One Planet Living framework adopted by Middlesbrough Council and partners is built around ten principles of sustainability that relate to environmental, economic and social aims. By gradually making changes to the way we do things we are making Middlesbrough a more sustainable community - one which lives within the planets available resources.

Referenced in the Middlesbrough Urban Design Guide the One Planet Living Principles are intended to be designed into the Stainsby Masterplan and these identified opportunities for each of the ten principles to work within the masterplan are shown below for guidance to developers;

Zero Carbon- The design although on the edge of the Middlesbrough Conurbation will ensure adequate reduction in car usage as well as maximising opportunities for buildings to utilise renewable energy and passive measures.

Zero Waste- The creation of the country park will ensure that on site cut and fill could be reused.

Sustainable Transport- The masterplan aims to wherever possible connect the development to existing sustainable transport links in the south of Middlesbrough. A key consideration will be a cycling commuter route throughout the development to allow quicker access to the town centre.

Sustainable Materials- Although largely controlled by other regulations it is likely that builders using standard house types will use a degree of sustainable materials.

Local and Sustainable Food- There may be opportunities to create food growing areas within the both the private areas as well as the public realm.

Sustainable Water- SuDs will be employed in a number of ways and as part of the wider development in order to create low run off rates minimising flooding issues on site. These areas with

higher flood risk on site will be considered within the detailed design process with opportunities for creation of detention ponds or larger areas of water retention as part of the landscape structure or country park. Provision of further Bluebell Beck Improvements in water quality and natural habitats and protection of the Beck from Impacts via appropriate buffers will be a key element of the design.

Land Use and Wildlife- Achievement of a well used and valued network of well maintained Open Spaces which cover all the future requirements of Middlesbrough residents in terms of different types of land use such as allotments, parks and gardens and natural green spaces. Open Space and the green infrastructure should be, where possible, publicly accessible, well signed and welcoming. Open Spaces will be used to support health and recreational activities. Middlesbrough residents will be actively involved in the future of their local spaces. Areas of cultural, historic or natural significance will be restored and interpreted Allotments into self use, volunteer groups supported in management and development of green spaces, key to the potential for a flourishing country park.

Culture and Community- The creation of the country park should assist with creation of a community and also improve wellbeing with access to high quality open space.

Equity and Local Economy- The masterplan seeks to create a mixed and balanced new community to provide new homes in strengthening the Middlesbrough economy, as well as provision of a new commercial hub in a sustainable location including shops, a school, commercial property and a country park visitor centre.

Health and Happiness- Improve the natural and urban environment to create healthy and sustainable opportunities to improve economic and social opportunities for community and volunteer engagement. The design and creation of a country park will provide a solid foundation for community engagement and events.



1.0 INTRODUCTION

1.5 Review of Existing Masterplan

Before undertaking any proposals GRADONARCHITECTURE with Valley Environmental have undertaken a full review of the Stainsby Draft Masterplan as completed by Niven Architects and TGP Landscape Architects.

This foundational process included initial site analysis and the development of a combined masterplanning & landscape strategy over three Public Consultations. These Consultations were vital in defining the parameters that brought about the positioning of the Primary Road and access to the development sites. The position of the local centre and landscape corridor concepts were also explored with initial ideas of the relationships between the developments and the green space.

GRADONARCHITECTURE and Valley Environmental have sought to use these thoughts as a basis of the Design Code, further enhancing and developing these concepts.





1.0 INTRODUCTION

1.6 Updated Masterplan

The detailed scheme seeks to define a landscape driven masterplan creating a development that is continually connected to the wider country park.

To do this a series of Site analysis and desktop studies have been undertaken to inform a naturalised approach to the landscape driven elements. These studies and parameters then inform a framework to define a series of Urban principles in response creating hierarchies and features to define and enhance local identity and placemaking.



2.0 SITE ANALYSIS



2.0 SITE ANALYSIS

2.1 SITE LOCATION

The site allocation lies to the South West of the centre of Middlesbrough in a prominent location bordering the A19 and the A174.

The site therefore has ease of connections to the surrounding boroughs through this network of wider connections.

2.2 EXISTING SITE PLAN

The site itself is predominantly greenfield site made up of Farmland, Green Open Spaces and Woodland.

Due to bordering the A19 and A174 the site itself is isolated from the neighbouring green field land and is currently inaccessible to public access.

2.3 SITE OWNERSHIP

The 140ha site is divided into three Principal Ownerships.

Middlesbrough Council own the public accessible land to the East which incorporates the Green Open Spaces, Woodland, Blue Bell Beck and Mandale Meadow.

The farmland is divided between two ownerships, the Pearson's and the Weightman's.

The southern most portion of the land has already transferred from the Weightman ownership to Story / Miller Homes who are in process of developing 299 homes with access from Jack Simon Way.



2.0 SITE ANALYSIS

2.4 SITE VIEWS

View 1

View of North Public Open Space from existing car park.

Features:

- Flat meadow.
- Mature trees bordering the A19.
- Existing residential houses to the left.

View 2

View from Mosswood Crescent over BlueBell Beck.

Features:

- Beck runs in a small depression with trees following the same route.
- Large public open space.
- Mature trees by the Beck and at the far site boundary.

View 3

View from access lane to Stainsby Farm.

Features:

- Land in foreground slopes steeply.
- Open farm land used for grazing.
- Long view to the Cleveland Hills.





2.0 SITE ANALYSIS

2.4 SITE VIEWS CONTINUED.

View 4

View from path near Curthwaite.

Features:

- Existing houses facing on Public Open Space.
- Beck runs in a small depression with trees following the same route.
- Open fields beyond the beck.

View 5

Features:

- House and helicopter hangar on the right of photo.
- Grass paddock in the foreground.
- Long views over Teesside.



2.0 SITE ANALYSIS

2.5 Topography and Flood Risk

The site has a range of ground conditions. The majority of the land within private ownership is gently undulating and sloping fields. However, this land slopes more steeply where the Blue Bell Beck and Saphwood Beck flow through the site and have created small valleys along their course. There is also a band of steeper sloping ground near Stainsby Farm.

The land owned by Middlesbrough Council features two flat areas of public open space, Acklam Meadow and Mandale Meadow, but the majority of the land is sloping towards the Becks.

There is a small area of the site that has been identified as a flood risk zone within the Pearson land ownership. The flood risk zone in general predominantly occurs along the course of Blue Bell Beck and particularly to the North-West just before it is culverted under the A19; at the highest point of the tidal reach.



2.6 Site Access

There are only two potential access points to the site. Access points are limited as no new access can be created from the A19 or A174 which border the south and west of the site. To the east is an existing residential area and any new access from here would create unacceptable additional traffic through these areas. Two potential site access points have been identified at north and south of the site.

South Access

The south access point will be an extension of Jack Simon Way. The first phase of development from this access has already been designed and approved. From this first phase of development there are two access points into the next phase of development.

North Access

The northern access point is from the existing Mandale roundabout where there is already an access to a small car park. This car park has the potential to be enhanced to serve as an additional entrance to the proposed Country Park and green open space network. It already serves as access to the green space for dog walkers and the local community and will be retained and enhanced in its function.



2.7 Rights of Way & Connections

There is one Public Right of Way to the South of the site. The route is along the site boundary next to the A19 and A174. The PROW crosses the A19 and A174 on single track road bridges. The crossing point of the A19 and A174 are important connections to retain as there are very limited crossing points on these roads for pedestrians, cycles or horses.

Within the council owned Public Open Space land there are a number of footpaths that are well used by the local community, although they are not formally recognised as PROWs. Some of these routes are permanent features with tarmac paths, other are more informal routes across the public open spaces that have been created by people using the space. There are a number of bridges crossing Blue Bell Beck but none across Saphwood Beck.

The country park will create an extensive network of formal and informal paths creating links to land to the west of Blue Bell Beck, which currently has no public access.



2.0 SITE ANALYSIS

2.8 Trees & Hedges

There is a ribbon of trees along the route of Blue Bell Beck and Saphwood Beck. This cluster that extends from the South, near Jack Simon Way and stretches North to Stainsby North. At the north of the site the woodland widens and expands beyond the route and frames part of Stainsby North.

Although there are the occasional mature trees much of the woodland is relatively new, apart from two significant areas of trees; West Plantation & Acklam Whin (or Fox Covert) which are shown on maps dating back to 1853.

The areas of agricultural fields are separated with hedge boundaries and the occasional hedgerow tree.

It is intended to retain the existing trees along Blue Bell Beck where possible and enhance with new woodland planting as appropriate as part of the new country park.



2.9 Air Quality and Easements

The most significant issues regarding air quality is anticipated to be traffic noise from the surrounding roads. The A19 & A174 both have high volumes of traffic.

Appropriate solutions to mitigate any noise pollution will need to be incorporated into the design. The height of the site in relation to these roads varies across the site and the noise mitigation measure may vary across the site to adapt. In some areas landscape bunds already exist to help mitigate the road noise, however in other areas the A19 is level with the site.

There is one significant easement that affects the site, but this is only on the north sections. Two large sewer pipes pass under the site and no development can be built within their easements.

Apart from the above there are no other significant easements that affect the site.



2.10 Site Analysis Summary

All the previous site analysis diagrams have been brought together in this image to give an overall appreciation of the site constraints and opportunities.

Key Opportunities & Constraints

- Large site with predominantly gently sloping land does not limit development layouts option.
- Sloping topography around the becks creates opportunity for interesting POS.
- Existing POS and mature woodland can be incorporated and expanded upon to create the Country Park.
- New development can face onto attractive POS.
- Very limited impact from flood risk.
- Main site entrance road from the South does not cause disturbance to any features within the site.
- Noise pollution from A19 & A174 will need to be mitigated.
- Site is not part of the Conservation Area.
- Easement around sewer pipe throughout the site will restrict certain uses and landscaping approaches.
- Links to surrounding residential areas possible.



2.0 SITE ANALYSIS

2.11 Desktop Analysis Overview

The site has undergone a desktop assessment for constraints associated with environmental impacts from residential developments. This includes a DEFRA MAGIC website search to identify potential receptors.

Fig 2 and fig 3 shows the comparison between the existing landscape, as shown by the satellite image, and the historical landscape, as shown by the extract of the 1853 map. Apart from the encroachment of housing and roads the pattern of the landscape is clearly identifiable. The locations of former orchards have been highlighted on the plan as has West Plantation and Fox Covert, which continues to form a significant landscape feature. As is evident the pattern of the landscape across the site has changed little, woodland cover has perhaps increased and West Plantation and Acklam Whin are easily identified, along with a number of orchards associated with the historic farmsteads. The location of West plantation has been transposed onto the satellite image, as has the location of the orchards; however the orchards are now gone or in severe decline. Existing woodland will be retained and enhanced and the presence historically of orchards will inform the design. The rectilinear pattern of later enclosure hedgerows, many of which are now missing or in decline or recently re-established, will not be retained and instead a more naturalistic approach will be used based on landform and drainage.

Fig.2 Existing Site Satellite



Fig.3 Extract map 1853



Fig.4 Habitats

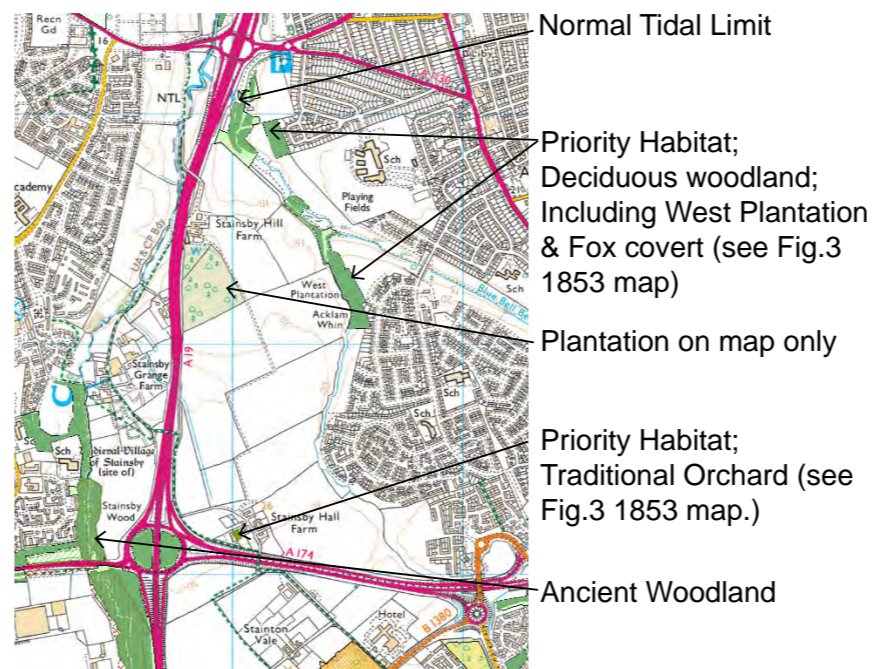


Fig.5 Biodiversity

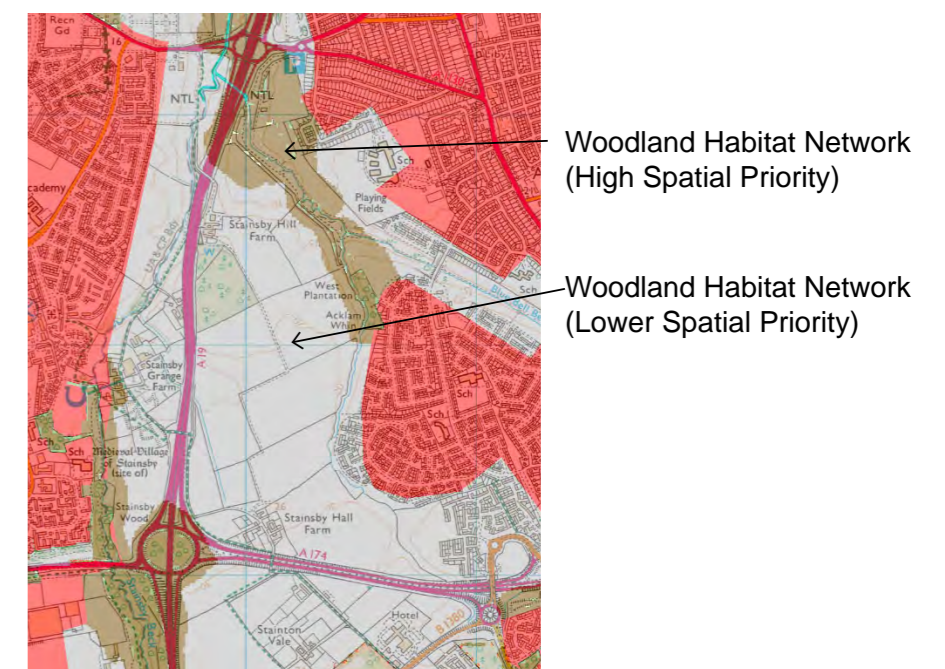


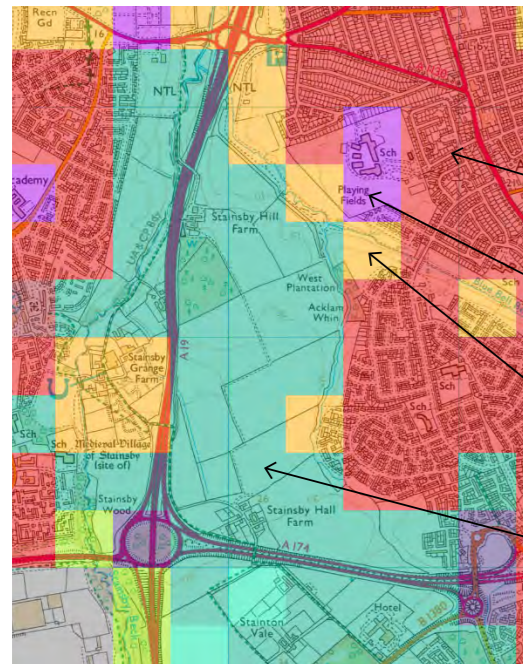
Fig 4: Habitats

Due to the predominantly agricultural nature of the landscape, habitats are restricted to narrow hedgerows and field edges. Small blocks of woodland follow the line of bluebell beck, the most significant of which are West Plantation and Fox Covert. Generally the riparian habitats associated with the beck are very narrow. The northern most point of bluebell beck is under the influence of the tidal reach, which may cause seasonal flooding into the area directly south of the culvert. The plantation shown on Fig.4 is not present and there is only marginal evidence remaining on site of the former farm orchards. The area of ancient woodland to the south west is disconnected from the site by the A19.

Fig 5: Biodiversity.

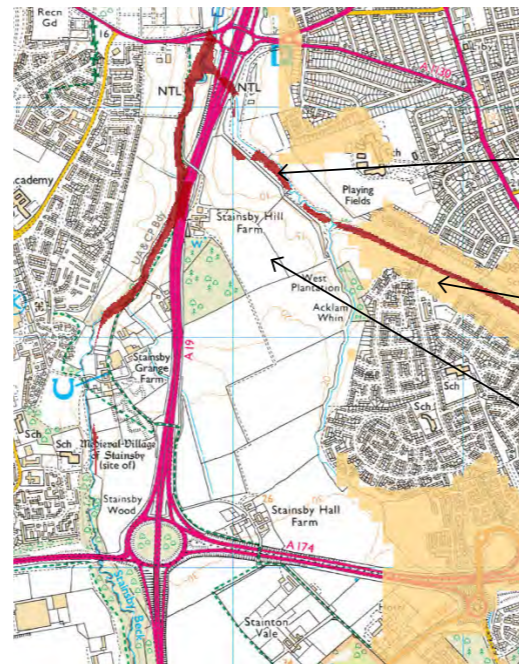
Whilst there are parts of the site with high biodiversity, Fig 5 helps to highlight the relatively low site biodiversity across the masterplan area and reflects the sites use as intensive arable farmland. Bluebell Beck however has the potential to deliver significant environmental benefits in the form a of a wide range of habitats including woodland, wetlands, grassland and riparian habitats and has a high priority.

Fig.6 Landscape



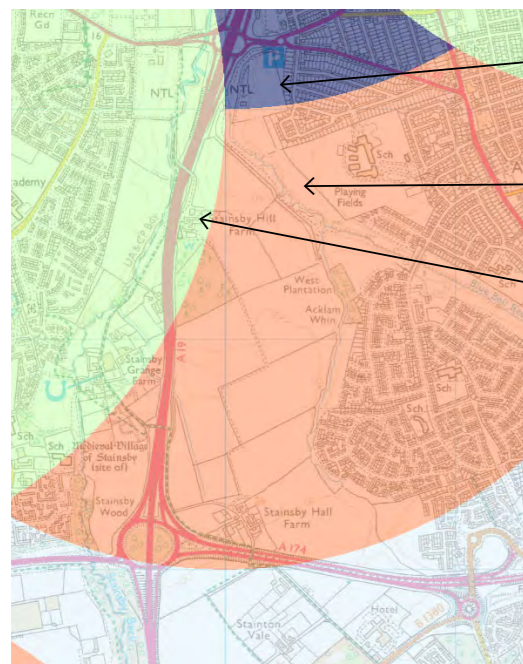
- NCA 23 Tees Lowlands
- Settlement inter-war to postwar
- Civic Provision; Education
- Recreation; Park & Garden, Planned Fields
- Enclosed Agriculture; Pre-Modern, Planned Fields

Fig.7 Water



- Woodland Flood Risk (High Spatial Priority)
- Woodland Flood Risk (Lower Spatial Priority)
- Keeping Rivers Cool

Fig.8 Species



- Lapwing/Tree Sparrow
- Tree Sparrow
- Grassland Assemblages Farmland Birds/Lapwing/Redshank/

Fig.9 Designations



- Stainsby medieval village & open field system Scheduled Monument
- Stainton Vale Farm House Grade II

2.0 SITE ANALYSIS

Fig 6: Landscape

The site is situated within the NCA Character area 23 of the Tees Lowlands and surrounded by urban development including the A19 to the west and residential development to the east, including schools, dating from the 1950s to the 1980s. The majority of the site itself comprises enclosed and private agricultural land. The development of the peripheral areas is shown on Fig 10.

Fig 7: Water

Bluebell Beck forms the primary water course within the area with a number of areas of standing water in the area. There is some flood risk from rivers within the Bluebell Beck area to the north of the site and some further surface water flood risk in the central east areas of the site.

Fig 8: Species

Notable species identified within MAGIC include Lapwing and Tree Sparrow to the north of the site as well as Grassland assemblages, Farmland Birds and potentially Redshank. Bat species are expected within Bluebell Beck and are likely to use this area as well as linear hedge features for foraging.

Fig 9: Designations - Heritage

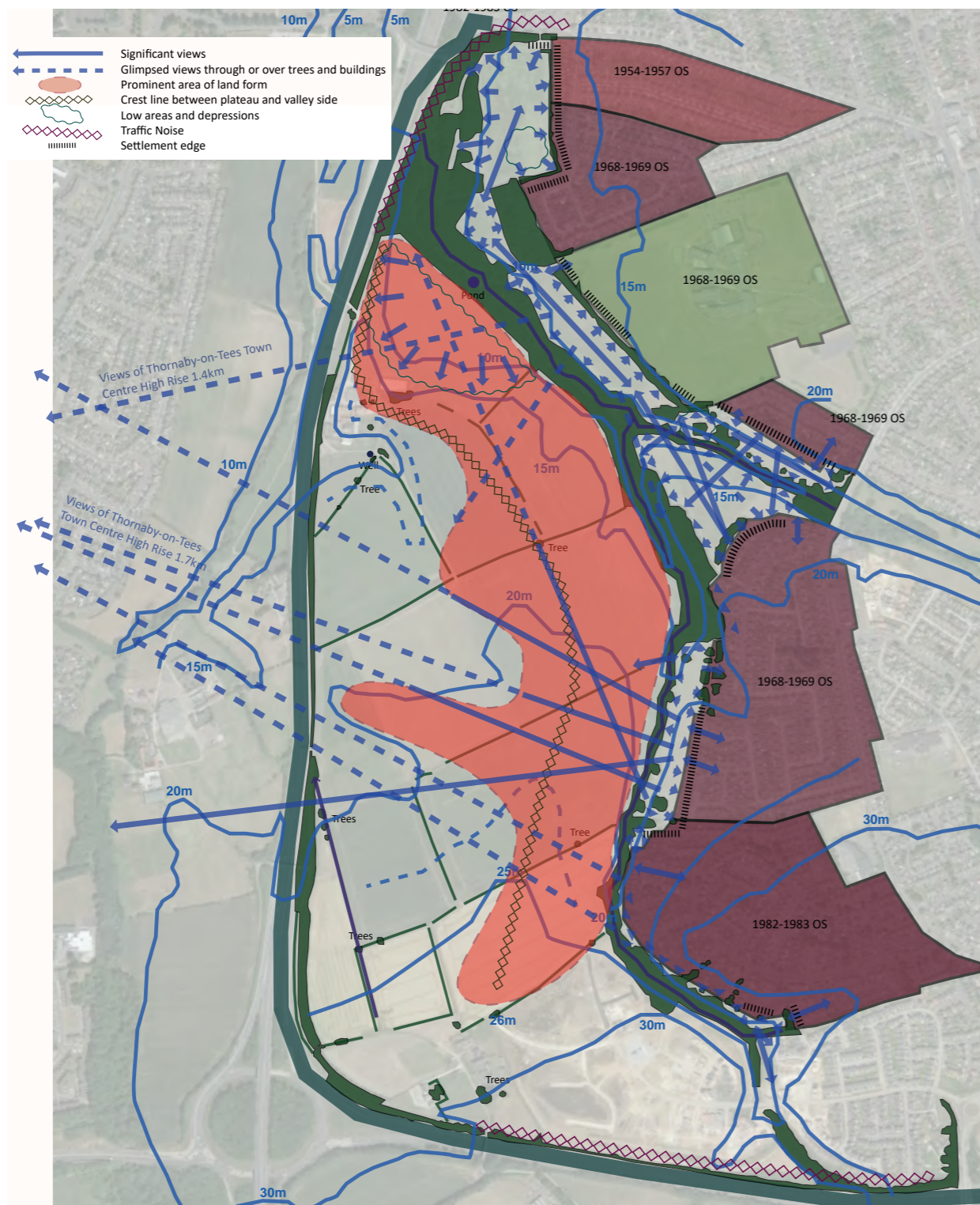
Stainsby Medieval Village whilst outside of the masterplan area, and cut off from it by the A19, there is potential that parts of the site may have historic/heritage value.



2.0 SITE ANALYSIS

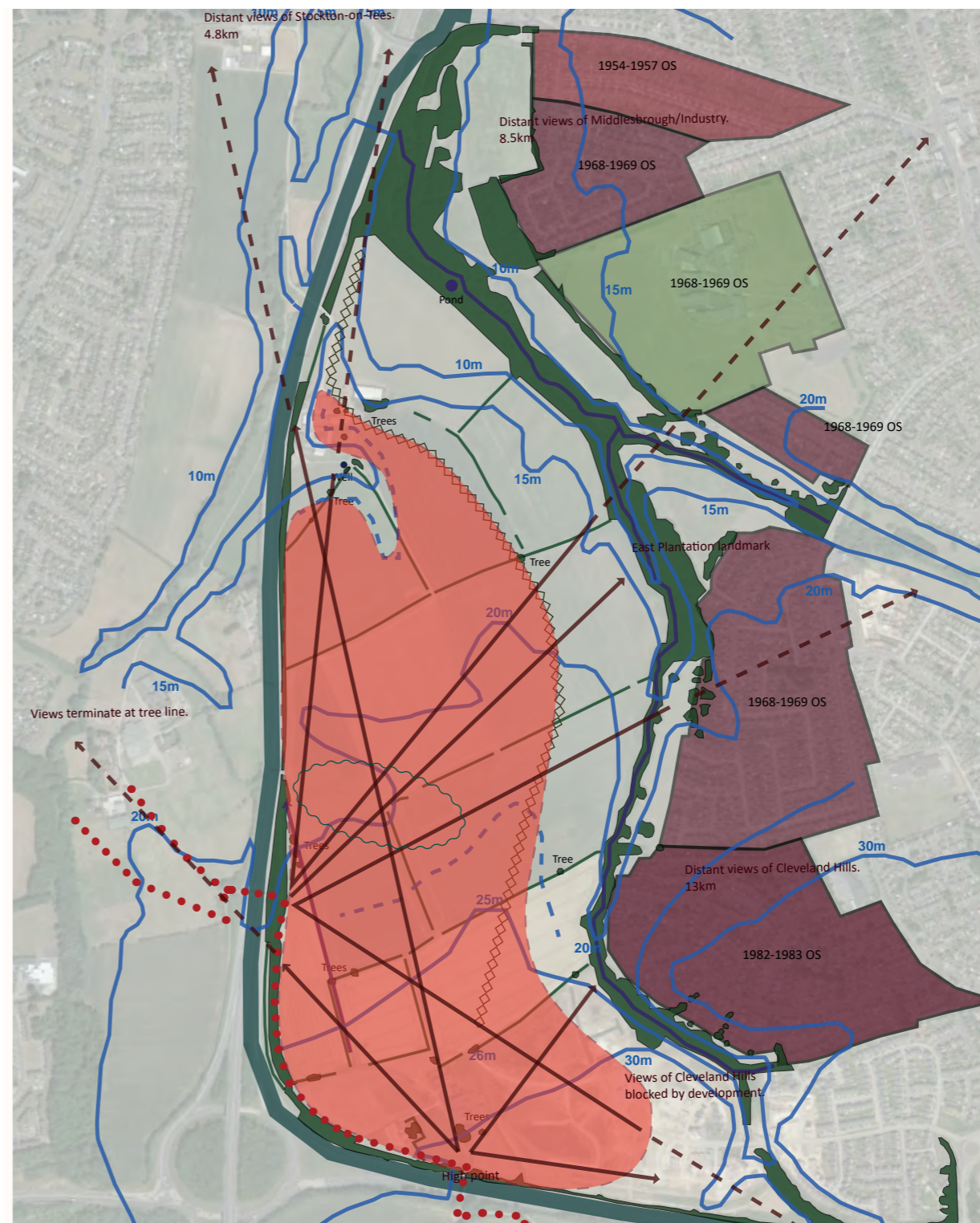
2.12 Views from Public Open Spaces

The POS to the East of Saphwood and Bluebell Beck is generally inward looking and compartmental, with the occasional long axis views through the compartments that often terminate at existing housing. There are occasional views over or through the trees to the high points within the development site, most notably from the POS to the west of Foxberry Ave, due to the narrow line of trees along the beck at this point. The development site occupies a gently undulating plateau spur, dissected to the west by the A19 and delineated by the course of the Bluebell and Saphwood Becks to the East, the sides of which become steadily steeper to the south. Other than blue bell beck here are few dramatic features other than shallow depressions and hollows. Due to the topography the most prominent views of the development site follow the crest line between the plateau and the steeper valley side. The multi-story buildings of Stockton-on-Tees form a focal point in many of the longer distance views.



2.13 Views from Public Rights of Way

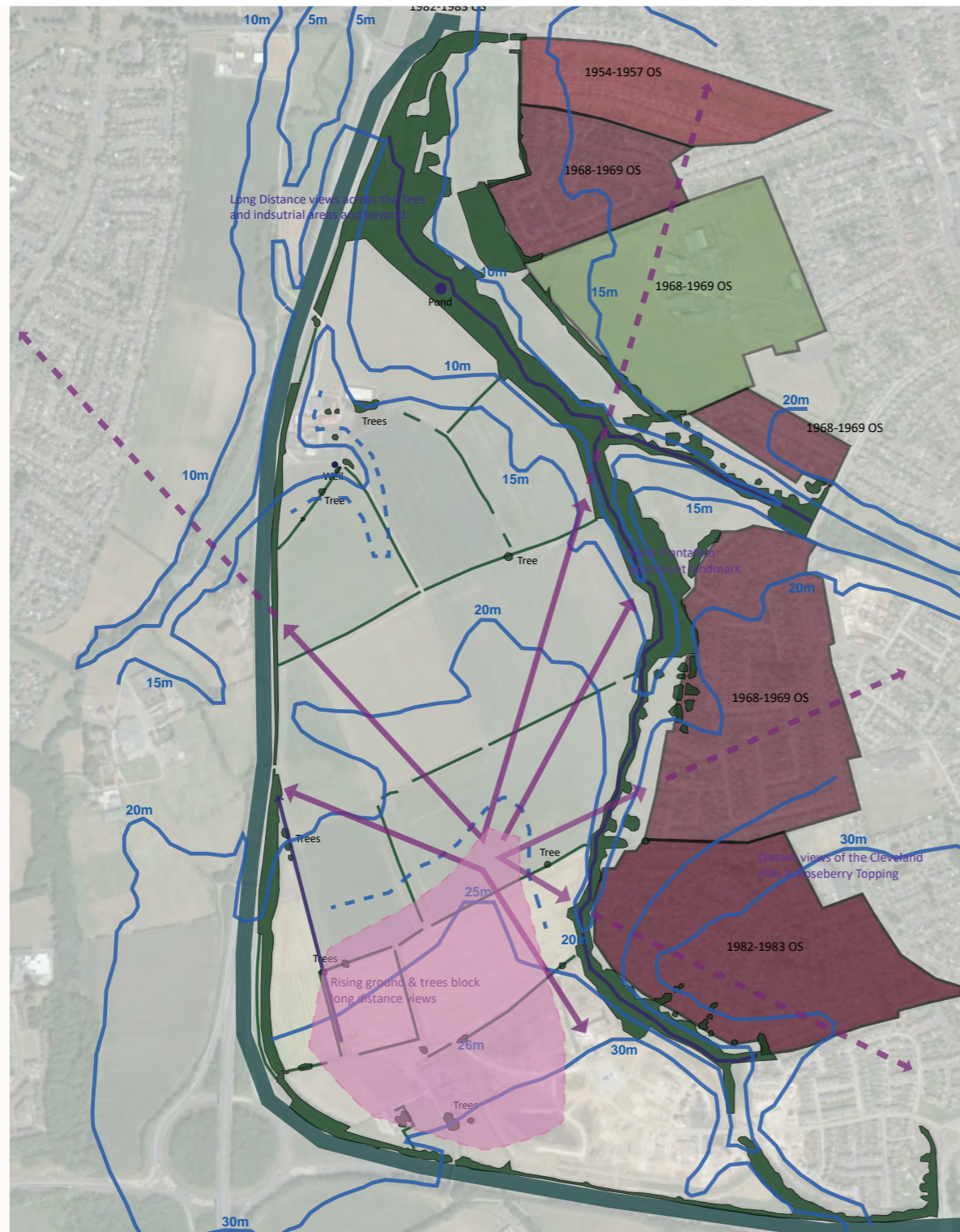
There is a high point to the south of the site, on the bridge that the footpath crosses, that provides good views across the site; To the North across Stockton and Middlesbrough and distant views East toward the Cleveland Hills and Roseberry Topping. Generally though views are not dramatic and are dominated by the tree line along Bluebell Beck, including the prominent East Plantation, and the existing housing development in the middle distance. The elevation of the high point is not great enough for views not to be blocked by the proposed development; other than from the very edges of the development.



2.0 SITE ANALYSIS

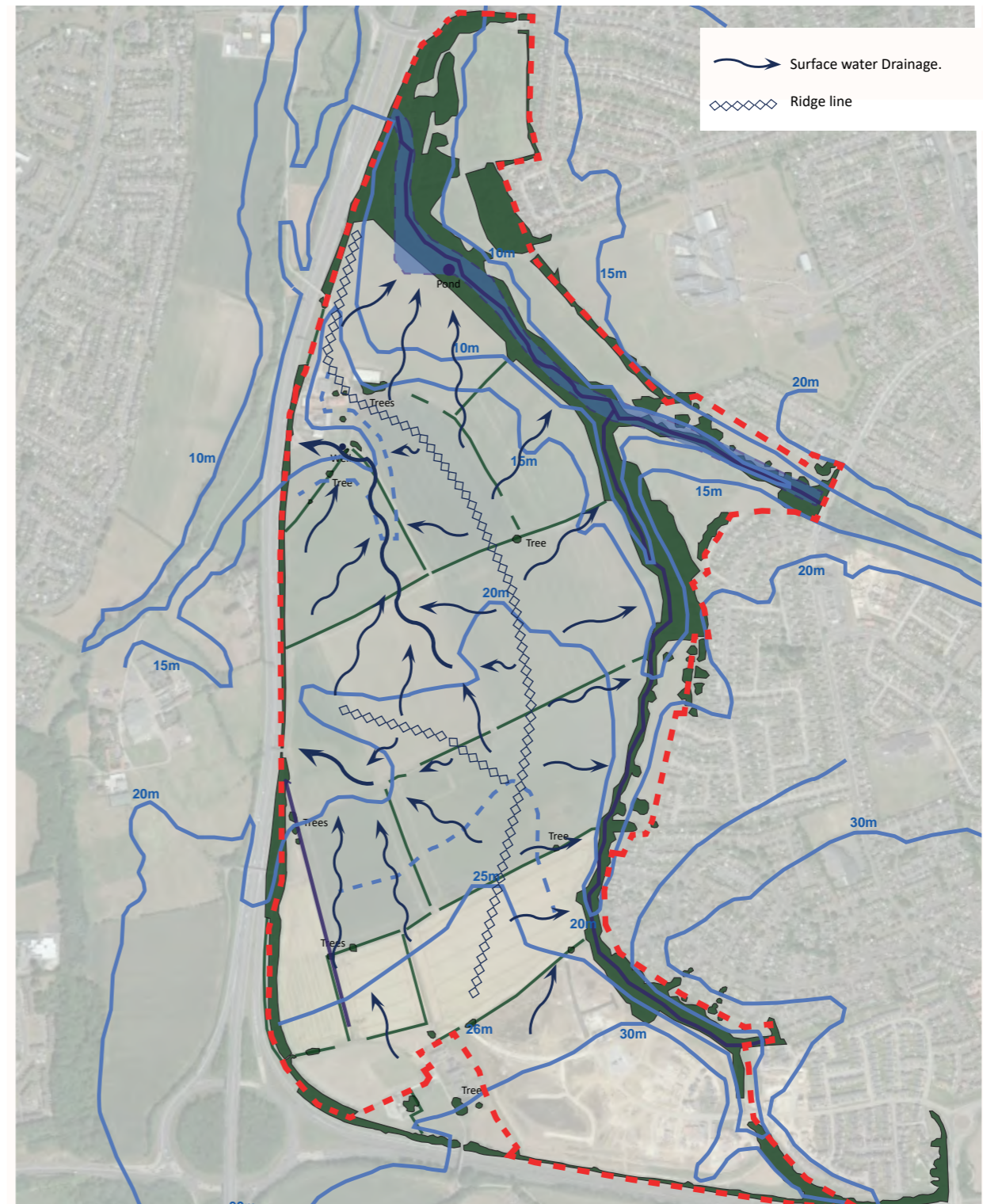
2.14 Views from Site.

The gently south sloping topography allows distant views North across Stockton and Middlesbrough, and distant views East toward the Cleveland Hills and Roseberry Topping. To the South views are terminated by the rising ground and the new development. There are no dramatic vantage points and only the edge of the development will benefit from views once completed, and these will generally be short distance, of the proposed beck and country park; except where space can be created to allow longer distance views from the site high ground.



2.15 Topography and Flooding & Surface Water

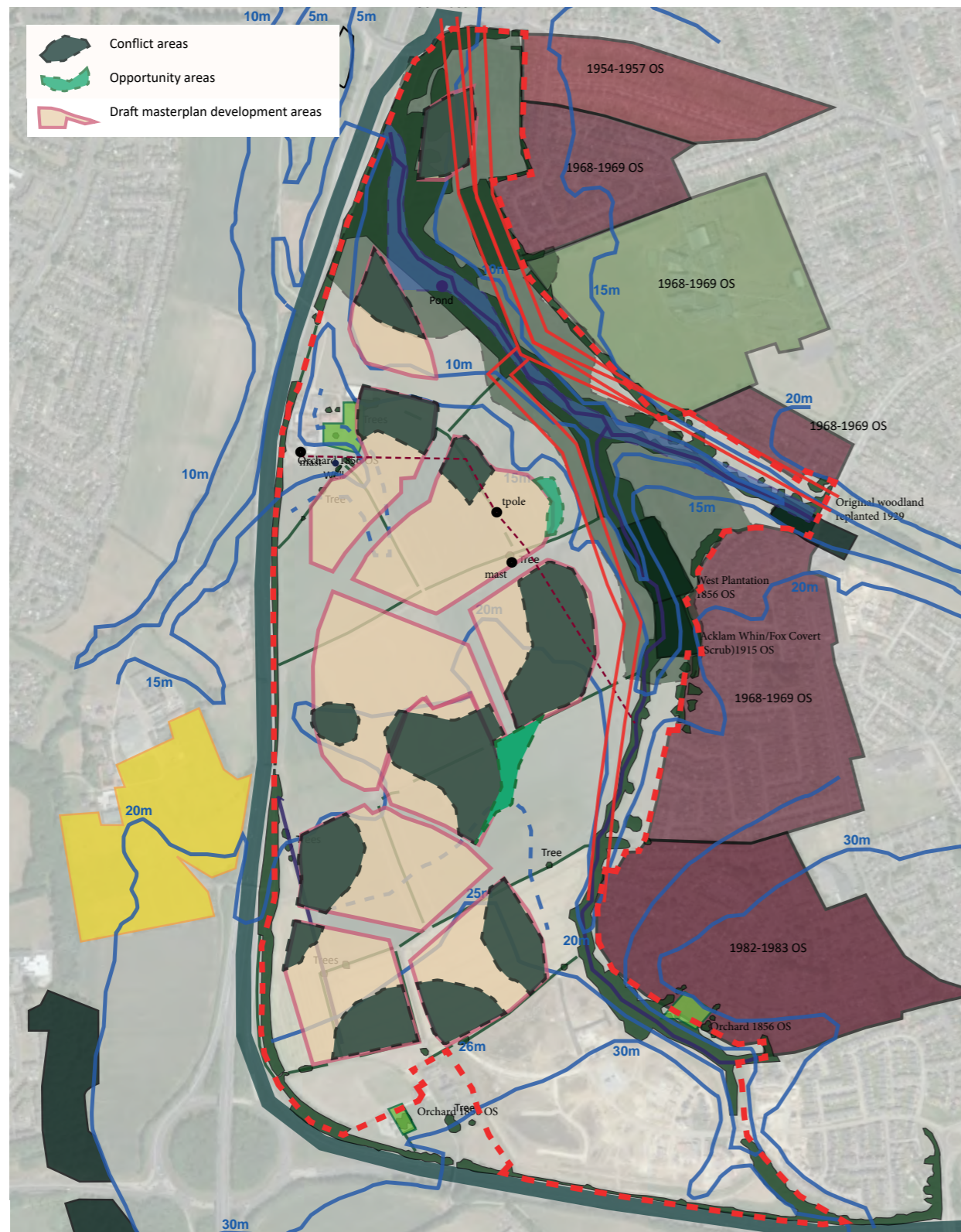
Where appropriate, and subject to detailed site investigation, the green links should be aligned to take advantage of the sites existing surface water drainage patterns, so that surface water can be managed as part of a naturalistic sustainable drainage system (SuDS), including ponds, wetlands and water channels for the benefit of amenity, biodiversity and water quality; in accordance with the CIRIA SuDS manual. SuDS elements should not be placed within existing flood zones, or within areas under the influence of the tidal reach; unless under specific circumstances for the specific benefit of wildlife and habitat.



2.0 SITE ANALYSIS

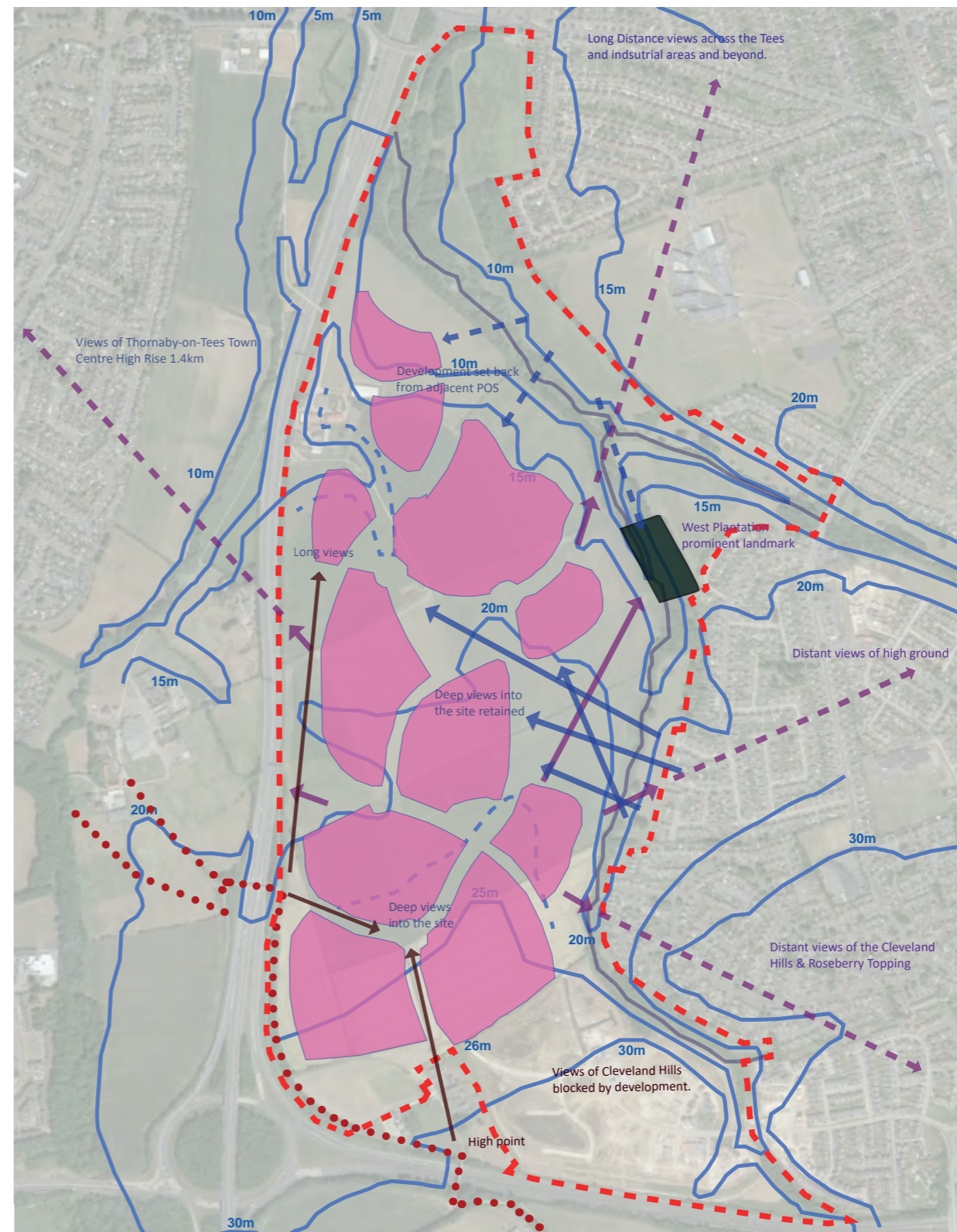
2.16 Assessment of Draft masterplan.

This plan assess the draft masterplan development areas against the site assessment, including; plotted views (in and out of site); significant site features (woodland, orchards, topography); site constraints (flooding and easements, traffic noise); strategic environmental objectives (MAGIC); and layout changes (single access and omission of northern part of development). It highlights potential conflicts and opportunities to be resolved by further design.



2.17 Retained Views

Visual analysis of the site has influenced the layout of the proposed development, helping to reduce adverse visual effects. Key views to and from the site have been retained where possible.



3.0 DESIGN DEVELOPMENT



3.0 DESIGN DEVELOPMENT

3.1 Primary Road, Sewer Easement and access road

The first layers considered in the Design Development are the fixed elements within the site including existing sewer easements, the development of the Primary Road, and existing access track.

An existing sewer easement exists within the site west of Saphwood Beck, running approximately north south. This establishes an undevelopable area and will restrict the habitats that can be created across it.

The location and design of the primary road has emerged following three consecutive rounds of community consultation. The road connects newly completed development at the south of the site through to the area of existing farm to the north.







A further connection will be created through to an enhanced roundabout entrance from Mandale Road. This Northern extension of road, that passes over Blue Bell Beck, is essential in providing the adequate transport capacities to service the allocated development site. This entrance arrangement will be carefully considered to minimise any impact on to the Green Open space and creating a gateway to the site. Re-allocated carparking will be provided off the Mandale Road exit to both sides of the spine road to serve Mandale Meadow for local residents and access the extended green open space network.

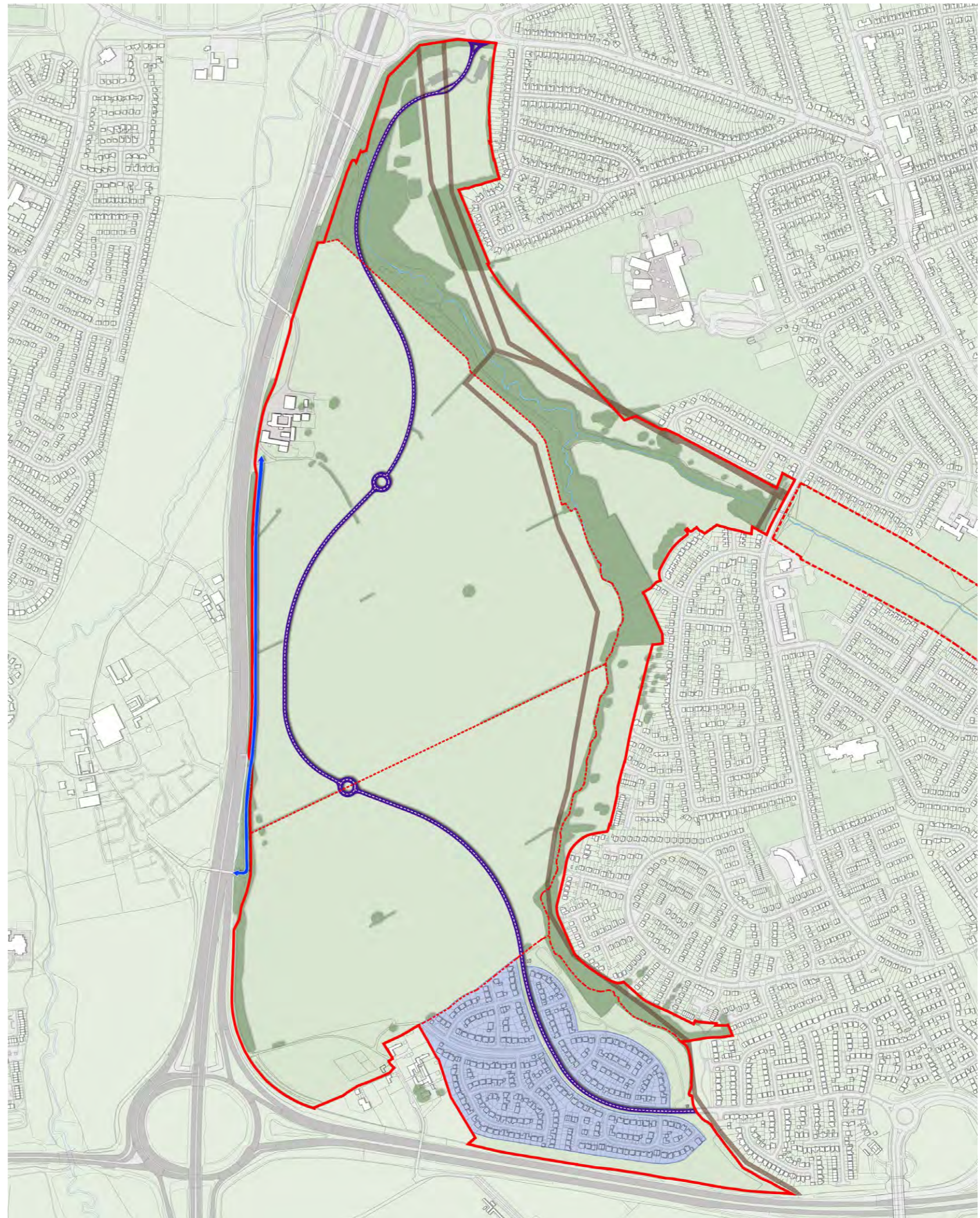
The configuration of the road is otherwise considered to be the most appropriate solution.

Connection from the development areas to the Primary Road will be wherever possible exclusively from roundabout situations. The design of the road is subject to junction capacity studies yet to be undertaken and will need to be developed in conjunction with detailed applications.

Landscape edge conditions and abutments will be addressed within section 4.0 Landscaping proposals and section 7.0 Urban Strategy Studies.

To the west of the site an existing access route will need to be maintained and a new track created between Stainsby Hill Farmstead and their connected farmland across the A19 to the South West along the site boundary.

- KEY
-  Existing Trees
 -  Existing Easement
 -  Phase 1 Development
 -  Primary Road
 -  Access retained between Stainsby Hill Farmstead and farmland across A19
 -  Land Ownership Boundaries





3.0 DESIGN DEVELOPMENT

3.2. SUDS Layout and Landscape Areas - Summary

The landscaping strategy forms the next layer of design development building on the fixed positions.

Diagram 3.2.1 shows an initial summary of landscape designated areas in conjunction with the proposed sustainable drainage corridors.

Landscape proposals on which the layout is founded are explored in detail within section 4.0. Landscaping.

KEY



Concept Green Landscape Strategy



Concept SuDs Landscape Strategy



3.0 DESIGN DEVELOPMENT

3.3 Developable Quantum Within Residential Context



Diagram 3.3.1 shows the resulting areas which may be considered as developable clusters following the integration of the proposed primary road within the network of SUDS and existing and proposed landscape forces.

The location of the central hub can be established at the point of convergence between the clusters of development, Primary access road and landscaping which is drawn into the heart of the scheme from the east.

Further Urban conditions are established in 6.0 Urban Strategy. Edge conditions and interface requirements between Primary Roads and built forms are detailed within section 7.0 Urban Strategy Studies.



KEY

-  Residential Development Zones
-  Central Hub Zone



3.0 DESIGN DEVELOPMENT

3.4 Secondary Road Links

Within the setting established for potential development, connectivity is required between the Primary Road layout and the residential clusters.

A network of Secondary Roads is shown in fig 3.4.1 representing an efficient form of connection that enables vehicle servicing links, whilst preventing congestion of the Primary Road and clearly generates a hierarchy of roads.

Edge conditions and interface requirements between Secondary Roads and built forms are detailed within section 7.0 Urban Strategy Studies.

KEY

 Secondary Road



3.0 DESIGN DEVELOPMENT

3.5 Strategic Cycle Route

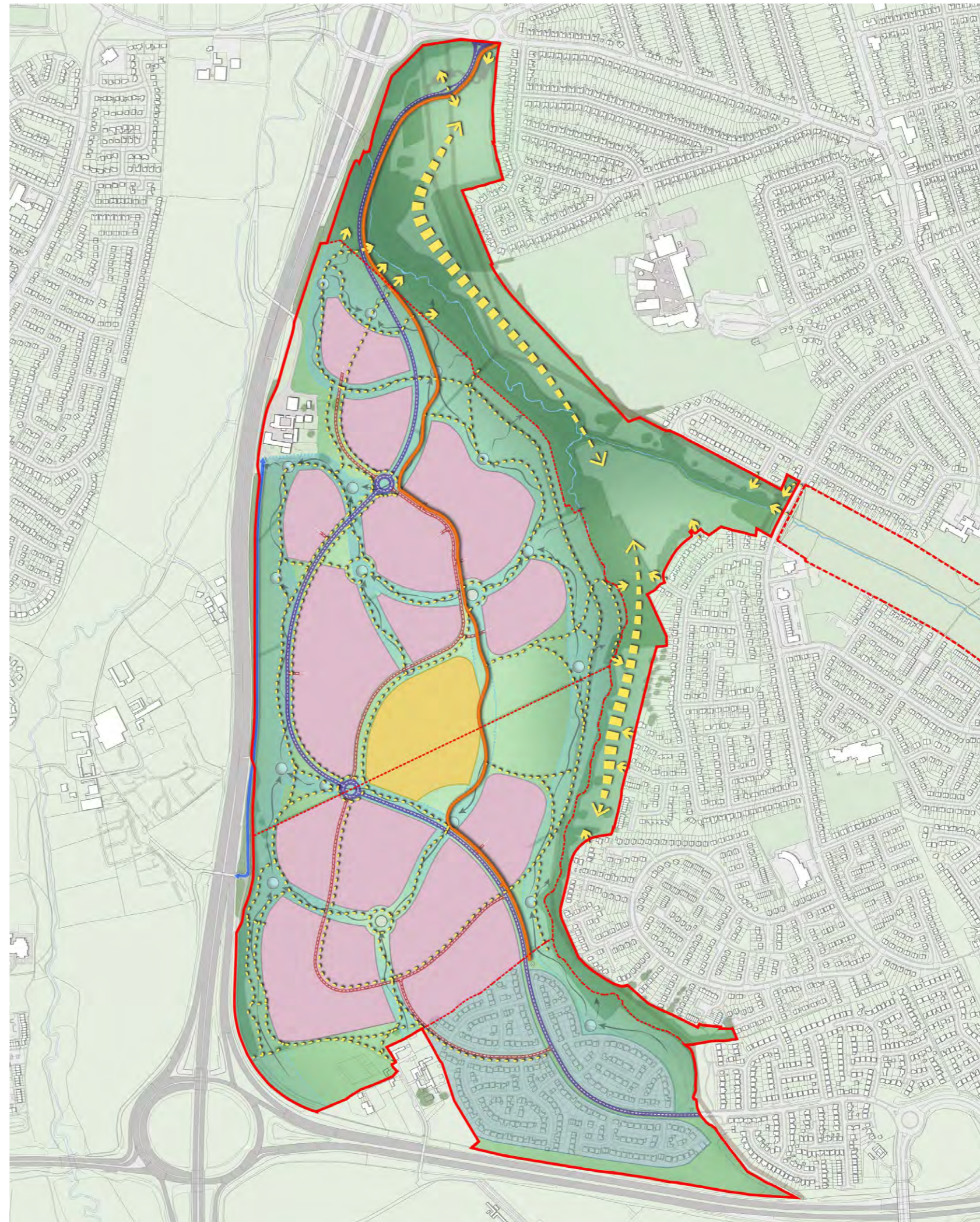
A strategic cycle route is then thread through the site, defining a priority through route that doesn't incorporate any road crossings. This route, as expanded on in Section 4.0 Landscaping, is significant in driving local connectivity within and beyond the site, prioritising a sustainable public network.

3.6 Network of Paths





The final layer to be applied to the design development will be a network of paths, connecting and tie together all aspects of the masterplan layers.

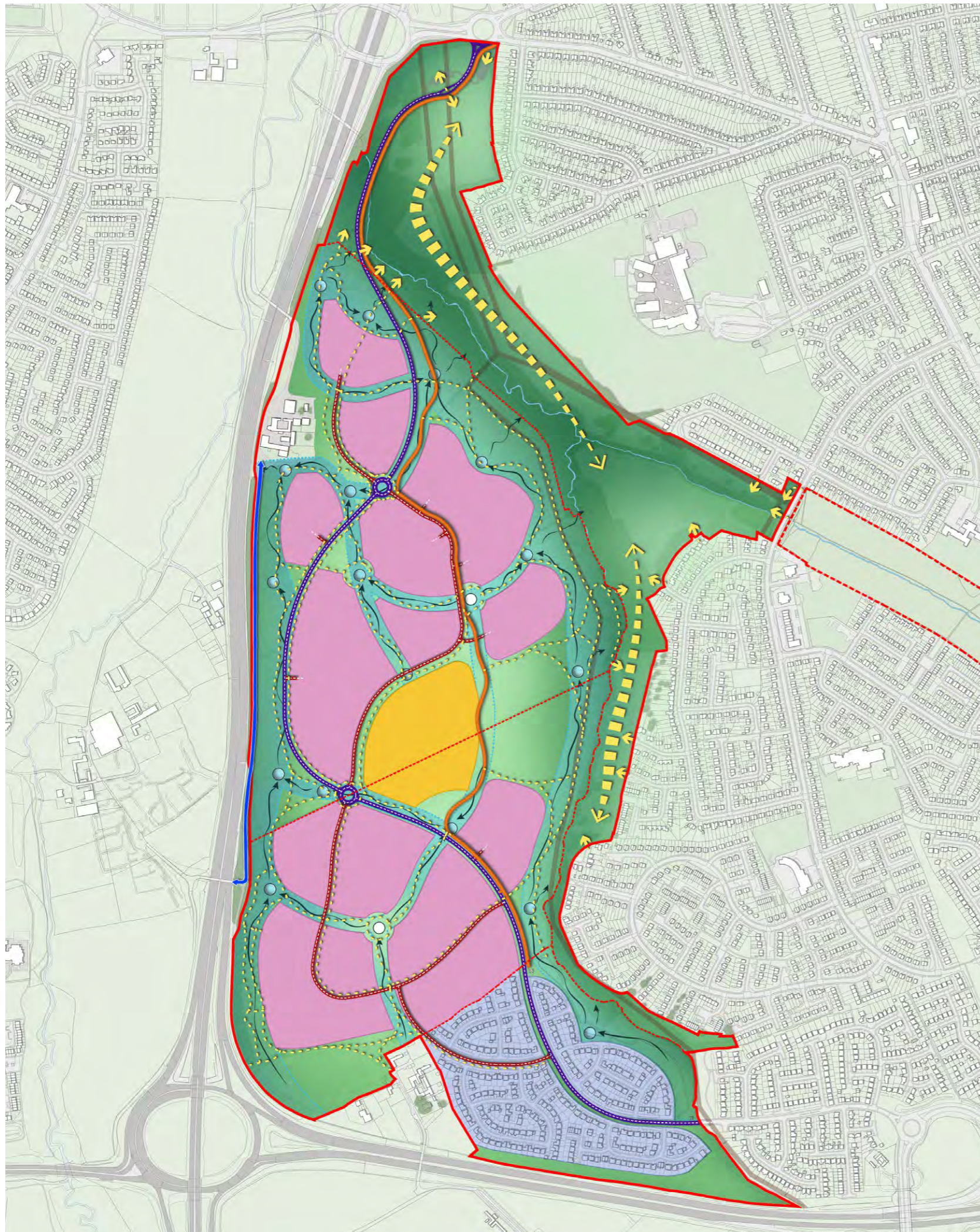
The path structure, as its developed, will navigate through all the Landscaping conditions creating a network of hierarchy throughout the site. Each development area should be able to access all areas of the site through this integrated path structure. See further studies in 4.0 Landscaping, section 4.5 Paths & Routes. At this stage all paths are shown as principles for indicative purposes.

Path networks will seek to connect the new development areas to the existing habitat areas without negatively impact these valued spaces. As the Country Park is developed a series of paths (formal and informal) will seek to connect these existing habitats together within the Middlesbrough Council's land ownership.



KEY

-  Strategic Cycle Route
-  Indicative Path Network
-  Potential entry points to existing habitats
-  Movement between existing habitats to be formed as part of Country Park



3.0 DESIGN DEVELOPMENT

3.7 Site Strategy Summary

These layers are brought together to begin to develop an overview of how the revised Masterplan has been informed. The detailed design principles are established in the subsequent sections as a result of this.

KEY

- Existing Trees
- Existing Easement
- Phase 1 Development
- Primary Road
- Access retained between Stainsby Hill Farmstead and farmland across A19
- Concept Green Landscape Strategy
- Concept SuDs Landscape Strategy
- Residential Development Zones
- Central Hub Zone
- Secondary Road
- Strategic Cycle Route
- Indicative Path Network
- Potential entry points to existing habitats
- Movement between existing habitats to be formed as part of Country Park
- Land Ownership Boundaries



3.0 DESIGN DEVELOPMENT

3.8 Green Space Calculation

The adjacent Diagram 3.7.1 indicates the strength of the landscaping proposal by displaying the extent of green space in comparison to development area, explored further in section 4.0.

The diagram highlights a primarily landscape setting for development clusters with connecting infrastructure.

An equivalent area to over **53%** of the total site is to be retained as green space.



4.0 LANDSCAPE



4.0 LANDSCAPE

4.1 Landscape Strategy & Masterplan

The proposed landscape for the development is as important as the built environment layout. It is considered a critical element of the sites overall success in delivering a great quality of place; informing its design and defining its character. Public consultation has shown concern over the loss of public open space, the open countryside, and the impacts on habitats, particularly Bluebell Beck and Mandale Meadow.

The site is predominantly agricultural land with no public access and relatively low levels of biodiversity in these areas. The design approach will deliver a net gain in biodiversity through the creation of a wide range of habitats across the site.

Key principles for the landscape strategy are:

- *Creation of an enveloping Country Park maintained and adopted by Middlesbrough Council*
- *Ensuring the Country Park permeates the development in terms of its character throughout the site*
- *Neighbourhood and Local Area Play and park elements through to formal recreational and sports pitches are provided*
- *Consolidation and strengthening of existing habitats including Bluebell Beck*
- *Natural Management of surface water integrated into the wider setting of the development*
- *Creation of a network of sustainable transport routes, walking paths and circular routes for recreation.*
- *Landscape Buffers between existing housing, existing transport routes and specifically between existing habitats*
- *Housing densities across the site lower in relation to open countryside and Bluebell Beck to reduce impacts*
- *A community and commercial hub within a green setting*

The landscape strategy will include the following elements;

Country Park & Facilities, including the anticipated form and function of the Country Park

Connectivity & Green Fingers, including how areas will connect via green links to both the commercial and community hub and the country park

SuDs, including how water will be managed naturally to help prevent flooding, improve water quality and create wildlife habitats and improve biodiversity, where this will happen and how it will integrate into road and street structures

Recreation & Circular Routes, including short walking routes and general recreation areas

Art and how public art and legibility will be blended into the development

Trim Trail & Dog Walking, running and walking routes, including areas that promote physical activity in a green environment and access for all

Formal Sports Pitches & Play Areas including the rationale for the location for these and the hierarchy and location of neighbourhood to local play areas

New Structural Woodland Habitat, the location and connectivity of this and how it is intended to create the setting for the development alongside other green infrastructure

A Green Commercial & Community Hub, and how this will relate to wider areas of green infrastructure and the setting that is envisaged.

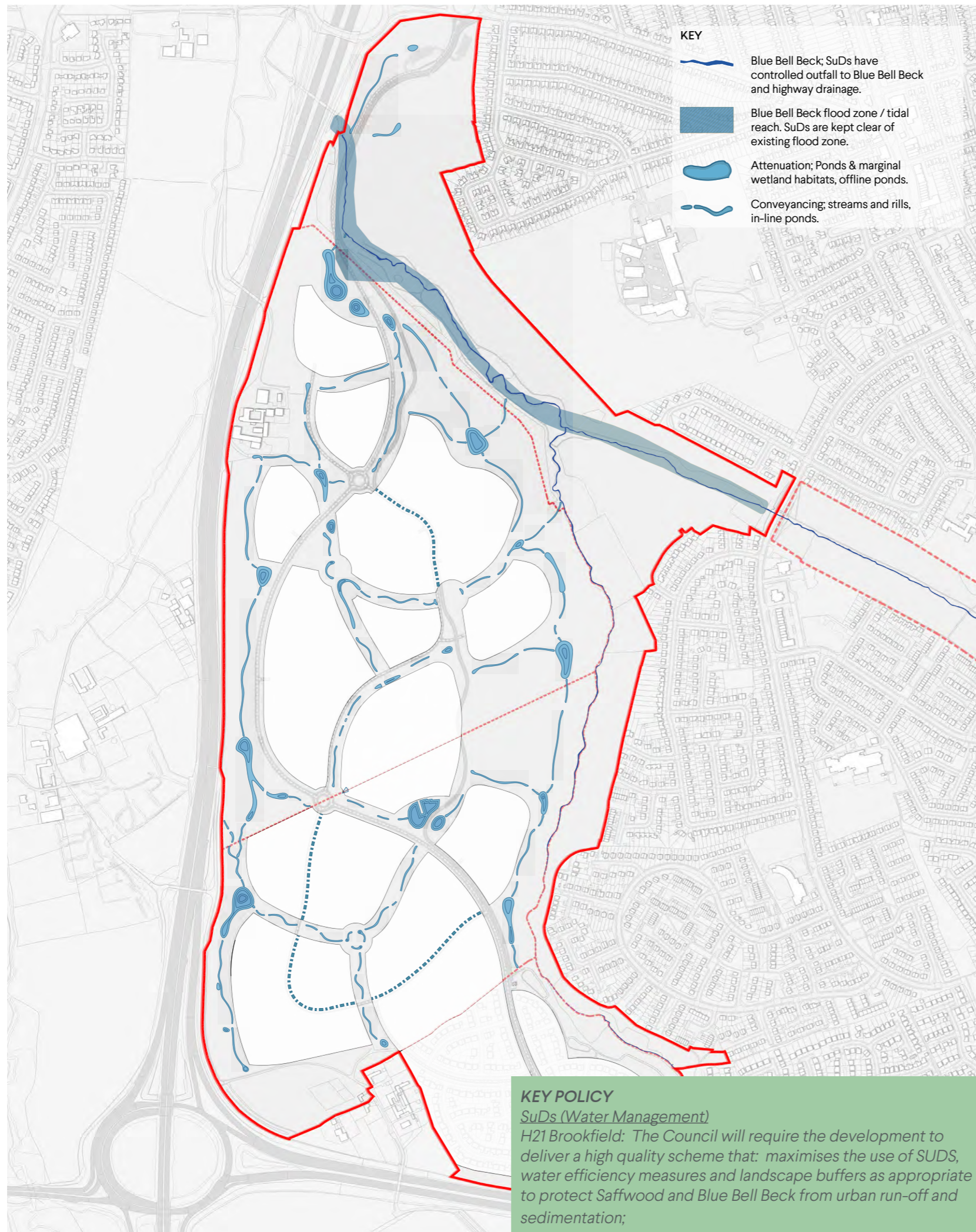
Food growing, allotments and public orchards

Quiet Areas, areas for solitude and wildlife watching as well as for other fitness activities.

Wildlife Areas, areas exclusive for wildlife:

- Wildlife movement corridors
- Segregated areas for different users of the country park/open space





4.0 LANDSCAPE

4.2 SuDs & Natural Water Management

The management of surface water will be provided by a range of naturalistic SuDs elements; including ponds and wetlands for attenuation and treatment and water channels for conveyance.

It is essential that SuDs are designed from the outset to be integrated into the open space network to establish the provision of important wildlife habitats that contribute to enhancing biodiversity. This is especially important for those elements that are provided within the Country Park and green corridors. Heavily engineered SuDs solutions that do not contribute to the design ethos for the development will not be supported.

The indicative surface water drainage strategy adjacent has been informed by landform and topography in accordance with CIRIA SuDS manual best practice guidelines. These in turn have helped derive the form and location of the green wedges that thread through the development; helping to bring dynamic nature into the heart of the development.

The SuDS features, ponds, wetlands and streams provide valuable habitats and public amenity resources that thread through the site interacting with other elements of the design including woodland and meadow, play areas and art features.

SuDS help to deal with surface water on site, prior to discharge into Blue Bell Beck, helping to prevent flooding and helping to improve water quality.

Landscape build outs on secondary roads are to incorporate rain gardens as part of the Urban SuDs network.



4.0 LANDSCAPE

4.3 Habitats & Biodiversity

The Stainsby site offers an opportunity to strengthen and expand upon significant existing habitats such as Mandale Meadow, Bluebell Beck and West Plantation and create a net gain in habitats and biodiversity by creating a wide range of new woodlands, wetlands and grasslands in the place of existing arable fields. The structure and variety of habitats will respond to site conditions and help to create a sense of place and variation, helping to make the landscape legible and guide people in and out of the Country Park and through residential areas. It is expected that these existing wildlife habitats will be incorporated into the country park in their current state with minimal intervention.

The basis of the landscape design is the interaction of three primary habitats throughout the site, for the benefit of people and wildlife; native woodlands, wetlands (SuDS) and diverse areas of grassland meadow. The site design should encourage dynamic process such as the controlled erosion of water channels the superseding of grassland by scrub and wetland habitats by dry grassland and trees, to create a mosaic of ever-changing habitats.

The three habitats interact to create the backdrop for human activities including informal and formal recreation, vibrant meeting and gathering places and places for solitude and peace and retrospection. Woodland planting will define areas of enclosure and open space, frame views and screen eyesores; creating a range of places for people to explore and move through.

New Native Woodland; naturalistic native woodland planting will recreate the feel of regenerating and recolonising native woodland throughout the site, providing a contiguous and linked habitat throughout the site and seamless transition from 'green wedge' to the wider extent of the country park, facilitating the movement of wildlife throughout the site and providing the dominant character of the site. Ultimately the woodland will grow into a mature woodland for future generations, providing shade and climatic control and ecosystem services for the wider environment.

The woodland type will be configured to suite the varying conditions across the site, using native species appropriate to the site, from oak woodland to alder carr and scrub, planted in accordance with 'Creating New native Woodlands' Rodwell et al, with the purpose of creating a natural looking native woodland including understorey and ground cover species planting.

Tree planting will need to be carefully managed and undertaken so that it adds to the value of existing habitats and does not detract from them. Significant new woodland planting is likely to be restricted to those parts of the Masterplan area that are currently given over to farmland.

Naturalistic drainage features in the form of water channels, ponds and wetlands meander throughout the site, established with the appropriate native wetland species, interacting with woodland and meadows and providing focal points for public spaces and activities. At the Community/Commercial Hub a key water feature creates the setting for entering the 'living in the park' concept.

Existing species rich grasslands and meadows will be protected from inappropriate planting either within or adjacent to them. It is essential that these habitats are integrated carefully into the wider open space network in a way that supports their value as species rich habitats. New diverse grassland areas will be managed as wildflower meadows with diverse line paths and glade areas kept close mown for informal recreation.

Existing woodland is predominantly new mixed woodland along the steeper and wetter areas of the Blue Bell Beck but also includes the older woodland of West Plantation and Fox Covert; which through their maturity and stature create an area of different character and a local focal point.

The existing hedgerows will be retained where possible and allowed to grow out. In other places new hedgerows will be planted where appropriate to break the monotony of the long sewer easements and provide a denser habitat and shelter for wildlife. The new model hedgerows will be planted wider than traditional farm hedgerows to help fulfil their new function.

Formal planting and tree avenues help to frame views, and line streets where space is limited, create a formal character, give direction to and from the Country Park, moderate climate and provide a vector for the movement of nature. Species can be used to create defined character areas and focal points.

Areas for food growing will be created, including orchards which will reflect the historical orchards that were once a feature of the landscape.

An area of formal parkland will be created around the sports pitches to enhance the areas, using native and ornamental tree species, widely spaced.

Additional native woodland planting down the Western boundary of the site will help to mitigate noise pollution along the A19 border, in combination with naturalistic earth mounding using the construction spoil.

Habitats and Biodiversity have been designed with a number of initiatives in mind including:

- *The National Pollinator Strategy: Implementation Plan*
- *Tees Valley Wild Green Spaces*
- *Urban Pollinators Project*



KEY POLICY

Habitat Creation

CS20 Green Infrastructure : Development will be required to contribute to the delivery and implementation of this network by, where appropriate, providing green infrastructure that: c) maintains and enhances bio-diversity to ensure that development and implementation results in a net gain of Bio-diversity Action Plan habitats; d) enhances existing, and creates new, woodlands; f) takes account of and integrates with natural processes and systems;



4.0 LANDSCAPE

4.3 Habitats & Biodiversity

KEY

- Existing Woodland, including West plantation and Fox Covert
- Hedgerows; new and existing hedgerows
- New Native Woodland including Alder Carr, Scrub appropriate to site conditions and area.
- Avenue Planting: Ornamental species in formal patterns to contrast with native planting, line paths & roads and reinforce the built form.
- Orchards / Fruit Trees
- Parkland; widely spaced native and ornamental tree planting around sports pitches.
- Amenity Grassland / Sports pitch
- Retained existing meadow habitats
- Species rich grassland created in glades & swathes

The adjacent diagram illustrates the potential extends of Habitats and Biodiversity created throughout the site through different methodologies. The scheme will also seek to minimise the conflict between wildlife and urban strategies, in particular along the primary road as it passes alongside Mandale Meadow, by integrating wildlife corridors/crossings where appropriate in the attempt to create safe movement of wildlife.

The following calculations have formed the basis of the indicative scheme to date and will be subject to a detailed design review through the subsequent development stages:

There is potential to accommodate up to 16ha of new native woodland creation. This would represent the planting of approximately 18,000-24,000 native trees and shrubs; planted at a variety of centres; from 2 to 4m and including open areas as 'glades' or to allow natural recolonisation from adjacent woodland (based on rodwell et al 'creating new native woodland').

There is also potential to create approximately 1.6km of new species rich/native hedgerow, which represents 16,000-24,000 hedging plants, planted as a double staggered row as a traditional hedge and also in deeper formations to create wildlife refuges, and another 100+ hedgerow trees.

In addition there are the potential for over 800 ornamental street trees and parkland trees proposed, and over 300 fruit trees to be planted as part of public orchards.



4.0 LANDSCAPE

4.4 Amenity, Movement & Play

Recreational amenity is a key design concept, intending to create recreation and amenity benefits across the site, for all users regardless of ability.

A hierarchy of local play facilities, from natural and creative play through equipped play areas to formal playing pitches has been developed for the site ensuring that all areas have an accessible play area, suitable for smaller children, such as door step spaces. Local landscaped areas have also been developed which could provide some equipment and further to this neighbourhood areas are shown which can provide a much more structured play area. The retention of formal sports pitches and user space ensures that high levels of play are included. This is supported by a Multi-Use Games Area.

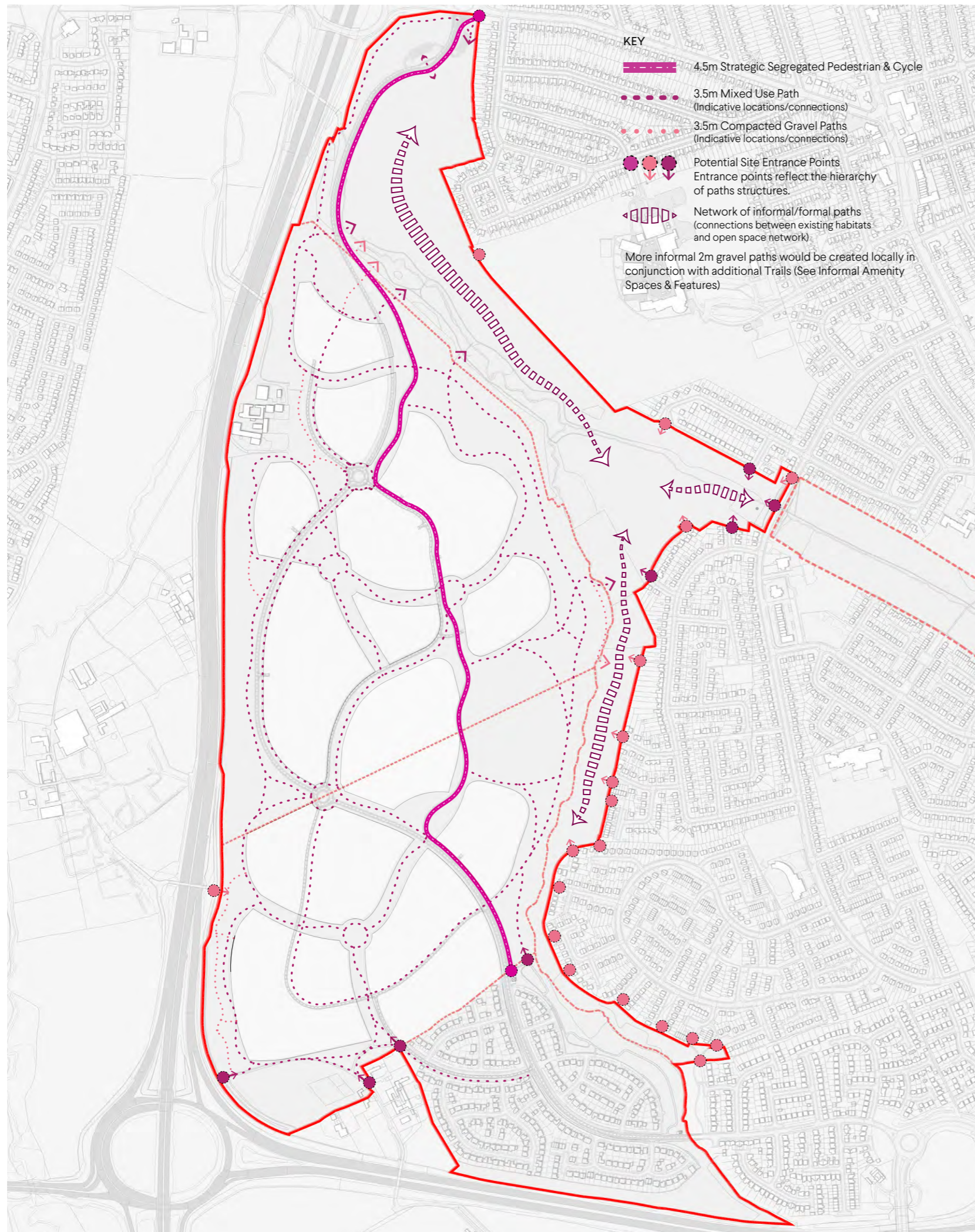
Further recreation is supported by a network of paths including waymarked circular walks around the community hub area and through the country park. The network of marked paths, and also informal cycle trails will ensure that everyone has access to a safe trail which priorities walking and crucially leads into the country park via a high-quality green space. Other opportunities for healthy activities in the country park will be provided by trim trails, event spaces for outdoor exercise and natural play areas.



KEY POLICY

Informal Amenity

Middlesbrough Open Space Needs Assessment 2018: Increase level of Amenity and Natural Green Space Aim to increase provision of amenity and natural greenspace by 1.5sqm per person



4.0 LANDSCAPE

4.5 Paths & Routes

A network of safe accessible routes and paths will be planned for Stainsby, to increase physical activity, promote sustainable and active travel and to support a friendly, sociable and cohesive community.

The strategy at Stainsby is simple. Everyone will be able to access paths and routes that take them where they want to be, whether this is to a neighbourhood play area, the commercial centre, the country park, or further afield and into Middlesbrough or the open countryside. The adjacent diagram shows an indicative proposal, subject to detailed design.

The hierarchy is such that a strategic segregated pedestrian and cycle route at 4.5m wide will be attached to the main roads providing a commuting 'superhighway' into Middlesbrough and surrounding areas.

The above path will be supported by a 3.5m wide mixed use tarmac path will provide a secondary route, permeating the residential areas. Further networks provided by compacted gravel paths, 3.5m wide, to be used as exploratory trails and particularly within the Country Park will provide amenity and recreation routes for walkers and cyclists.

Paths and Routes will be present at the Community Hub where circular routes will be used to define the space and provide easy to navigate 'healthy' trails accessible to all users, including office and retail workers.

Routes will be clearly signed and maps provided at key locations; including distance and path difficulty, gradients and terrain along with information on the landscape and habitats that they pass through.

See Section 4.14 Country Park paths for information on the unadopted path strategies. For details on the adopted path network see Urban Strategies section 6.4 Adopted Paths.

KEY POLICY

Paths & Routes

Middlesbrough Design Guide SPD: 3.16 When designing for pedestrians or cyclists, some requirements are common to both:- a) routes should form a coherent network, and be of an appropriate scale; b) in general, networks should allow people to go where they want, unrestricted by street furniture and other obstructions or barriers; c) routes need to be safe – this applies to both traffic safety and crime; and d) the environment should be attractive, interesting and free from graffiti and litter, etc.



4.0 LANDSCAPE

4.6 Country Park Paths

A clearly defined hierarchy of paths will be used to provide access to and circulation around the country park. The paths will link to the urban network of paths providing residents and visitors easy access to the country park.

This path network, as it is developed with the parameters of the Country Park and green open space network, will seek to minimise conflicts between wildlife and public users through the careful integration of path connections in order to maintain a diverse and balanced environment. The management of environments for different users/wildlife will be further detailed and developed as part of the management plan.



3.5m Multi-use Path

The multi-use path forms the backbone of the path network and will be defined by artwork and clearly signed as part of the artwork and wayfinding strategy, linking key areas and forming defined loops for circulation around the country park. Where possible the multi-use path will provide access for all. The edges of the path will be close mown for convenience, before blending in to the predominant habitat adjacent the path. The multi-use path will be formed from compacted gravel.



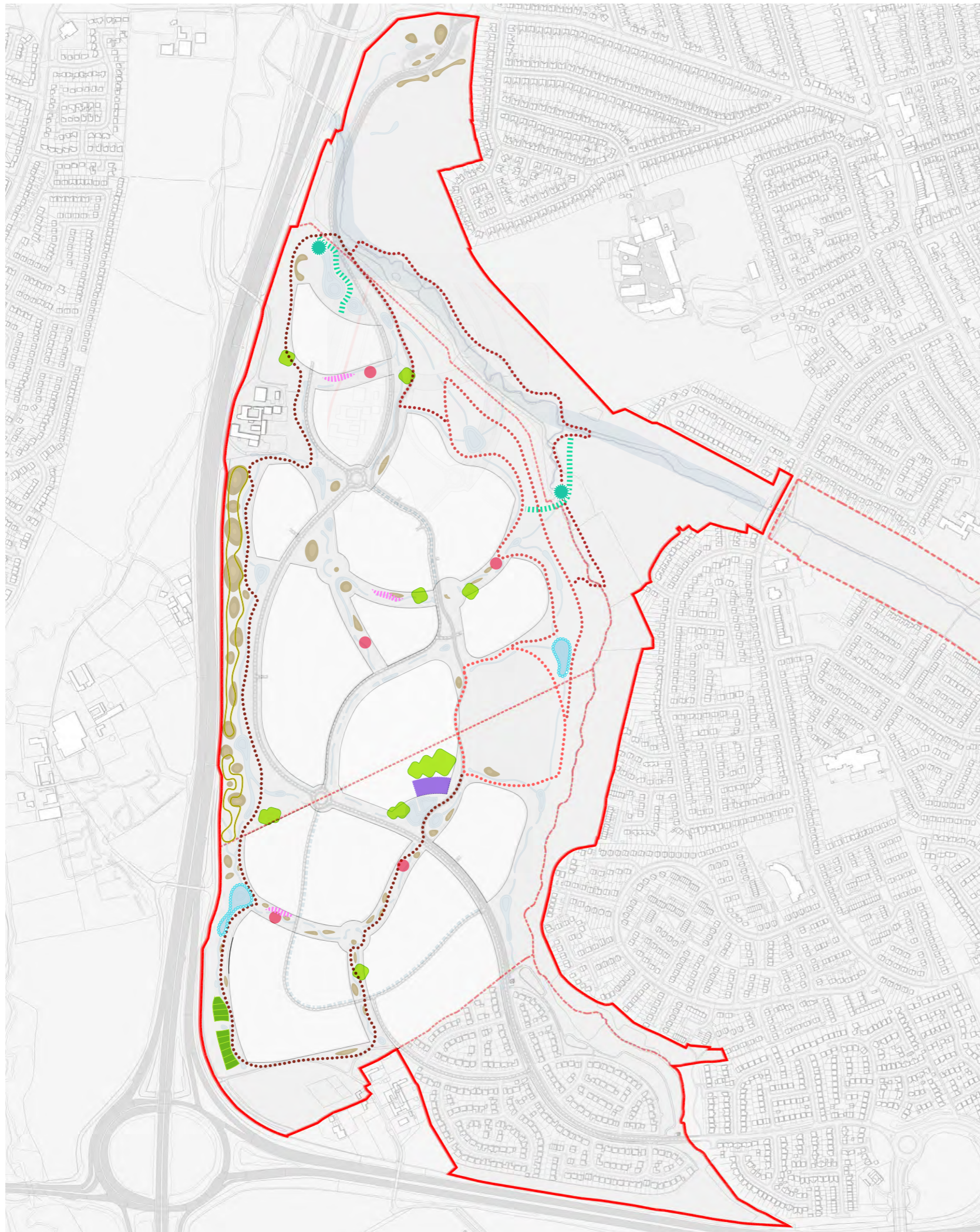
1.8-2.4m Informal Path

Informal paths will provide access to remoter and more wilder parts of the site. The paths will be clearly signed but, less frequently. Artwork will form focal points along the paths as part of the artwork and wayfinding strategy. The path will be formed from compacted gravel. Edges will be close mown.



1.2-1.8m Desire line Path

Desire line paths will be created by the visitor and formalised, often on a temporary basis, by close mowing. The paths will not be surfaced or signed and will informally be created through woodland and meadow as dictated by the users of the park.



4.0 LANDSCAPE

4.7 Informal Amenity Spaces & Features

The Stainsby development and the country park will be punctuated by a number of features and spaces that define character, designate space and create a feeling of quality. These features and areas are intended to enhance the general amenity of the site.

Specifically these include:

- Growing Areas and Food Production Spaces including Orchards
- Decks and Dipping areas specifically interacting with SuDs
- Wildlife Hides and Observation Decks
- Quiet Areas, Seating and ample bins and toilet facilities
- Trim Trails with Distances suitable for a Park run
- Wildlife and Nature Trails suitable for Outdoor Education
- Amenity Cut Grassland suitable for informal recreations & picnics
- Lighting and Fencing at appropriate points creating defensible yet open and safe spaces

Much of the above elements are often forgotten but combine to make a space usable and memorable as a Country Park. The adjacent diagram shows a depiction of how these could be integrated within the masterplan and will be adapted and incorporated into the final proposals.

KEY

-  Landforms created throughout the site to enhance the landscaping and development areas edge conditions and where applicable create separation between the A19 to disperse noise pollution.
-  Bike Tracks - Single / Pump
-  Walking/Running Routes - 0.5 / 1.5 / 2.5 / 3 miles
A variety of difficulties for all users and abilities including wheel chair friend routes. The longest loop also provides a potential future Park run circuit. (Lightest to Darkest colour path = Difficulty gradient from Easiest to Hardest). Further routes are available from the path network created.
-  Trim Trails - activity trails situated within the Green Corridors between developments
-  Wildlife Trails - informal paths linking nature trails with more formal paths
-  Wildlife Hides - positioned to view different habitats created throughout the site
-  Docking / Dipping Ponds
-  Allotments
-  Community Growing positioned throughout the site for local involvement
-  Seating and Bins cited around smaller orchard clusters
-  Community Centre with event space, cafe, public toilets, community growing gardens & composting



4.0 LANDSCAPE

4.8 Play and Sports Provision

Play and Sports areas are to be provided to ensure the new and existing Stainsby population has access to facilities for healthy living from a young age through to late adulthood. The masterplan takes a Fields In Trust approach to providing play areas throughout the development including Local Play Areas (LAP), Local Equipped Play Areas (LEAP), Neighbourhood Play Areas (NEAP) as well as formal playing pitches and supporting pavilion development.

Where practical LAP's and LEAP's are to be accommodated within the green open space network whilst best serving the proposed development areas. Developers are to cater for the needs of the open space requirement throughout their development approaches. The integration and locations of these open play space requirements will be determined throughout the planning stages through discussion and development with the Planning Authority.

The following page expands on the requirements of each of these scale of play spaces that can be integrated at different junctures throughout the masterplan.



KEY POLICY

Play and Sports

CS20 : Green Infrastructure: Development will be required to contribute to the delivery and implementation of this network by, where appropriate, providing green infrastructure that: g) is managed and funded in urban areas to accommodate, and provide for sport and recreation;

4.0 LANDSCAPE

4.9 Local Area for Play (LAP)

The LAP is a small area of open space specifically designated and primarily laid out for very young children to play close to where they live i.e. within 1 minute walking time. The LAP is a doorstep play area by any other name. LAPs are designed to allow for ease of informal observation and supervision and primarily function to encourage informal play and social interaction. The LAP requires no play equipment as such, relying more on demonstrative features indicating that play is positively encouraged.

The main characteristics of a LAP are:

- *It is intended primarily for children up to the age of 6, though it will be used by older children at different times of the day or evening*
- *It is within 1 minute walking time of the child's home*
- *It is best positioned beside a pedestrian route that is well used*
- *It occupies a well-drained, reasonably flat site surfaced with grass or a hard surface*
- *The recommended minimum activity zone is 100 m²*

LAPs form an integral part of the built environment and are regularly distributed throughout the development areas.



LOCAL AREA FOR PLAY (LAP) PRECEDENT
Goldsmith Street, Mikhail Riches © Tim Crocker



LOCAL EQUIPPED AREA FOR PLAY (LEAP) PRECEDENT
Great Land, Corte, Rome © Musacchio Ianniello

4.10 Local Equipped Area for Play (LEAP)

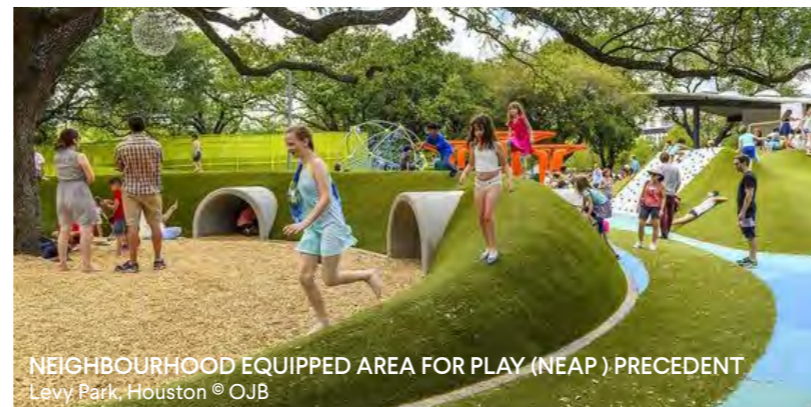
The LEAP is an area of open space specifically designated and laid out with features including equipment for children who are beginning to go out and play independently close to where they live, usually within 5 minutes walking time. Experience has indicated that to provide equipped LEAPs within 5 minutes walk of all houses in a new development can on occasion be impractical and difficult to achieve. An alternative to the LEAP is therefore to provide a Local Landscaped Area for Play.

Play features including equipment are an integral part of the LEAP and the attractiveness of such spaces, though it is also important that the space can be used for physical activity and games. LEAPs can be the place for boisterous activity and therefore it is important to give careful consideration to siting. In summary, if a LEAP is properly sited, equipped, overseen and maintained it can meet the needs of children without being a source of nuisance to other residents.

The main characteristics of a LEAP are:

- *It is intended primarily for children who are beginning to go out and play independently*
- *It is within 5 minutes walking time of the child's home*
- *It is best positioned beside a pedestrian route that is well used*
- *It occupies a well-drained, reasonably flat site surfaced with grass or a hard surface, together with impact absorbing surfaces beneath and around play equipment or structures as appropriate*
- *The recommended minimum activity zone is 400 m²*

Three such LEAPs are indicated on the masterplan, located to provide total coverage of the site, often at the intersections of the green wedges where additional space is provided to accommodate them.



NEIGHBOURHOOD EQUIPPED AREA FOR PLAY (NEAP) PRECEDENT
Levy Park, Houston © OJB

4.11 Neighbourhood Equipped Area for Play (NEAP)

The NEAP is an area of open space specifically designated, laid out and equipped mainly for older children but with play opportunities for younger children as well. Located within 15 minutes walk from home, the NEAP is sufficiently large to enable provision for play opportunities that cannot be provided within a LAP or LEAP. Play equipment is a particularly appropriate form of provision for younger children. As children grow older, towards the latter stages of primary school age, they are looking for different challenges and stimuli. They engage more in wheeled activities and informal ball games, sometimes taken up as formal sport. As they move towards their teenage years, young people increasingly seek out opportunities to meet friends away from home, looking for places to meet socially.

The NEAP can provide a greater variety of opportunity for both active and passive play. It can provide play equipment, and a hard surface area for ball games, or wheeled activities such as roller skating or cycling. It may provide other facilities such as a ramp for skateboarding, a rebound wall, and a shelter for meeting and socialising. The facilities are linked in the one site because children of different ages and abilities like to take part in a range of activities, as do their siblings. Careful consideration should be given to the location and interaction of the different facilities provided both on site and in relation to the local environment. Consultation is a key ingredient of successful design and community acceptance.

The main characteristics of a NEAP are:

- *It is intended primarily for use by older children of relative independence, who have the freedom to range further from home*
- *It is within 15 minutes' walking time of the child's home*
- *It is best positioned beside a pedestrian route that is well used*
- *It occupies a well-drained site, with both grass and hard surfaced areas, together with impact absorbing surfaces beneath and around play equipment or structures as appropriate*
- *The recommended minimum activity zone is 1000 m², comprising an area for play equipment and structures, and a hard-surfaced area of at least 465 m² (the minimum needed to play 5-a-side football)*

The NEAP is located centrally, at the heart of the development, and forms part of the HUB.



4.0 LANDSCAPE

4.12 Signage, Wayfinding and Public Arts

Legibility is a key consideration for the development, either through ensuring traffic is aware of the environment that they are entering, via guiding people on safe, well lit routes through the residential areas to the community hub, or ensuring the extensive country park can be navigated by all. Way markers and signs will provide time and distance to key features such as the community and commercial hub integrated into the proposed path network, with welcoming high quality entrances to the Country Park, clearly marked. Art features and signs will also provide information about the site helping to educate and inform in an interesting and engaging fashion. Waymarking strategies will however be subservient to the surrounding habitats, not seeking to overpower these key local habitats, but rather enhancing the connections within the open space network.

Public Art will interact with the site in a number of ways and at a number of locations to be decided, and will ensure the Country Park has a distinct character. This will work with signage and provide focal points and art trails for communities and visitors alike; providing stimulation and excitement.

Specifically at the community hub public art will provide an inspirational brand image for the development and focal points for orientation outwards to the residential areas, community facilities and the park.

KEY POLICY

Art and Signage

Middlesbrough Design Guide SPD: Legibility, clear definition of public/private realm 3.11 h) incorporating memorable public art at landmark locations; i) reinforcing visual connections along routes with appropriate landscaping, lighting and signage;





Low Native Hedge

Low native hedges can be used when some surveillance is required or space is tight or a tall hedge would be oppressive. They can be kept neatly trimmed when a formal character is appropriate or used in combination with fences in urban areas. Low native hedges can be formed from native species and traditionally laid or from more ornamental species such as beech or holly.



Tall Native Hedge

Tall Native Hedges can be used where screening is required. The hedge can be formed from native species and planted wider and allowed to grow taller to create a wildlife refuge and used to link existing habitats across easements; or in other circumstances when tree planting isn't allowed. The tall hedge can be used to define transitional areas between informal and wild areas and formal and urban areas, or screen wildlife areas from busy play areas or roads.



Estate Fencing

Estate Fencing is 'stock proof' while allowing surveillance. An overhang can deter climbing or jumping. Metal estate fencing can be used in more formal areas.



Riven Oak Fencing

Riven Oak Fencing creates a strong natural or 'rural' feel and can be used as an alternative to metal estate fencing in more 'wild' locations.



Highway verges

The highway verge will be managed as diverse grassland/wildflower meadow, with the appropriate mowing regime and timing of cuts.

The verge directly adjacent the highway can be maintained at an appropriate height for convenience and safety; and to facilitate sight lines.

4.0 LANDSCAPE

4.13 Boundary Conditions to the Country park

The purpose of the boundary is to make the public open space safe for children and animals and create a barrier to urban areas and roads. Strong boundary treatment helps to define the public open space and create a sense of arrival into the country park. A range of boundaries can be used depending on whether screening or surveillance is required and whether a formal and urban character is required or a less formal and rural character is appropriate.

These boundaries will also be integrated with wildlife corridors/crossings to protect wildlife by attempting to provide safe movement in conjunction with the urban strategies.



4.0 LANDSCAPE

4.14 Country Park

“The purpose of a country park is to provide a place that has a natural, rural atmosphere for visitors who do not necessarily want to go out into the wider countryside. Visitors can enjoy a public open space with an informal atmosphere, as opposed to a formal park as might be found in an urban area.”

“A country park usually has some more formal facilities, such as a car park, toilets, maybe a cafe or kiosk, paths and trails, and some information for visitors. Some have much more, with museums, visitor centres, educational facilities, historic buildings, farms, boating, fishing, and other attractions.”

“Many larger country parks organise entertainment for visitors, and are venues for firework displays, shows and fairs and other large, outdoor events.”

The proposed new Country Park is the jewel in the crown of the Stainsby Development and will provide the setting for new homes and is intended to be distinctive and dynamic, encouraging people to interact with it and have a positive influence upon it; and be positively influenced by it. Intertwined with existing habitat and providing new green infrastructure however, it is much more than just a setting and its features and functions have been further developed within the next stages of this masterplan.

Outcomes from public consultation were significant regards the Country Park and these included:

- *Concept of Country Park generally well received*
- *New roads should be kept away from the Country Park*
- *Concern over loss of habitats and green spaces including loss of views*
- *New walking and cycling routes were seen as crucial to the country parks offer*
- *Clarity over the status of and works near Bluebell Beck.*
- *Community Sports facilities well received*
- *Proposed access to green space was well received*
- *Concern over a detrimental impact on wildlife*

The above issues have been further addressed in the design of the Country park via this masterplan with positive elements enhanced and concerns over wildlife addressed via increased habitat areas for example.

Middlesbrough Council intend for the country park to be adopted and to achieve Natural England accredited Country Park Status. The below review intends to demonstrate where these essential elements can be achieved within the country park, as well as the potential integration of the desirable criteria considered within the masterplan. The precise boundary of the Country park will be identified at implementation stage and will be incorporated into the management plan for the park.

Country Park Essential Criteria Design Check

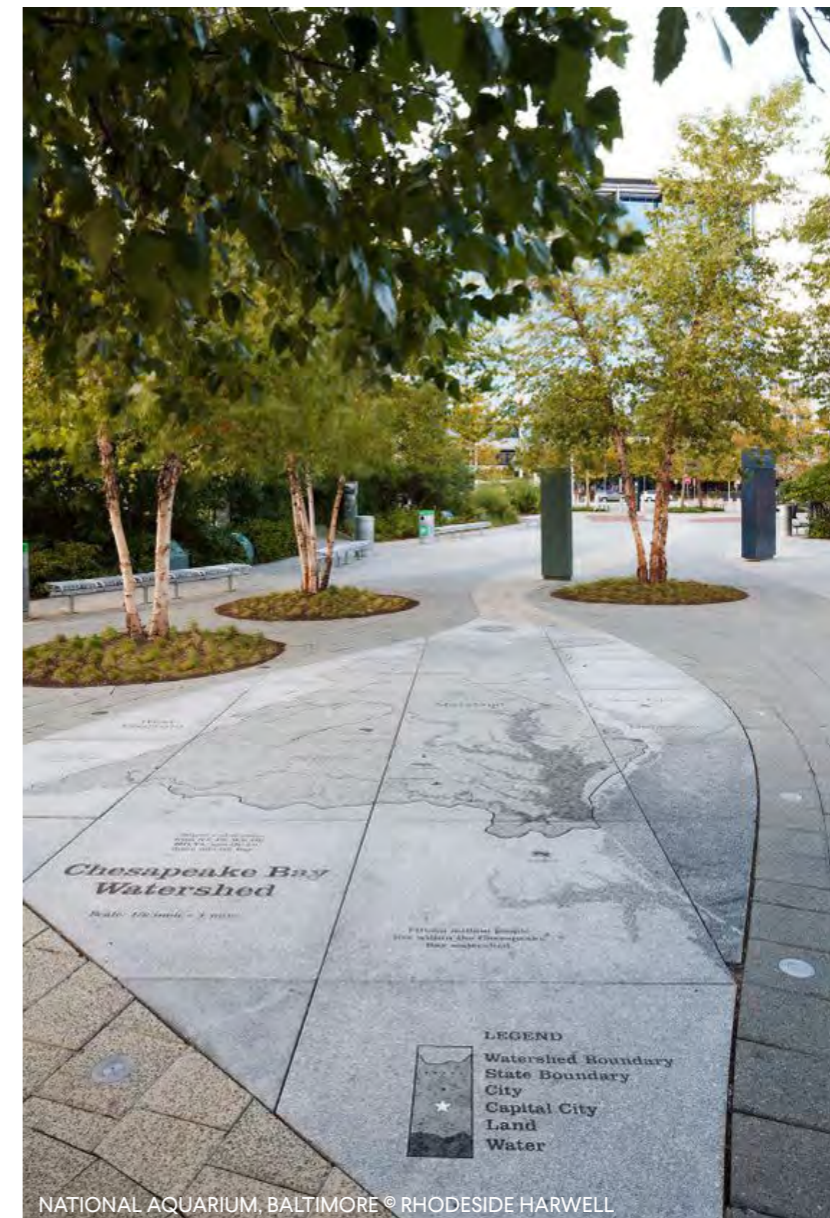
- The Country Park is significantly over 10 ha in size of green space.
- The park will be clearly defined and openly accessible via a number of routes and entrance points, free and easy to enter
- The park will comprise a number of habitats including woodland and community woodland, building on existing, amenity grassland, meadow and wetland habitats. These habitats will also extend into the development
- The country park will have natural legibility with trees and woodlands guiding people into the park as well as specific signage that confirms distances and times to country park features as well as the proposed visitor centre
- The park will be maintained by the local authority with facilities such as toilets at a new community hub, and opportunities for community involvement and events

Country Park Desirable Criteria Design Opportunities

- A new commercial but also community hub and visitor centre is designed into the masterplan, at a key location which will connect to the country park with a permanent staff and visitor facilities. This hub building is intended to be multi-functional with opportunities for a range of adaptable uses.
- The visitor centre is intended to form the backdrop and initial setting for the county park with the community hub blurring the lines between park and commercial activity, including food and drink opportunities as well as activity events, use of the water feature for outdoor events and continued community involvement and volunteering.
- The masterplan has integrated a number of multi use trails, some of which would be acceptable for horses as well as other users, all with a legibility that guides people into the country park via specific points of public art as well as signage showing times and distances for walks for all.
- The above points ensure that the park is working towards a Green Flag Status as a park. The below points guide developers as to how to achieve green flag status right from the off, providing a quality space from the point of development.



Riverside Lünen, WBP Landschaftsarchitekten © Claudia Dreyfge



NATIONAL AQUARIUM, BALTIMORE © RHODESIDE HARWELL

4.0 LANDSCAPE

4.15 Achieving Green Flag Status for Stainsby

The Country Park from its fruition should be ambitious in its function and form. It is intended via this masterplan that the park should from the beginning seek to achieve green flag status and be designed accordingly within formal planning applications.

The below details are taken from the Green Flag Guidance Manual and are relevant to the future design of the Country park within the development.

A Welcoming Place

- + *Well considered and innovative, yet practical, design features can really encourage people to enjoy using the site. Incorporate elements such as interesting planting, varied textures, and natural and built features that can be explored in play and used for relaxation*
- + *Presence of clear sightlines in and out, and welcoming entrances*
- + *Public transport links and whether they can be improved*
- + *Pedestrian routes – whether they are logical, useful and suitable for the whole range of users. Are cycle routes designed to be complimentary and minimise conflict?*
- + *Vehicles on site (including service vehicles), appropriate signage, control and safety measures, including how shared access between vehicles and pedestrians is managed*
- + *Car parking – if provided, appropriate provision for the quantity and range of visitors*
- + *Equality of access including disabled access – the site should adhere to relevant national legislation*
- + *Sites attracting visitors from a distance may benefit from installing signs from major routes; for others with mostly local and repeat visitors a sign at or near the entrance is enough; for some rural site*

Healthy, Safe and Secure

Healthy facilities and activities can include:

- + *Play and exercise equipment, trim trails, active volunteering programmes, health and fitness activities and suitable sporting facilities*
- + *Provision of seating – for contemplation, physical rest, solitude, and enjoyment of nature*

Biodiversity Landscape and Heritage

The overall country park and its elements should be designed in such a way to consider:

- + *Potential for sites to form part of a network for wildlife, as natural floodways or open spaces, to buffer and enhance*
- + *The presence of any ancient trees, or historic tree or plant collections and how they are identified, managed and promoted*
- + *Local historical or social links with types of biodiversity or particular habitats*
- + *Links to wider local and national strategies – including Local Nature Partnerships, National Pollinator Strategy, health and wellbeing and nature, natural play, forest schools, involving people in ‘growing their own’, green infrastructure and climate change adaptation*



































4.0 LANDSCAPE

4.16 Summary of Landscape Strategy

All the elements of Landscape Strategy are devised to collectively meet the criteria for a Green Flag status Country Park, that surrounds and pervades the proposal and forms the heart of the development.

The adjacent diagram shows an indicative approach as to how all of the Landscape principles established could be integrated and work together throughout the Stainsby site.

KEY

-  Existing Watercourses
-  Flood Zones
-  SuDs Attenuation
-  SuDs Conveyancing
-  Rain Gardens
-  Existing Woodland
-  Hedgerows
-  Proposed Woodland
-  Avenue Planting
-  Orchards
-  Parkland Trees
-  Amenity Grassland / Sports pitch
-  Retained existing meadow habitats
-  Grassland
-  4.5m Strategic Segregated Pedestrian & Cycle
-  3.5m Mixed Use Path (Indicative)
-  3.5m Compacted Gravel Paths (Indicative)
-  Site Entrance Points
-  Network of informal/formal paths (connections between existing habitats and open space network)
-  Formal Sports Provision
-  Multi-Use Games Area / Courts
-  Landforms
-  Bike Tracks - Single / Pump
-  Walking/Running Routes - 0.5 / 1.5 / 2.5 / 3 miles
-  Trim Trails
-  Wildlife Trails
-  Wildlife Hides
-  Docking / Dipping Ponds
-  Allotments
-  Community Growing
-  Seating and Bins
-  Community Centre



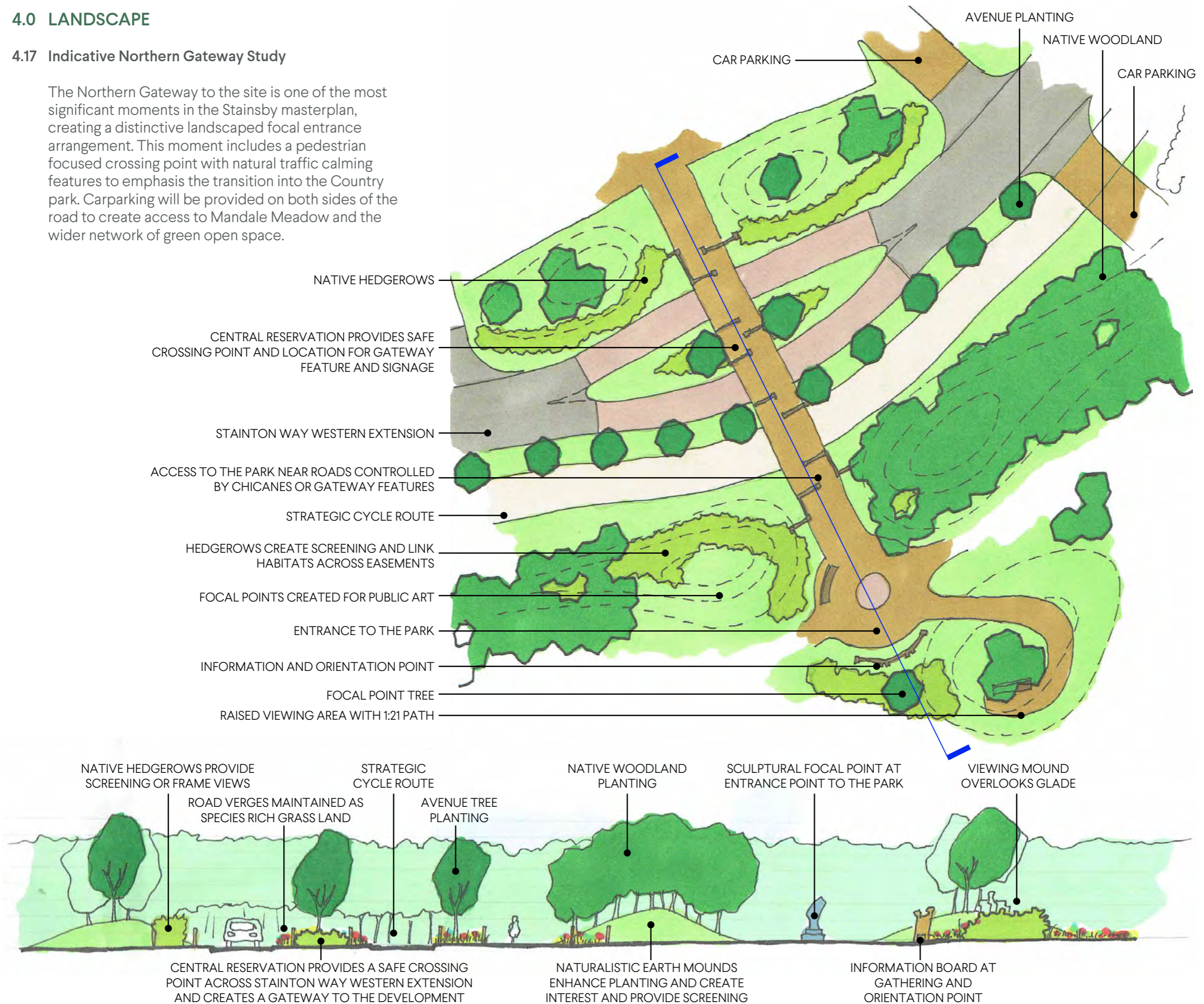
LANDSCAPE STUDIES



4.0 LANDSCAPE

4.17 Indicative Northern Gateway Study

The Northern Gateway to the site is one of the most significant moments in the Stainsby masterplan, creating a distinctive landscaped focal entrance arrangement. This moment includes a pedestrian focused crossing point with natural traffic calming features to emphasis the transition into the Country park. Carparking will be provided on both sides of the road to create access to Mandale Meadow and the wider network of green open space.



NEW NATIVE WOODLAND PLANTING FRAMES AND DEFINES WALKING/RUNNING TRACKS THROUGHOUT THE SITE OPENING OUT INTO GLADES FOR AMENITY USE

3.5m MULTI USE PATH

PRIMARY ROAD PEDESTRIAN CROSSING POINT

INFORMAL PATH

WILDLIFE VIEWPOINT OVERLOOKING SUDS WETLAND

NATIVE HEDGEROW SCREENS AND PROTECTS WILDLIFE AREA

STRATEGIC CYCLE WAY

LEAP - INCORPORATES FORMAL AND INFORMAL PLAY WITH CREATIVE / NATURAL PLAY ELEMENTS

SUDS WETLAND; POND AND MARGINAL HABITATS SUCH AS REED BEDS, BOGS, & MARSH ATTENUATES AND TREATS WATER SURFACE

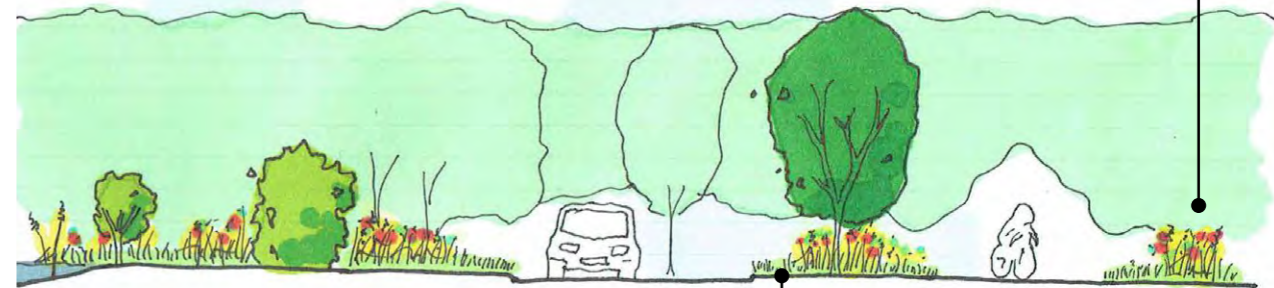
STAINTON WAY WESTERN EXTENSION

WILDLIFE ISLAND (OR FLOATING ISLAND)

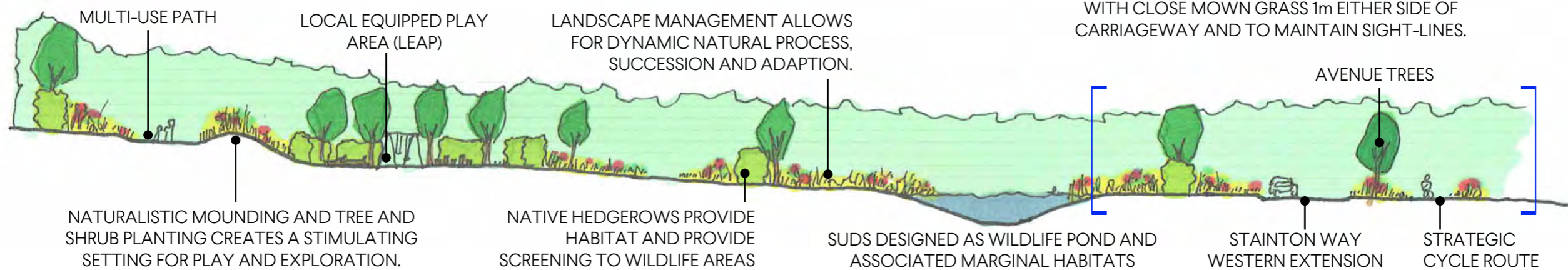
BOUNDARIES TO COUNTRY PARK WILL BE DELINEATED BY NATIVE HEDGEROWS, WHERE SCREENING OR SHELTER IS REQUIRED, OR ESTATE / RIVEN OAK FENCING WHERE SURVEILLANCE IS REQUIRED.

FORMAL PLANTING DEFINES LEAP CREATING CHARACTER INSTANCES

MOUNDING CREATES INFORMAL LANDSCAPE FEATURE THAT CAN BE INTEGRATED INTO LEAP



ROAD VERGE MAINTAINED AS SPECIES RICH GRASSLAND WITH CLOSE MOWN GRASS 1m EITHER SIDE OF CARRIAGEWAY AND TO MAINTAIN SIGHT-LINES.



MULTI-USE PATH

LOCAL EQUIPPED PLAY AREA (LEAP)

LANDSCAPE MANAGEMENT ALLOWS FOR DYNAMIC NATURAL PROCESS, SUCCESSION AND ADAPTION.

AVENUE TREES

NATURALISTIC MOUNDING AND TREE AND SHRUB PLANTING CREATES A STIMULATING SETTING FOR PLAY AND EXPLORATION.

NATIVE HEDGEROWS PROVIDE HABITAT AND PROVIDE SCREENING TO WILDLIFE AREAS

SUDS DESIGNED AS WILDLIFE POND AND ASSOCIATED MARGINAL HABITATS

STAINTON WAY WESTERN EXTENSION

STRATEGIC CYCLE ROUTE

4.0 LANDSCAPE

4.18 Indicative LEAP and SUD

A typical example of a SUDs zone adjacent to a Locally equipped area for play. This example is situated at the Northern extent of development area adjacent to the Primary Road. It shows how naturalistic features can be utilised to create screening for public safety and to enhance wildlife and ecology.

4.0 LANDSCAPE

4.19 Indicative LEAP Node & Green Corridor Study

A typical example of how a landscaping corridor between developments might intersect a LEAP node between developments. These extensive corridors define naturalistic routes between the development zones in line with the SUDs routes, drawing the public through the site and around congregation and play points.

A RANGE OF BOUNDARY FEATURES WILL BE USED TO DELINEATE THE BOUNDARY OF THE COUNTRY PARK, INCLUDING THE GREEN WEDGES; SUCH AS ESTATE TYPE METAL FENCING, BESPOKE FENCING, HEDGEROWS AND RIVEN OAK FENCING DEPENDING ON WHETHER VISIBILITY IS REQUIRED.

RAISED TABLE CROSSING
PEDESTRIAN PRIORITY

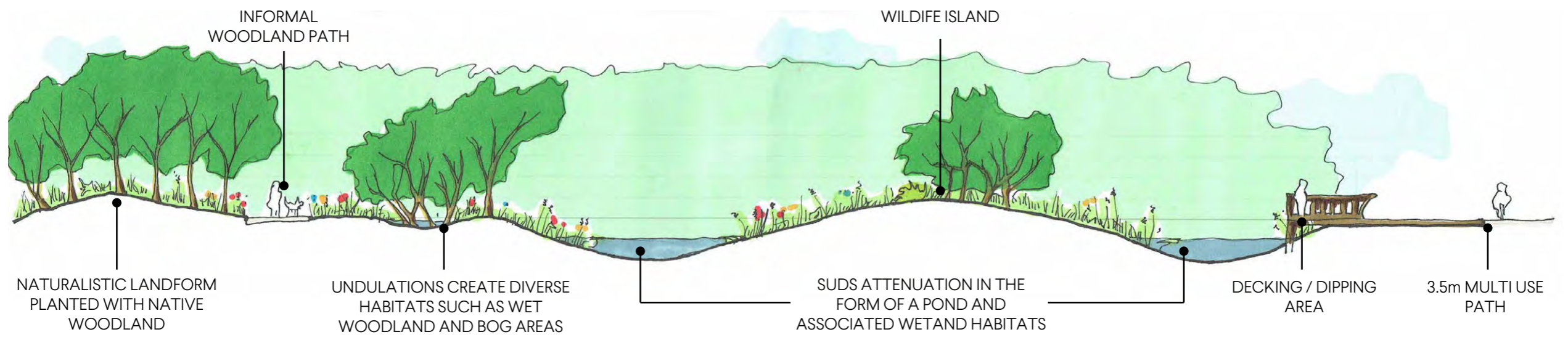




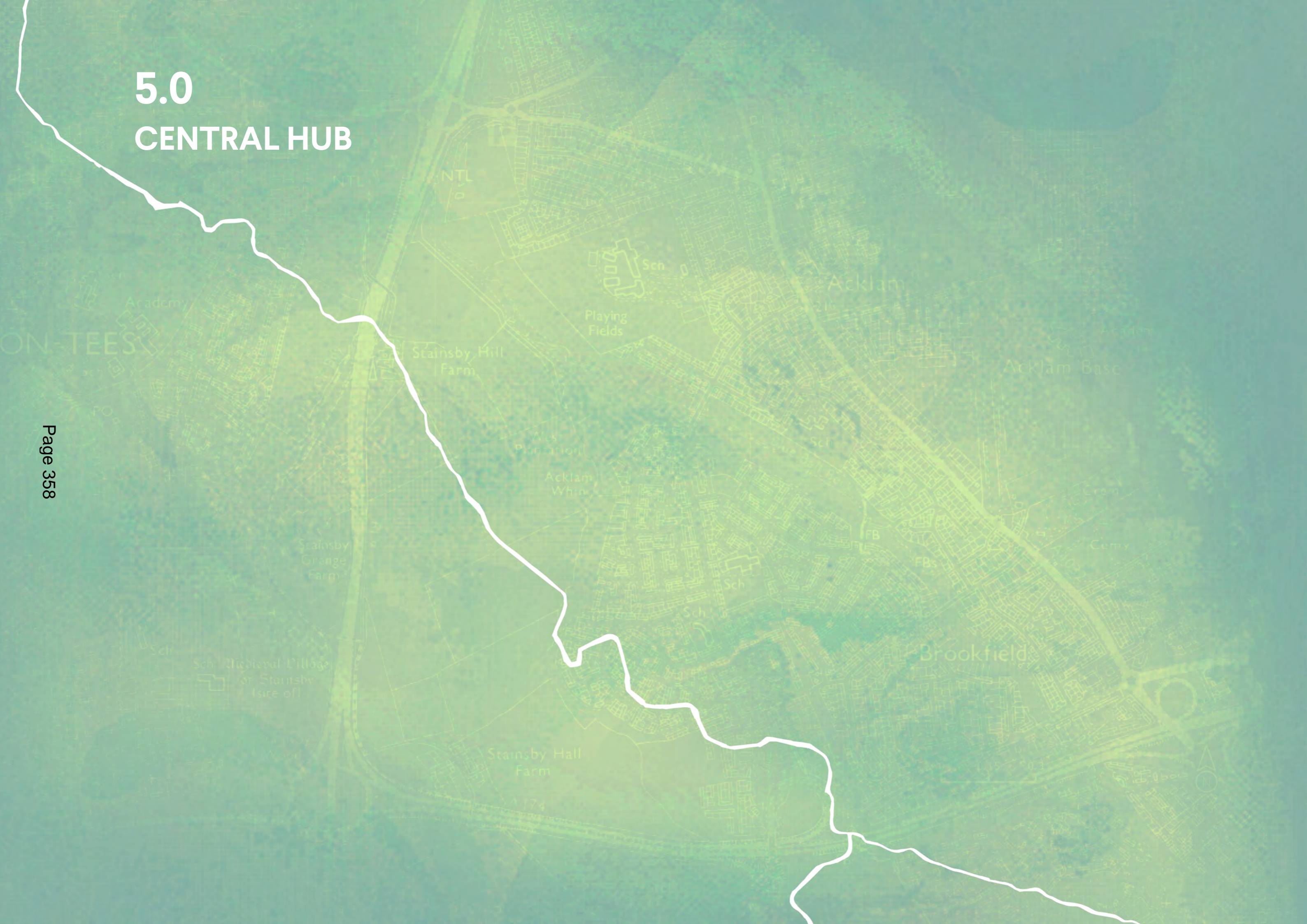
4.0 LANDSCAPE

4.20 Indicative SuD and Green Corridor Study

A typical example of how a landscaped corridor might meet the public open space at the perimeter of the site. These corridors provide opportunities to be lead into the wider network of the site. The landscaped edges will provide acoustic and visual separation to the surrounding road networks, protecting the character of the site. These wider networks can be used as dog walking, running or cycling tracks, with all routes leading to the wider site with clear access to the development areas.



5.0 CENTRAL HUB





JULES VERNE SCHOOL, ARCHI5 © Sergio Grazia



MAISON DE SANTÉ BERNARD QUIROT ARCHITECTE © Luc Boegly



Chrysalis Childcare Centre © Collingridge And Smith Architects



Gekko, Moke Architecten © Thijs Wolzak



Bezons Angela Davis School / archi5 + Tecnova Architecture © Sergio Grazia



Hiribarren-Gonzalez + Estudio Urgari © EGOIN



Kurve 7, Stu/D/O © Stu/D/O, Ketsiree Wongwan



SAHID KUTA LIFESTYLE RESORT © PT ENVIRO TEC INDONESIA

5.0 CENTRAL HUB

5.1 An Aspirational Community Facility

As proposed in Section 4.13 Country Park, a 'Central Hub' will be a highly desirable attribute to support the achievement of Country Park Status.

The site as designated sits at the centre of the proposed masterplan, providing the most accessible location for this bridge and gateway between the Landscape and the Urban.

This community focused area will seek to meet the requirements for the Country Park and any other commercial and educational needs for the site. These expanded needs will be subject to further development with Middlesbrough Council following detailed design assessments.

An aspirational centre is therefore a high requirement focusing on naturalistic materials in the context of a country park, defining spaces that focus on the surrounding park and enhancing the setting. The adjacent precedents give a taste of high quality design and interfaces between indoor and outdoor spaces.



5.0 CENTRAL HUB

5.2 Education Facility

Initial requirements for a primary school provided by Middlesbrough Council have been assessed and organised to understand the potential scale and massing required with the associated external curriculum and servicing areas to be located within the Central Hub. BB103, or the most current government guidance, should be adhered to when defining site areas for future Educational facilities.

A key driver for the future design of the school is how the building could be used to transition from urban areas into parkland, creating a suburban facility within a parkland setting. A sharing of community and education facilities may enable an efficient design solution. Habitat could be used to create buffers between the surrounding road network, also following BB103 guidances for area provisions for external areas.

SCHEDULE OF ACCOMMODATION TOOL FOR ANY MAINSTREAM PRIMARY SCHOOL												SoA tool 7.3 Mar 2018	
date		age range		school name		as a check, if new:							
24/6/19		3-11		420 place Primary School - Example									
Parent ADS Code	Final ADS Code	reception places		classes		net capacity		site: area (m ²)		net capacity			
		2	60	2	4	for SoA below = 420	type	ample site	for recommended				
60		30	120	8	4	within a potential range of:	number of storeys:	single storey	SoA below:	420			
120		39	240	14	8	378 to 420	existing buildings to be:	none (all new)	378 to	420			
240		Total Mainstream Places		420		4 to 11 places		8 float if min net		not over min gross			
78		Additionally resourced FTE places for:		max. group size		average area of space (m ²)		TOTAL AREA (m ²)		NON-NET AREA (m ²)			
		aged 2 to 3 nursery FTE		-		TOTAL AREA (m ²)		SUPP AREA (m ²)		organisation options for:			
		- SEN		-		-		-		infant			
		-		-		-		-		junior			
		-		-		-		-		specialist practical spaces			
		-		-		-		-		area of space (m ²)			
		-		-		-		-		no. of spaces			
Basic Teaching Area													
classrooms or classbases/ shared teaching													
						(16)							
PRI03	PRI03	nursery playroom	42	83	2	166		79 m2 or 2.3m2 per place min recom'd	83	2			
PRI13	PRI13	reception classroom	30	62	2	124		62 m2 minimum recommended	62	2			
PRI23	PRI25	extensive infant classroom	30	62	4	248		62 m2 minimum recommended	62	4			
PRI33	PRI33	junior classroom (with sink)	30	55	8	440		55 m2 minimum recommended	55	8			
specialist practical/ other													
PRA12	PRA12	food/ science/ DT area art/ DT area	30	62	1	62			62	1			
TOTAL AREA BB103 range 1019 to 1153													
OK: area within recommended range													
Large spaces: halls, studios and dining													
HAL13	HAL13	main hall (primary)	330	180	1	180		dining options	60 mins all in main hall	180	1		
HAL11	HAL11	studio	30	55	1	55		105 m ² min recom'd for all pupils dining	55	1			
		small hall	30	80					80				
TOTAL AREA BB103 range 226 to 272													
OK: area within recommended range													
Learning Resource Areas													
LIB01	LIB01	library (primary)	20	34	1	34		30 m ² minimum recommended	34	1			
SEN11	SEN11	SEN therapy/ MI room	5	12	1	12		12 m2 min recommended	12	1			
SEN01	SEN01	SEN resource base	5	12	1	12		group room suitable for SEN/ multi-agency	12	1			
RES00	RES02	small group room	4	9	3	27			9	3			
RES00	RES01	small group room (nursery)	4	9	3	27			9	3			
TOTAL AREA BB103 range 60 to 130													
OK: area within recommended range													
Staff and Administration Areas													
OFF31	OFF31	staff room (prep and social)	22	41	1	41		54 m ² min total staff area recom'd	41	1			
OFF33	OFF33	staff work room	4	13	1	13			13	1			
ADM11	ADM11	head's office (meeting room)	6	16	1	16			16	1			
OFF10	OFF10	office/ meeting room	3	9	1	9			9	1			
OFF35	OFF35	staff work room (with sink)	6	16	1	16		16 m2 recom'd for nursery	16	1			
ADM05	ADM05	general office (1 recep desk)	4	20	1	20			20	1			
ADM08	ADM08	reprographics room	-	11	1	11			11	1			
ADM31	ADM31	entrance/ reception (50% circ)	-	10	1	5	5	5 m ² non-net circulation included adjacent to entrance/reception	5	5	1		
ADM02	ADM02	interview room	4	6	1	6		semi-open area adjacent to reception for preparing snacks and drinks for pupils	6	1			
ADM03	ADM03	sick bay	2	4	1	4			4	1			
ADM22	ADM22	kitchenette bay (nursery)	1	4	1	4			4	1			
TOTAL AREA BB103 range 130 to 199													
OK: area within recommended range													
Storage													
STT00	STT01	teaching store (off nursery)		4	3	12		furniture store options	store used as servery zone	4	3		
STT00	STT02	teaching store (off reception)		3	2	6				3	2		
STT00	STT03	teaching store (off infants and juniors)		1.5	12	18.0				1.5	12		
STT05	STT05	specialist store (food, sci, DT)		5	1	5				5	1		
STH00	STH04	PE store(s) (off hall)		18	1	18		18 m ² total recom'd		18	1		
STH05	STH05	external PE store		8				6 m ² total recom'd		8			
STH15	STH15	secure/ exam/ archive store		4	1	4				4	1		
CIR06	CIR06	wheelchair/ appliances bay(s)		14	1	14		14 m ² needed to store all chairs & tables		14	1		
STN01	STN01	personal storage (coat hooks)		8						8			
STN11	STN11	cloakroom (early years, 50% circ)		1.5	2	3.0		wheelchair/ appliance bay(s) recommended		1.5	2		
STN31	STN31	cleaners' store(s)		3.0	12	36.0		OK		3.0	12		
STN32	STN32	general store (stock/ maintenance)		1.5	3	4.5	12	6 m ² non-net circulation incl in each		1.5	3		
TOTAL AREA BB103 range 95 to 165													
OK: area within recommended range													
Float 0 to 155													
NOTE: float available 8													
Total Net Area min 1684 max 1819													
OK													
Non-net Area													
KIT02	KIT02	kitchen prep/ servery suite		58	1	58		(pupil toilets: 29 recom'd 30 provided)		58	1		
KIT20	KIT21	kitchen dry store		3	1	3		54 m ² min recom'd for full service		3	1		
KIT40	KIT40	kitchen toilet/ changing area	2	3.5	1	3.5		63.5 m ² min recom'd for whole kitchen		3.5	1		
TOC30	TOC31	supervisable toilets (early yrs)	1	3	14	42		14 if 1 per 9.9 early years places		3	14		
TOC10	TOC14	other pupil toilet suite(s)	1	3						3			
TOC06	TOC06	small hygiene room (2 doors)	5	15	3	45		15 if 1 per 20 KS1 & 2 places		15	3		
			1	9	1	9		9 m ² minimum if ceiling mounted hoist		9	1		
			1							12	0		
TOC10	TOC12	staff toilet suite(s)	2	6	1	6		including lobby with coat hooks		6	1		
TOC21	TOC21	accessible/ staff toilet	1	3.5	1	3.5		also for visitors and staff		3.5	1		
plant indicative area, as %age of net area: for new:													
PLA01	PLA01	central plant room		3.1%	incl ICT hubs and risers					3.1%			
PLA05	PLA05	ventilation and other plant		32	1	32		32 m ² minimum recom'd in new build		32	1		
		distribution boards						Contractor to add area if provided		0			
		risers, flues and vertical ducts		1	4	4		Contractor to add vent ducts if provided		1	4		
PLA10	PLA12	server room (2 cabinets)		9.5	1	9.5				9.5	1		
PLA10	PLA18	ICT hub(s)		6.8	1	6.8				6.8	1		
circulation as percentage of net area: for new:													
		stairwell(s)		24.3%	incl circ noted above	(17)				(17)	24.3%		
		lift						20.0% of net min circulation for 1 storey		0	0		
								24.3% of new build net area recommended		0	0		
CIR01		main circulation remaining		393		393		15% of net min rec'd for horizontal circulation		393			
		partitions		4.5%	for new build	76				76	4.5%		
TOTAL AREA BB103 range 690 to 758													
OK: area within recommended range													
Total Gross Area min 2392 max 2641													
OK													
Total Gross Area (including supplementary area) 2392 of which 0 retained, so: gross area to be built 2392 m ²													
Gross area as proportion of net 142%													



5.0 CENTRAL HUB

5.3 Central Hub Design

A concept organisation diagram for the central hub is shown adjacent.

The hub is perceived as the fulcrum of the development, creating a community heart and sense of place. Retaining and promoting views and connectivity are key principles of the central hub.

The hub is ideally located to serve all local residences providing education, retail, amenities and appropriate built infrastructure to the Country Park. A transport 'super-stop' will be strategically placed to promote high-quality sustainable travel featuring real time displays for transport links and cycle parking for door to door connections.

Approaching the hub from the south, with development areas to both sides, culminates in a residential gateway. A view could then open up to the setting of the central hub within parkland bleeding into the development from the west and permeating through to the east. A visitor centre should be considered a key building within the development underlining the Country Park. The relationship of this building to Sustainable Drainage should be considered to strengthen the setting.

The hub is to be set within a tree lined setting, removing the dominance of the vehicles from the public realm and emphasising the importance of buildings offering frontage to both urban and parkland areas. A suggested viewing corridor is set up running east-west through The Hub, narrowing down to form a sheltered community focal area but emphasising the connection to landscape.

The educational facility should provide active frontage to both the urban edge of The Hub and a softer connection the east allowing the external areas to connect visually with the park to the east. Eliminating rear conditions is of primary importance to create a pleasant environment which is animated, surveyed and passively policed for safety and enjoyment.

Parking areas should be positioned for convenience to promote frequent use but are to be diluted with the inclusion of landscaped areas at a ratio of approximately one space to five car parking spaces. Integration of electric vehicle charging provision is encouraged as part of the local facilities (including at the Mandale Road country park entrance car parks).

As gateway to the county Park, The Hub buildings should be a permeable design, articulated with natural and organic textures and surfaces.



URBANISME © LOUKAT

KEY POLICY

Retail & Community Hub

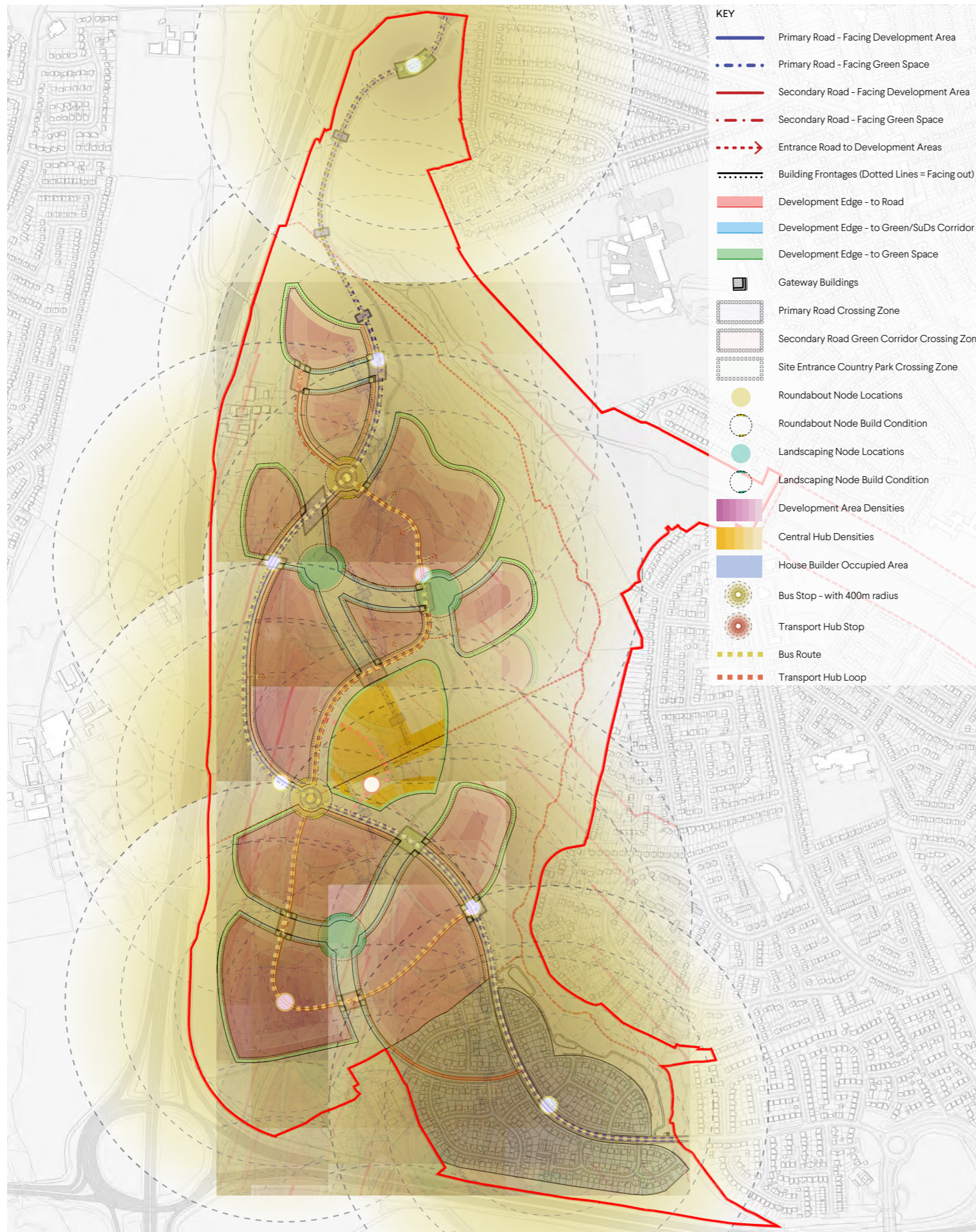
NPPF Para 91 c) enable and support healthy lifestyles, especially where this would address identified local health and well-being needs – for example through the provision of safe and accessible green infrastructure, sports facilities, local shops, access to healthier food, allotments and layouts that encourage walking and cycling.



6.0

URBAN STRATEGY





6.0 URBAN STRATEGY

6.0 Introduction

The Urban Strategy is made up of components which combine to formulate a masterplanning strategy that directly relates to the context of a Country Park, devising hierarchies and strategies to define new local identities for each development area. The key aim here is to create spaces that continually link to the wider green open space network and country park, prioritising the public pedestrian movement over vehicular access.

This culminates in a series of Urban strategies to help create a framework to develop within towards this goal. The adjacent diagram shows the combined layers of these urban strategies that will get unpacked in this section. These principles will then be expanded upon in Section 7.0 Urban Strategy Studies in order to create a guideline for options that are applicable in different situations.

The following principles will be introduced within this section:

6.1 Roads / Circulation and Access

6.2 Highway Design

6.3 Bus Linkages

6.4 Adopted Paths & Artificial Lighting Strategy

6.5 Crossing Points

6.6 Housing Mix & Layout

6.7 Parking

6.8 Garages

6.9 Garage Type Matrix

6.10 Boundary Treatments

6.11 Boundary Treatment Indicative Study



6.0 URBAN STRATEGY

6.1 Roads / Circulation and Access

Roads within the site must be designed with a hierarchy of highways which become more informal as they step down/get further into the development. 'Manual for Streets 2' should be used as a basis to inform the internal layout. Wherever possible the impact of roads should be minimised and methods to naturally encourage slower vehicle speeds must be incorporated into the road design. The whole development must have a maximum design speed of 30mph.

The following road types have been developed to inform a network hierarchies throughout the site. Further design details of road compositions can be found in Section 7.1 Urban Strategy Studies - Roads.

Primary

The Primary road network will be designed to naturally restrain vehicle speeds to an absolute maximum of 30mph. Measures will include regular gateway/crossing features (which incorporate pedestrian/cycle crossing points), route alignment and the strategic placing of junctions to reduce the length of straight sections of carriageway. The landscaping strategy will contribute towards slowing traffic to natural visibility levels.

The primary route, also known as the Stainton Way Western Extension, will run north-south through the Stainsby site, providing vehicular access to the development as a whole as well as improving the future resilience of the local road network. The width of the Primary carriageway linking Jack Simon Way (B1380 Low Lane and A1130 North Road) is proposed to be 7.3m, with localised widening at junctions.

No house plots should be accessed directly from a Primary road, however building frontages are permitted in specific instances (see further guidance within Design Code) with pedestrian access facing the road and landscape borders only.

For Bus connectivity see section 6.3 Bus Linkages.

Secondary

Secondary roads will also be designed to naturally restrain vehicle speeds to an absolute maximum of 30mph. This road type will be utilised from the Primary Road structure leading key routes through the development areas. This should always be a through route leading directly back to a Primary Road or Roundabout junction. These roads permit

direct access to housing, see Studies for further conditioning. Bus stops will also be integrated into this typology providing local access (see section 6.3)

Tertiary

Tertiary roads are a smaller road limited to 20mph only accessible from Secondary Roads. These provide access into the heart of development areas creating through links back to Secondary roads or loop routes depending on the application area. These routes don't contain a multi use path creating a more local hierarchy, and don't allow for bus access. Local Area Play can therefore be hosted along these routes, which also cater for access to Homes Zone and Private Drives.

Homezone

Homezone areas may be created where low volume traffic is expected, typically within the centre of residential clusters accessed directly from Tertiary Roads. The principles are to create a high quality shared surface area that prioritises pedestrians and serves no more than 15 dwellings to create a balance between the local community and drivers.

These spaces are limited to 10mph and the entrances must be defined by a raised access level with varied texture to enforce low speeds. Intersections with Tertiary Roads are to be framed in pedestrian footpath surface material to instil a pedestrian hierarchy over vehicular. Centrally a change in surface material and colour is to be provided and road markings removed to warn motorist of the change in the perception of the road. Within Home Zone areas the road and pavement areas are one level surface in a different material and colour to the surrounding road networks to emphasise the difference of use. Local Area Play can also be utilised in this areas.

This typology can be used to create access to areas adjacent to Primary Roads or Green Edges.

Private Drive

Private Drives are again a shared surface that can define a pedestrian walkway to oneside. The number of dwellings served by these access drives can be discussed with Middlesbrough Council Planning department during detailed design. Private drives can be utilised from Tertiary roads or Home Zones and can also be utilised for access to areas adjacent to Primary Roads or Green edges. These spaces should be in an alternative material and also colour to the surrounding road network. They can use the same material as a Home Zone so long as they are in an alternative colour.

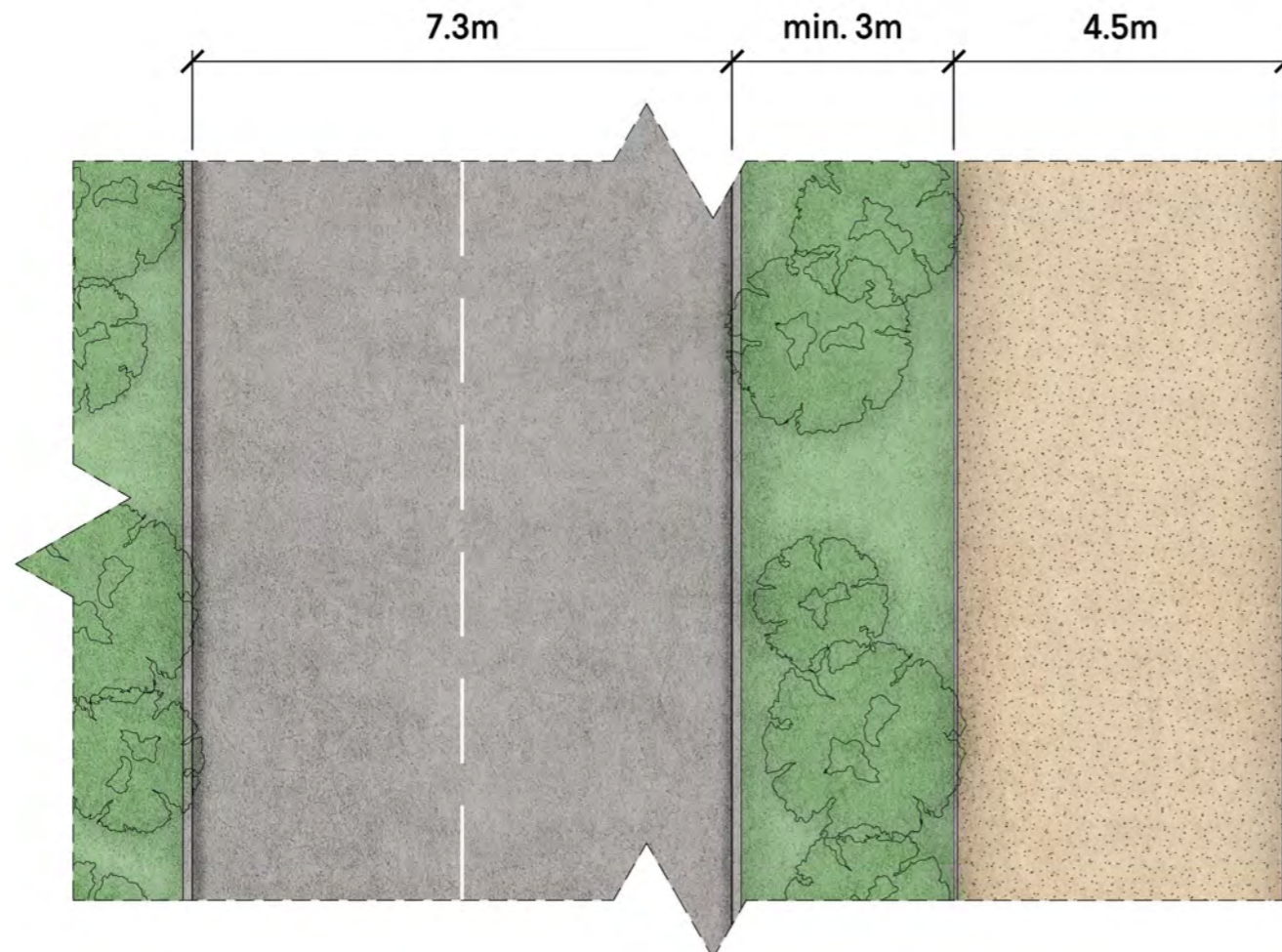


6.0 URBAN STRATEGY

6.2 Highway Design

The following road build ups highlight the design intention based on their hierarchy. They explore the principles of composition, and are indicative at this stage. The Primary, Secondary and Tertiary roads will be created as adoptable highways, whilst the Home Zone and Private drives are defined for placemaking principles.

All designs will be developed in conjunction with Highway Designers & Transport Engineers to ensure quality for the future.

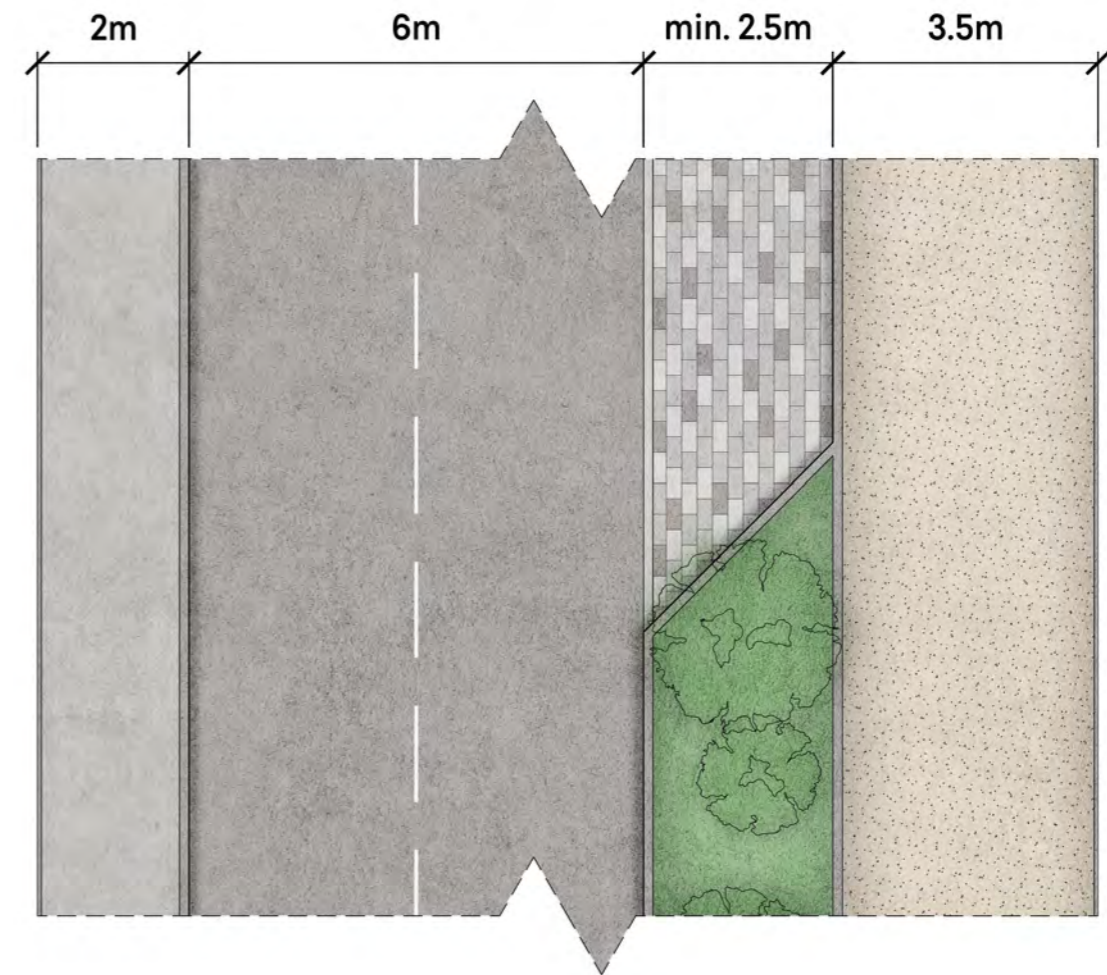


6.2.1 Primary Road

The 7.3m primary road will generally be bordered on both sides by a landscaped zone of a minimum 3m offset. A varied landscaping strategy will be used throughout to define characterised area identities. Whilst shielding the Primary road from development areas it will also form part of the natural traffic calming features to ensure the whole site remains safe and access is predominantly focused on the residential zones set within the Country park.

For the most part the Strategic cycle route will follow the Primary road creating connection from the North to South of the site. See Section 6.4 Adopted Paths for the Strategic Cycle Route.

A durable tarmac surface will be used for this adopted Primary road. Further detailed design will be confirmed by Highway Designers & Transport Engineers to adopted standards.

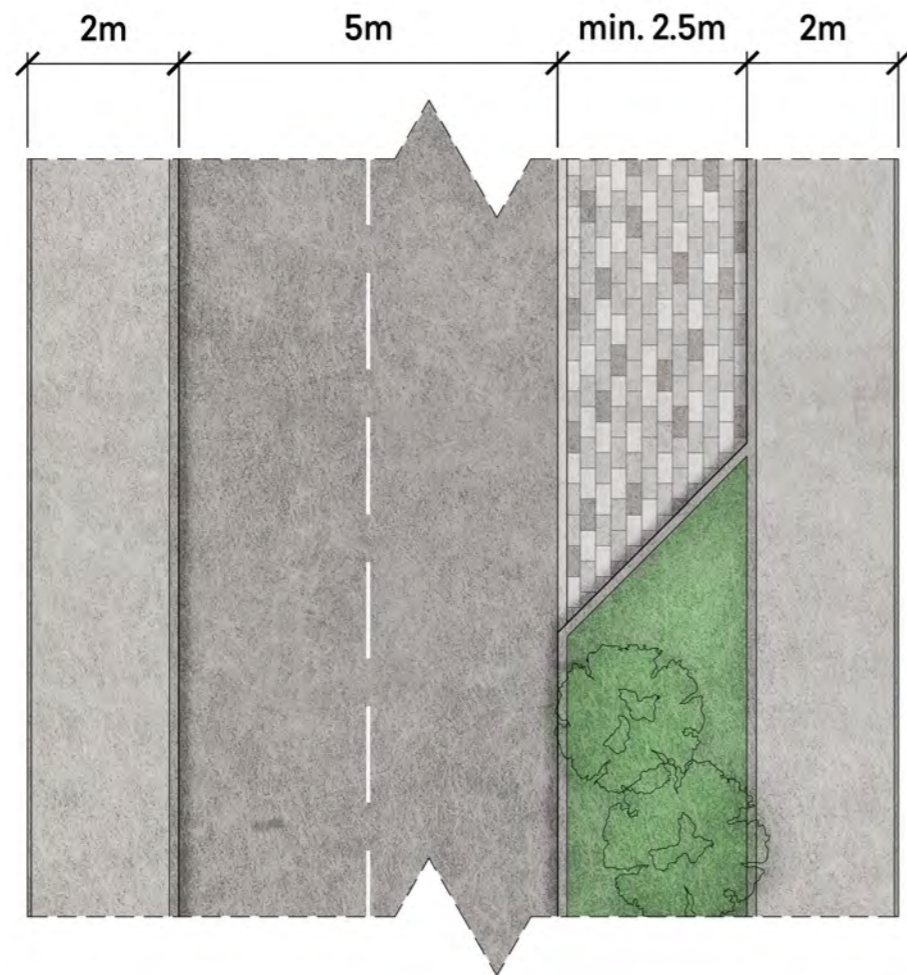


6.2.2 Secondary Road

The secondary road will be comprised of a 6m carriageway cradled to one side by a landscaped border or varying dimensions through the site. At a min 2.5m, the border can host the visitor car parking spaces. This will be delineated in a different material to demark them as separate to the main thoroughfare.

A multi use path will also be adjacent to the landscaped border providing connectivity throughout the site. Whilst a standard pavement will be utilised on the opposite side of the carriageway.

This adopted highway composition will meet Highway specifications and is subject to further develop with the relevant stakeholders.

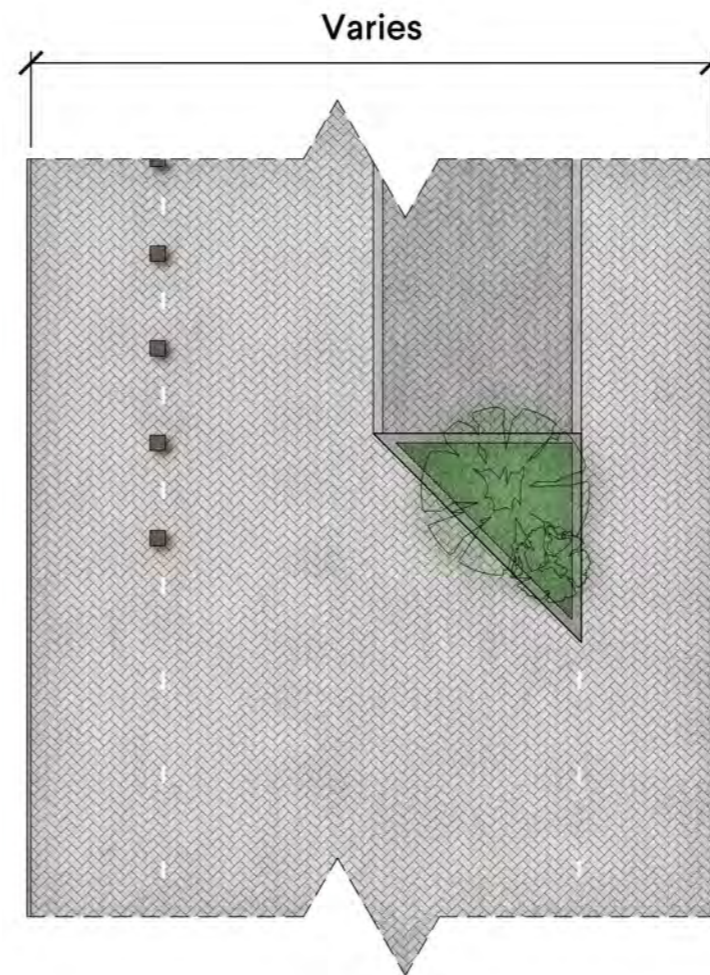


6.2.3 Tertiary Road

The Tertiary road will be built up in a similar manner to the Secondary road with a reduced carriageway of 5m to encourage slower movement of traffic and discourage large volumes.

As this road will serve residential areas rather than through routes, standard pavement build ups will be used on both sides in conventional materials to match in with the developments.

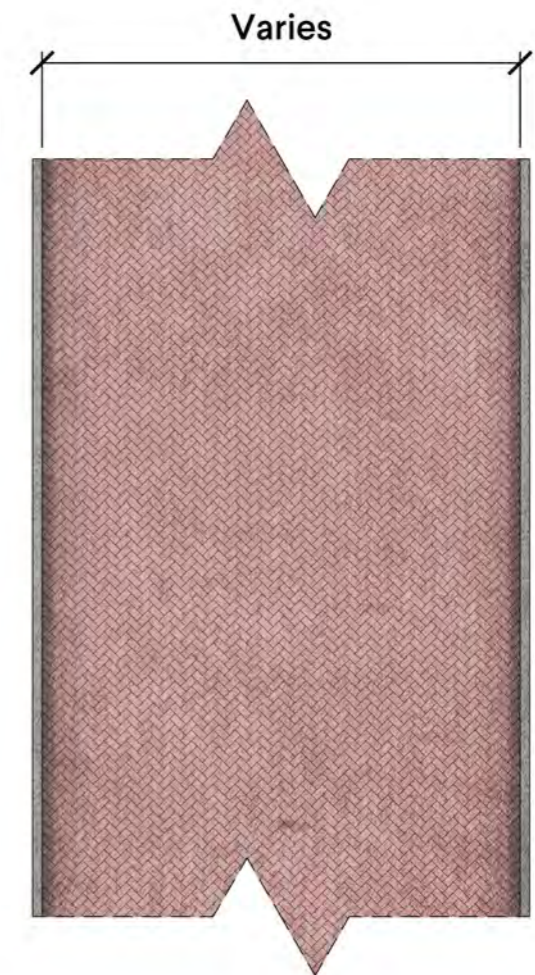
The landscaped border will be retained and will vary as required, still at a min of 2.5m to host visitor parking as part of the overall masterplan parking strategies. This will discourage parking on the pavements and draw the park into the development areas.



6.2.4 Home Zone

Home Zones will be varied in size and function throughout the site, creating unique place settings for dwelling clusters. These shared surfaces will host varied planted features and parking spaces for visitors. These adaptable spaces will promote pedestrian priority hierarchy and will encourage community interaction and play.

The surface should vary in material from the surrounding road network to demark this unadopted space.



6.2.5 Private Drive

Private drives will be unadopted roads used to create a quality living environment. The design will vary across the site defining distinct communities and closes. This shared surface will seek to promote pedestrian priority hierarchy within intimate dwelling settings.

The materials will vary to the road network to make the distinction between the adopted spaces and private spaces.



6.0 URBAN STRATEGY

6.3 Bus Linkages

Connectivity is central to the design focus at Stainsby. Bus routes will be integrated along the main Primary Road and Secondary road to ensure that no dwelling is more than 400m from a bus stop. These routes will form part of the larger Middlesbrough network creating a sustainable transport network for door to door connections.

A Transportation Hub will be hosted as part of the Central Hub facility at the heart of the site. The creation of a 'super-stop' will featuring real time displays for transport links and a cycle park to encourage sustainable travel. Bus priority roads will be integrated into the design to enhance connectivity and efficiency.

Bus routes and details will be advanced with key partners throughout the detailed transport network development and design process.

KEY



Bus Stop - with 400m radius



Transport Hub Stop

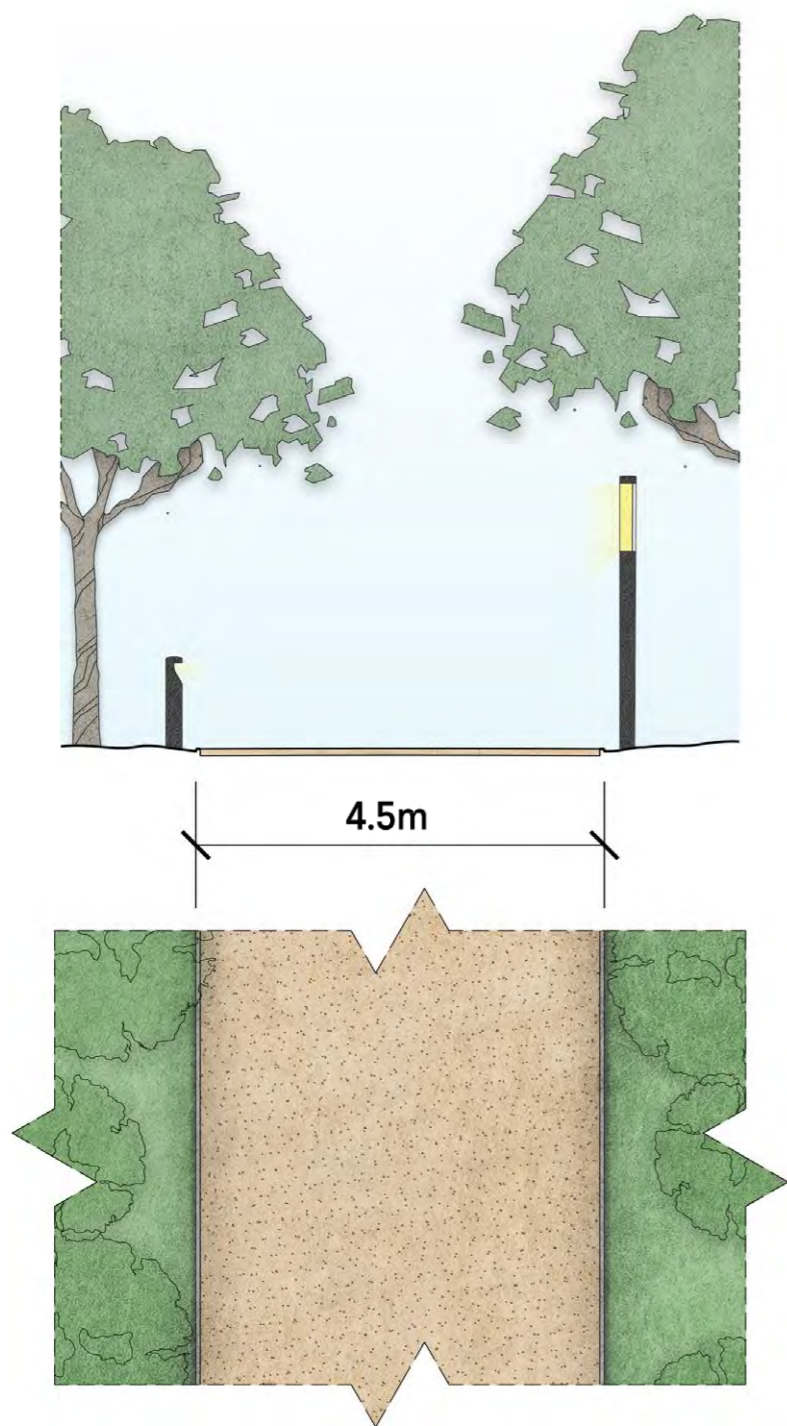


Bus Route



Transport Hub Loop

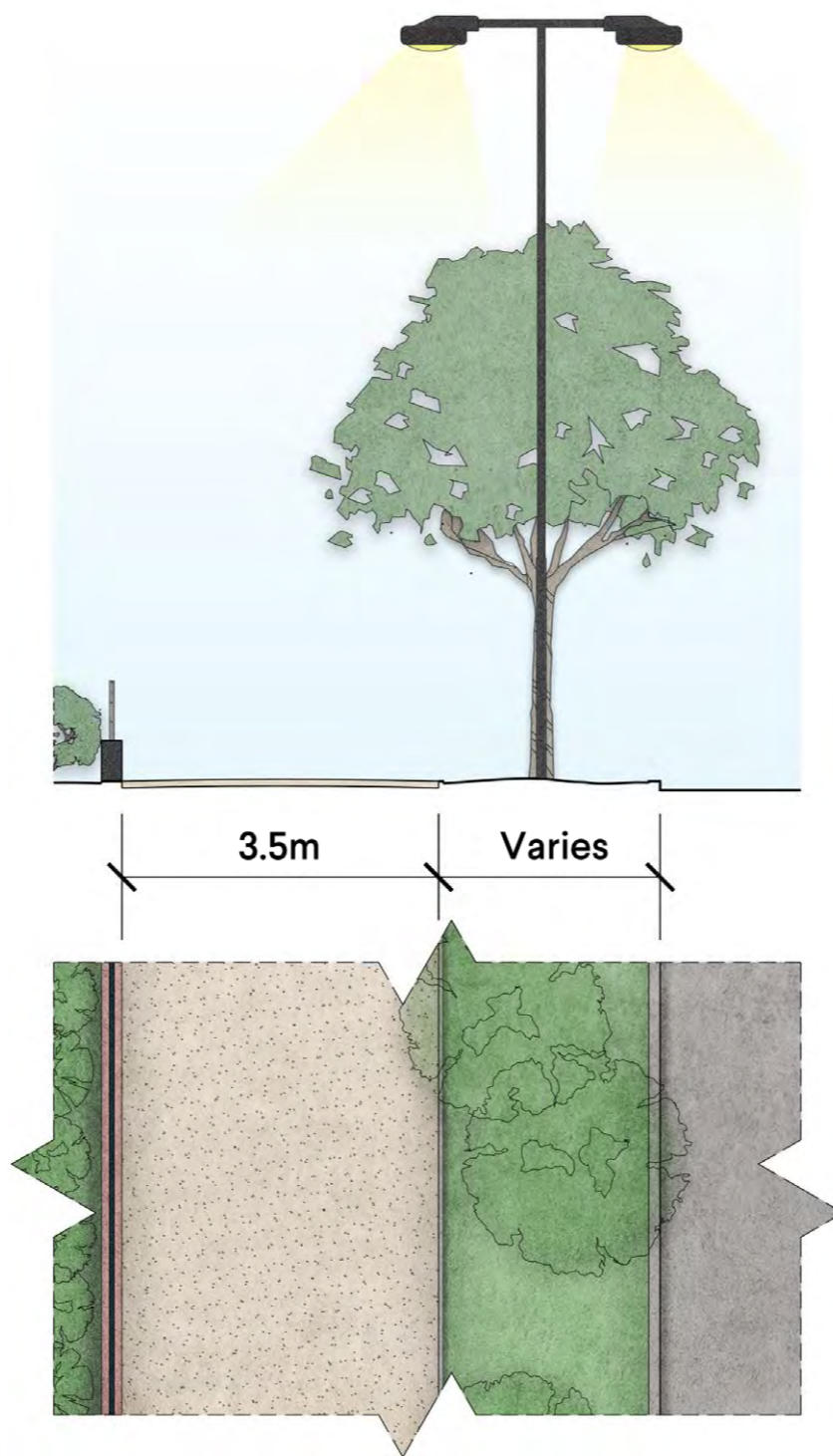




6.4.1 Strategic Cycle Route - 4.5m

An anti-skid surface will be utilised for 2-way commuters on this primary adopted path, and will be formed in contrasting appearance to the adjacent paths and routes to give it a distinct identity. This route will generally be flanked by landscaped verges on both sides to enhance the journey to attract greater usage.

A designated lighting strategy will be utilised on this route applying varying low-level lighting options for environmental & wildlife sensitivity as the journey transitions between urban & landscaped areas throughout the site. A variety of different lighting columns, including bespoke designs, can be used to create a strong visual identity to form part of the overall high-quality country park.



6.4.1 Multi-use Path - 3.5m

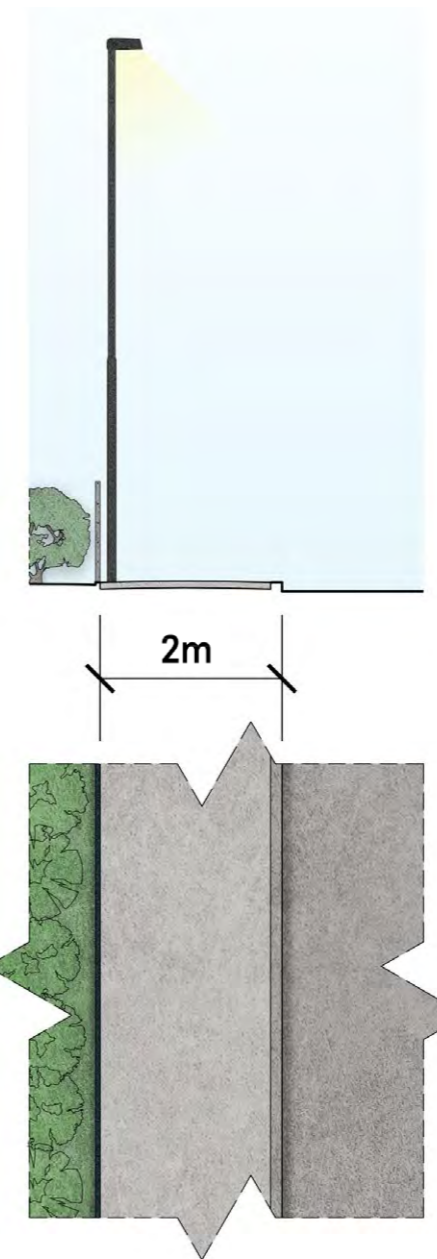
The multi-use adopted path will be the most commonly utilised connectivity path throughout the development. This accessible path network will utilise a differing material finish to give it its own distinct identity throughout the site, to encourage pedestrian connection. These paths will always lead back to the Country Park and Central Hub.

A landscaping border will be created wherever the path lies adjacent to a road. In this instance a shared adopted highway lighting strategy will be utilised. If the path is bound by the Country park, low-level environmental lighting strategies will be utilised where required tying into the wider country park aesthetics.

6.0 URBAN STRATEGY

6.4 Adopted Paths & Artificial Lighting Strategy

The following studies show the indicative build ups associated with the Adoptable path strategy. These paths will be integrated throughout the Urban and Landscape strategies to create formalised routes throughout the whole development.



6.4.1 Pavement - 2m

Adopted pavements will be utilised throughout the site, creating accessible routes to dwellings. These will generally be directly adjacent to an adopted highway and front onto a dwelling boundary.

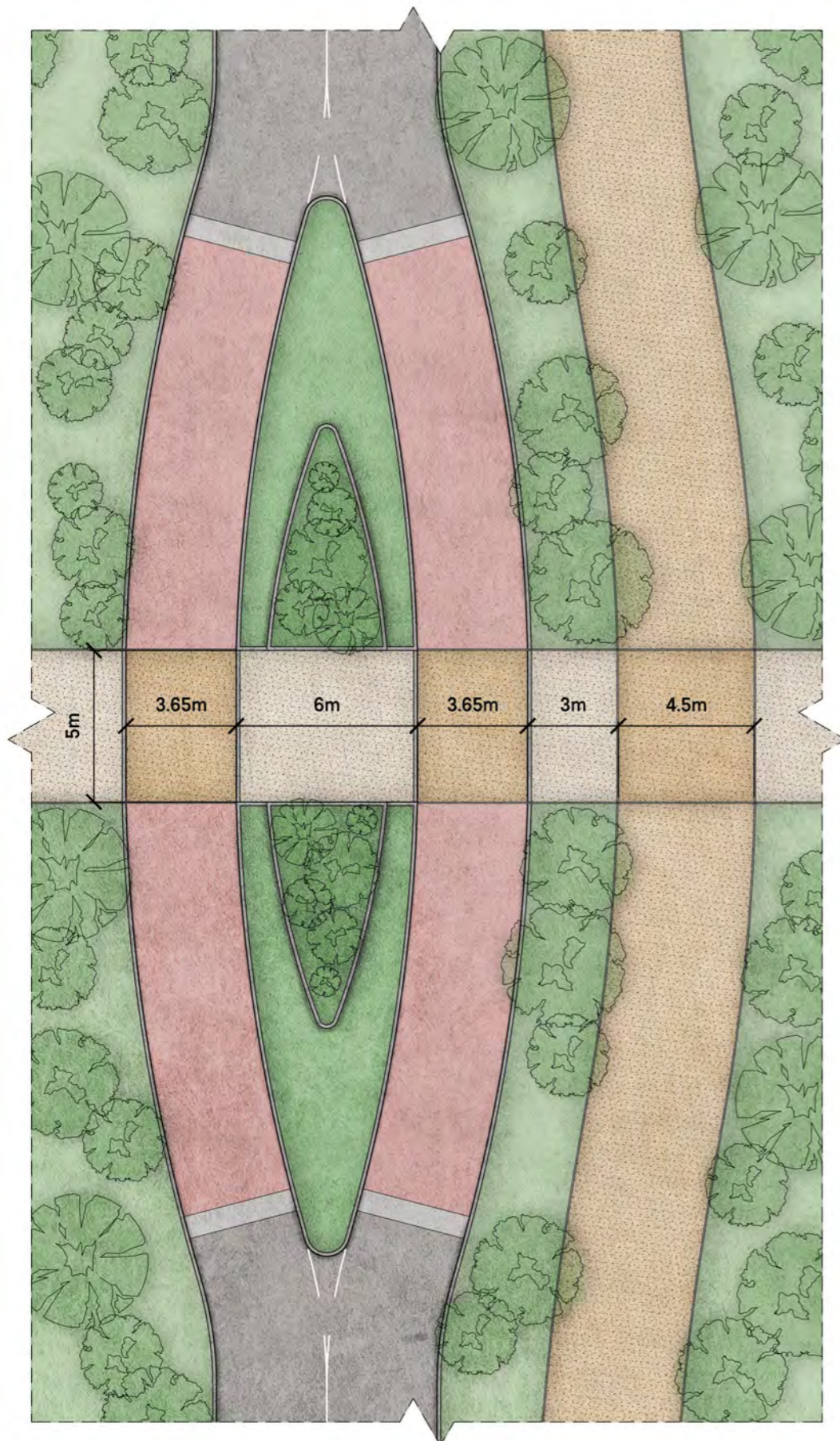
Typical adopted highway lighting will be utilised here to light the carriageway and footpaths simultaneously. Lighting column design will be coherent with the overall masterplan aesthetic, creating high quality public realm that leads into the country park.

Artificial lighting should be carefully integrated throughout the whole scheme in order to minimise impact on wildlife and habitats.

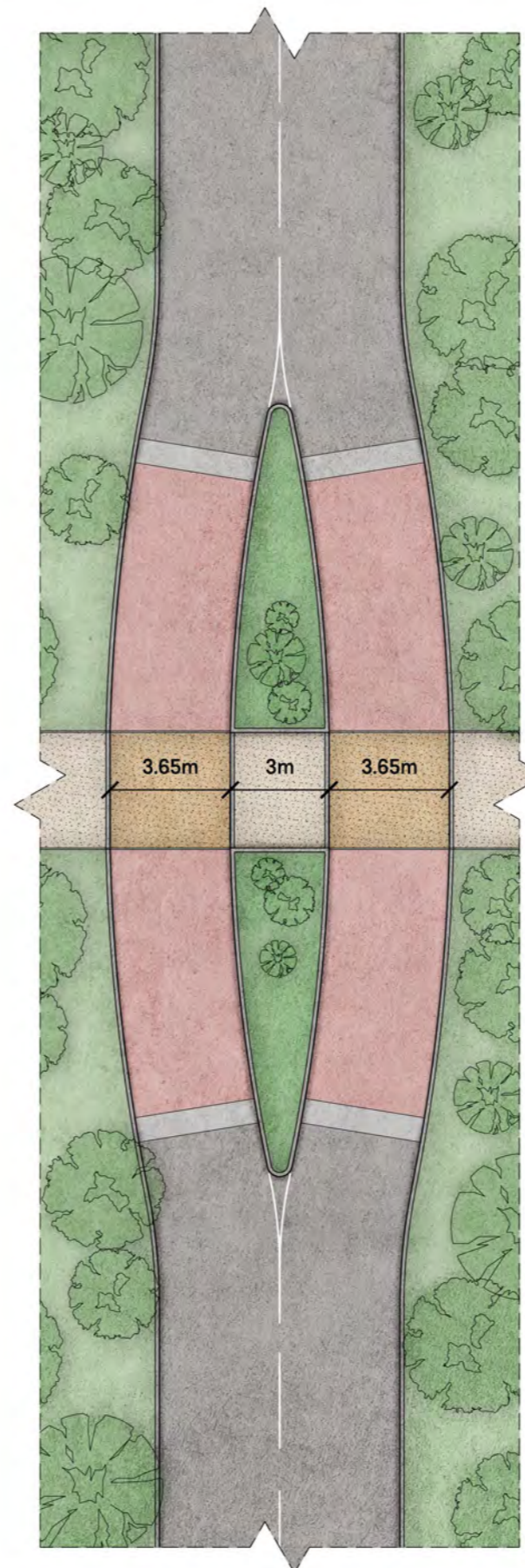
Adopted routes with required lighting will be planned away from strategic wildlife areas.

Low level lighting will be used throughout the country park, with unidirectional lighting onto footpaths to minimise light exposure in habitat zones.

Lighting in general will be resisted where it could profoundly impact areas of significant wildlife.



PRIMARY ROAD - GATEWAY CROSSING POINT



PRIMARY ROAD - TYPICAL CROSSING POINT

6.0 URBAN STRATEGY

6.5 Crossing Points

A series of crossing points have been developed in conjunction with Middlesbrough Council which aim to encourage pedestrian priority movement whilst creating natural traffic calming features at intervals between the paths and road intersections throughout the masterplan.

6.5.1 Primary Road Crossing Points

Strategic crossing points will demark pedestrian level access crossing along the Primary Road. In both these instances the road will be split by a naturalised landscaped planter, will splay zones for pedestrian and vehicular visibility designated by mown grass a minimum of 1m, or a low shrub zones.

Both indicative designs seek to include a rumble strip zone (or textured block paving) before entering a contrasting coloured raised road to encourage slow speeds. The pedestrian access path will be delineated by an alternative coloured crossing material to define the pedestrian priority.

The primary road Gateway crossings will create a larger feature with more mature planting to shield the central crossing point, whilst retaining the visibility splays (Refer to the Landscaping strategy for planting methods). These Gateway features will occur predominantly at the Northern and southern entrances to the site, where a carpark creates a Northern entrance to the Country Park, and to the South where the design opens out to show the Visitor centre gateway to the Country Park.

The typical crossing point will be utilised along the length of the Primary road for any other crossing. This involves a shorter length of road division, whilst retaining an adequate central resting point for cyclists and other pedestrians. Lower level planting will be used throughout the whole of the central reservation to allow for clear visibility of the public.

Further details will be developed with Highway Designers and Transport Engineers prior to adoption.



6.0 URBAN STRATEGY

6.5.2 Secondary Road Crossing Points

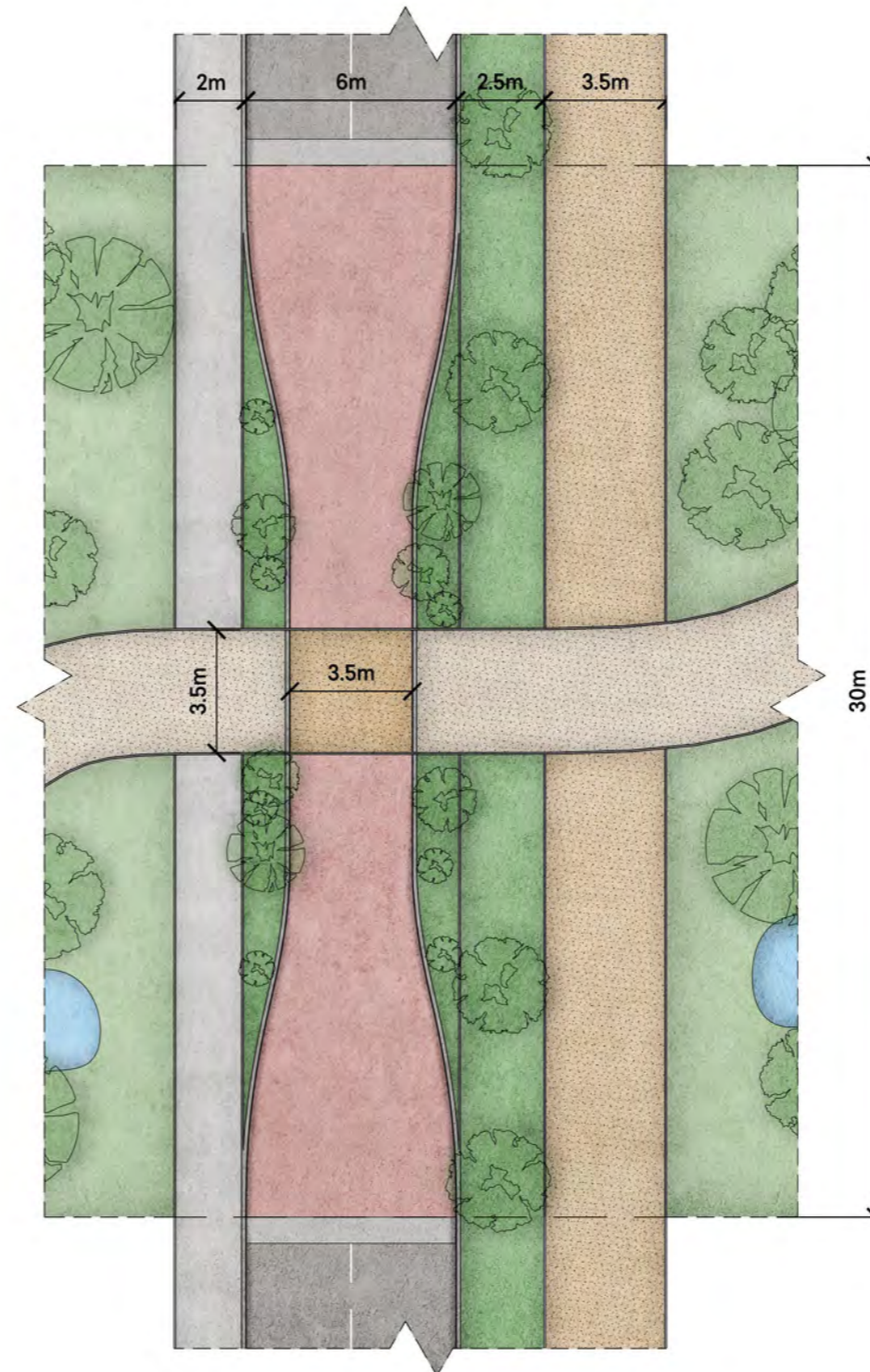
Along the Secondary Roads an alternative strategy will be utilised to enhance the landscaping whilst creating naturalised traffic calming methods. Public crossing visibility will be a priority in all instances, with careful selection of planting to be utilised in each instance, to enhance local character and ecology.

In general on a Secondary Road the carriageway will be narrowed wherever a crossing point occurs, build out in a landscaped planter to encourage slower speeds than the Primary route, and prioritise pedestrian crossings. These zones will also be demarked using a rumble strip (or textured block paving) to enter a raised contrasting coloured road surface. The pedestrian access path will again be delineated by an alternative coloured crossing material to define the pedestrian priority to tie in with the surrounding pathways.

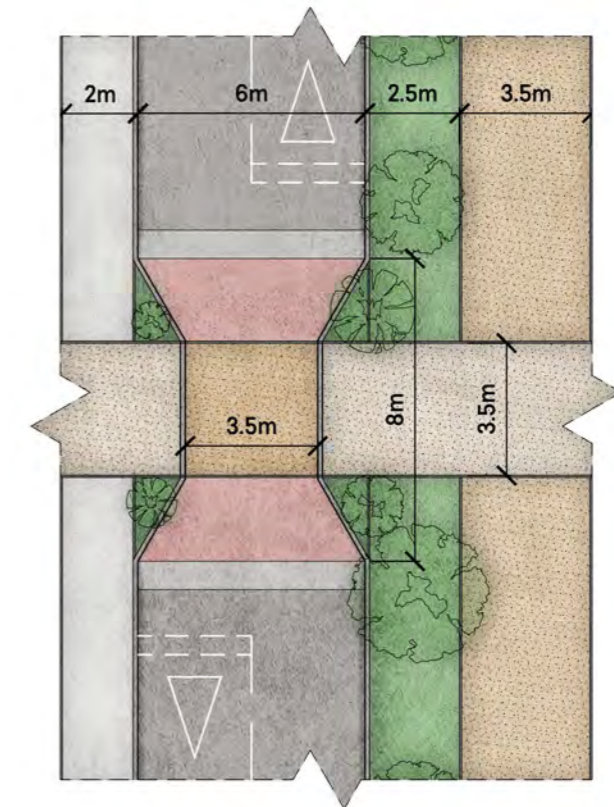
Where a Secondary road crosses a Green corridor, the whole width of the road will be treated in the contrasting material. An organic planter will be utilised across the length to reduce the carriageway width to a single vehicle. The alignment of the single vehicular access can vary in different locations throughout the site, not being limited to central only access depending on the site configuration.

All typical crossings will have a much shorter restricted passage zone whilst still being enhanced by landscaped borders and material applications.

The operator priority system will be developed with Highway Designers and Transport engineers throughout detailed design.



SECONDARY ROAD - LANDSCAPE CORRIDOR CROSSING POINT

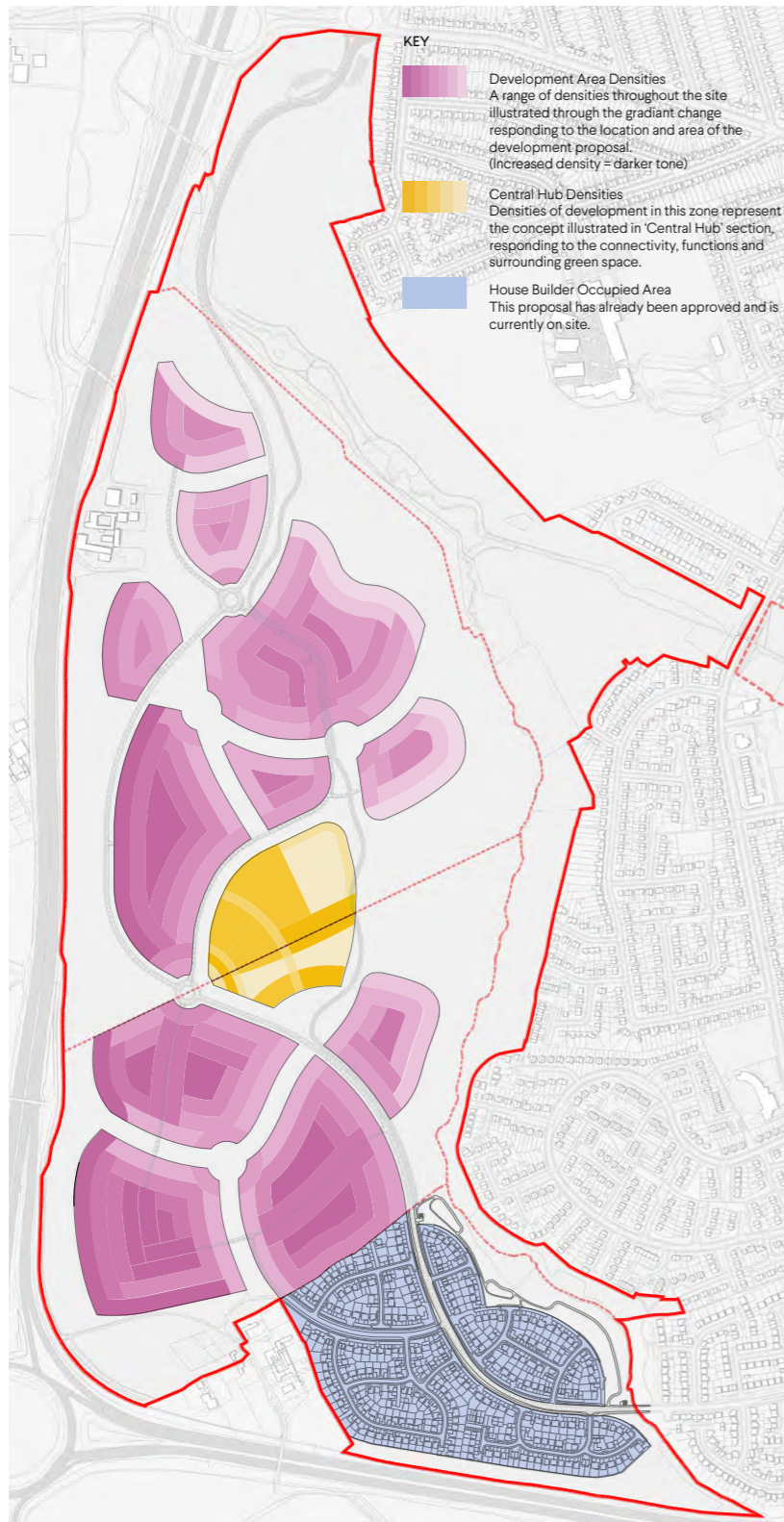


SECONDARY ROAD - TYPICAL CROSSING POINT

KEY POLICY

Pedestrian Access & Circulation

Middlesbrough Design Guide SPD: Where possible, the movement of pedestrians, cyclists and public transport should be considered equally, if not above, those of car users. Desire lines are important in the consideration of pedestrian and cycle routes, and where possible routes should be direct, safe and easy.



KEY POLICY

Housing Mix & Layout

H21: Brookfield: The following uses are considered appropriate:
 i. residential – 1670 dwellings of which a minimum of 1125 to be completed within the Plan period up to 2029; ii. employment (B1 use) 2ha iii. local retail centre – to be provided when need arises, and iv. primary school – to be provided when needs arises.

The Council will require the development to deliver a high quality scheme that: a) creates residential development in neighbourhoods of identifiable character which provide variety and diversity in layout and design; b) provides a mix of dwelling types and sizes, including three and four bedroom detached and semi-detached dwellings;



6.0 URBAN STRATEGY

6.6 Housing Mix & Layout

The development as a whole should create a distinct sense of place, with the individual development areas each having it's own unique sense of character from one another. This will help define a strong sense of place and allow for defined wayfinding within the scheme, forming communities that all exist as part of the wider landscape led masterplan hosted in the Country Park.

A variety of design approaches will therefore be required across the site, varying material palettes and applications from area to area in relation to the site features. The mix, style and layout of the houses must create a high quality and spacious development.

The site should accommodate a range of home types and size. This could include, low rise apartments, bungalows, short terraces of houses, semi detached and detached homes.

The appropriate density of development will vary across the site. Higher densities will be more appropriate to the West and South of the development with lower density towards the Eastern edge bordering the Green space and Northern areas of the site. Refer to the adjacent diagram.

Building layouts will vary depending on where it is located on the site and character of that area. There are however some principles that will be relevant to housing across the whole site. Streets must have either; house frontages on both sides or for single sided streets house frontages facing green space. At the corner of street junctions a specific corner-turning house type must be used which has windows facing both streets.

Distances between dwellings must uphold local guidance, however reduced distances may be accepted if it creates a better urban design and placemaking strategy. Reduced distances may be acceptable where dwellings create pinch points or focal points, by offsetting window positions within the dwelling types.

Refer to Section 7.0 Urban Strategy Studies which demonstrate a range of development principles across the site.



6.0 URBAN STRATEGY

6.7 Parking

Considerate parking design is essential throughout the development to minimise the impact of vehicles on the landscaped masterplan and urban design principles.

Residential parking arrangements should be varied across the site for diversity and to formulate sense of place and areas of individual character. Where possible vehicular access areas should be less clearly defined to encourage low speed driving and promote connection to the landscape amenity.

All properties must be provided with parking in accordance with the Tees Valley Design Guide & Specification. In addition to these guidelines the following rules apply:

- Regardless of size all houses must have a minimum of 2 designated parking spaces.
- Garages cannot be counted as a parking space.
- Integrated covered parking and open garages can be included within the parking provision.

There are different parking conditions that will be considered acceptable in different areas of the masterplan:

No parking will be permitted on or directly accessed from the Primary Road.

Parallel parking may be provided for visitor use only, integrated within a 2.4metre landscape buffer provided not more frequently than every third dwelling for single spaces and every nine dwellings for up to three spaces together. This may occur in Secondary and Tertiary Road locations. In Home Zones and private drives visitor parking can be informally suggested by positioning of landscape and a change in surface material but engineered to prevent indiscriminate parking. This can be achieved by ensuring that such parking is impossible unless it would prevent free traffic flow and / or vehicular access to house plots and private parking bays / areas. One visitor space per four dwellings to be provided.

Frontage parking may occur only to one side of secondary or tertiary roads, where plots have sufficient depth (8m minimum from curtilage edge to front of dwelling) to enable integration of landscape, this also includes a 1m planting zone in front of the dwelling. No frontage parking should occur on any dwellings facing a Primary Road or Green edge.

Side Parking in between dwellings is to be encouraged. Lower density areas, park edges and opposite streets with frontage parking on one side are suitable locations. Garages should be sufficiently recessed from the building line for vehicles to be concealed by the building.

In all cases the width of the driveway at the point where it meets the footpath or road cannot be greater than 75% of this property boundary. Where smaller dwellings with narrower frontage require two parking spaces, one space should be provided in-curtilage and the second one elsewhere in clusters of no greater than four spaces within landscape setting.

Parking courts should only apply in locations where front in-curtilage parking cannot be achieved and they aid in the achievement of good design. Where hard edges are necessary in relation to roundabouts and addressing Primary Roads, all effort should be made to make provision to the rear of dwellings within the dwelling curtilage.

Where this is not possible, parking spaces should be provided in courts serving no more than ten dwelling with no more than four consecutive spaces without a landscape buffer.

Parking courts must always be designed to have active residential frontage on a minimum of one side. A court should be a space with parking contained rather than a car park.

Driveways should be constructed from a material that contrasts in type or colour from the adjoining road and pavement to clearly delineate between public and private space.

Electric vehicle charging points will be encouraged throughout the site as a whole, with provision integrated into public car parks and proposed dwellings alike to contribute towards a greener future and infrastructure in line with Middlesbrough Councils commitments.

6.8 Garages

The adjacent page displays a matrix of acceptable garage types throughout the development. The varying types of garage and locations seek to propose a balanced mix of hierarchical approaches based on road type adjacencies throughout the site.

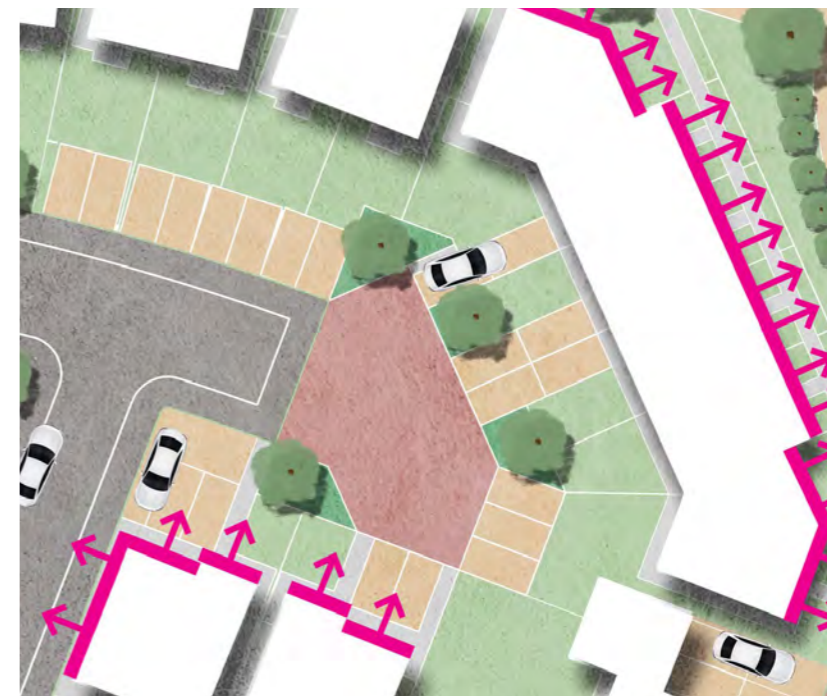
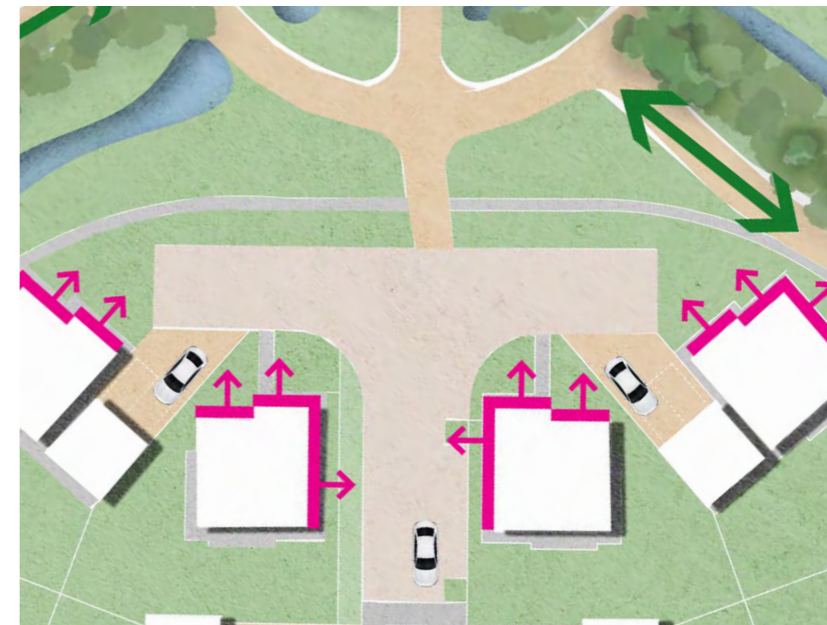
KEY POLICY








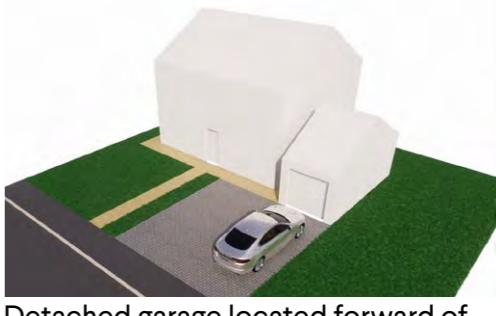
Parking

Middlesbrough Design Guide SPD: .17 Parking provision and its design should be integrated into housing layouts and not all placed together in a single bank.

Middlesbrough Design Guide SPD:

Where cars are parked in traditional rear courtyards or squares they should be small ideally not exceeding 10 no. spaces, avoid a sterile appearance, and be easily supervised from adjacent housing, street or courtyard.



				
	Detached Double Garage located to the side of the property	Detached Single Garage located to the rear of the dwelling.	Detached double garage at the rear of the back garden.	Integral Single Garage set back min 1m from front elevation.
Primary Road	*	*	*	*
Secondary Road	✓	✓	✓	✳
Tertiary Road	✓	✓	✓	✓
Homezone	✓	✓	✓	✓
				
	Integral Single Garage located within the front elevation of the house.	Carport condition located within the front elevation of the dwelling.	Integral garage set forward of the main elevation of the house.	Detached garage located forward of the front elevation of the dwelling.
Primary Road	✗	✗		
Secondary Road	✗	✗		
Tertiary Road	✓	✓		
Homezone	✓	✓		

Key	
✓	Acceptable Garage Condition
✗	Unacceptable Garage Condition
■	Garage Condition not to be used in any instance
*	Condition not to be accessed by Primary Rd however can front onto a Primary Rd
✳	8m frontage between driveway house and dwelling - 1m landscaping buffer between driveway and dwelling

6.0 URBAN STRATEGY

6.10 Boundary Treatments

The application of boundary treatments are essential to the successful implementation of the masterplan. Choosing the appropriate type of boundary treatment will ultimately define community and character throughout the site. The application will define orientation, accessibility, permeability and connection within the development areas.

The following treatment examples, whilst only indicative of the possible treatments, begin to define the approach in specific circumstances.

NOTE: These boundary treatments may not feature as shown in the final development. Designs will be determined through the detailed planning application stage and in discussions between the developer and planning authority.

6.10.1 Dwelling - Rear Garden Boundaries

Boundaries to the rear of dwellings should seek to create a private garden environment for the residents, defining the boundaries using higher fence designs. Trellis' may be integrated into these full height walls in order to encourage community between adjacent neighbours only whilst retaining levels of privacy. Materials applications will vary depending on plot adjacencies:

Brick Wall Full height

Used between dwellings and a road/drive where there is alternative property surveillance onto space.

Brick Wall with Wooden Trellis for visibility

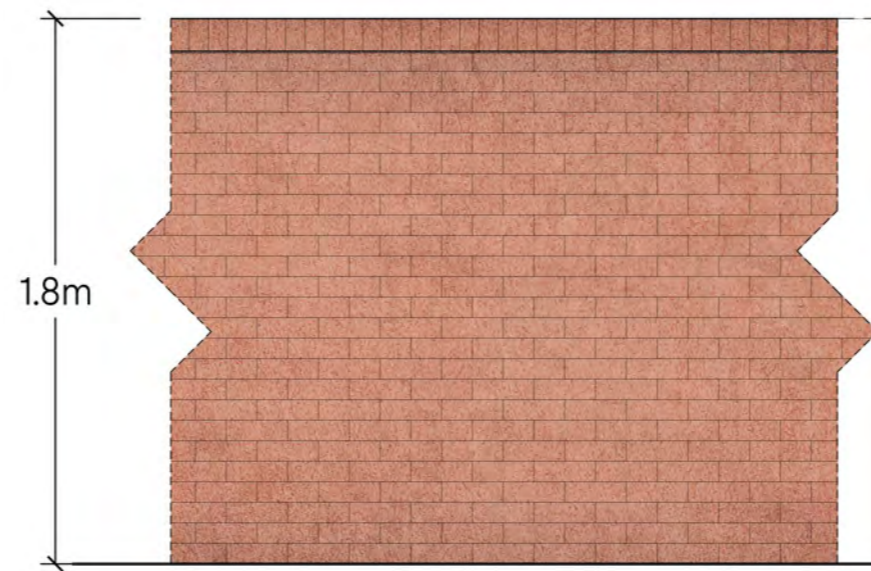
Used between dwellings and a road/drive edge where there is limited alternative overlooking to increase safety and surveillance.

Close Boarded Fence

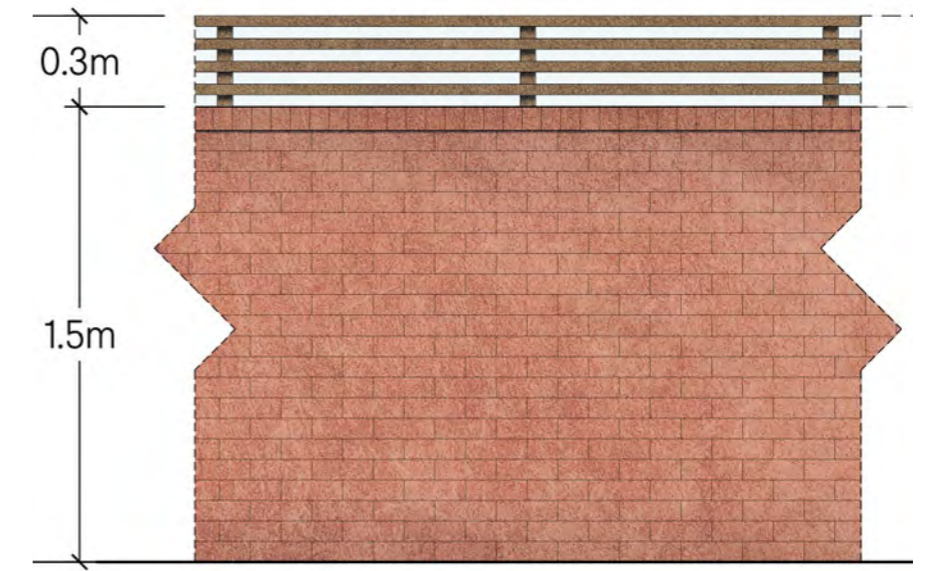
Used for privacy between back gardens

6.10.2 Private Drive Gateposts

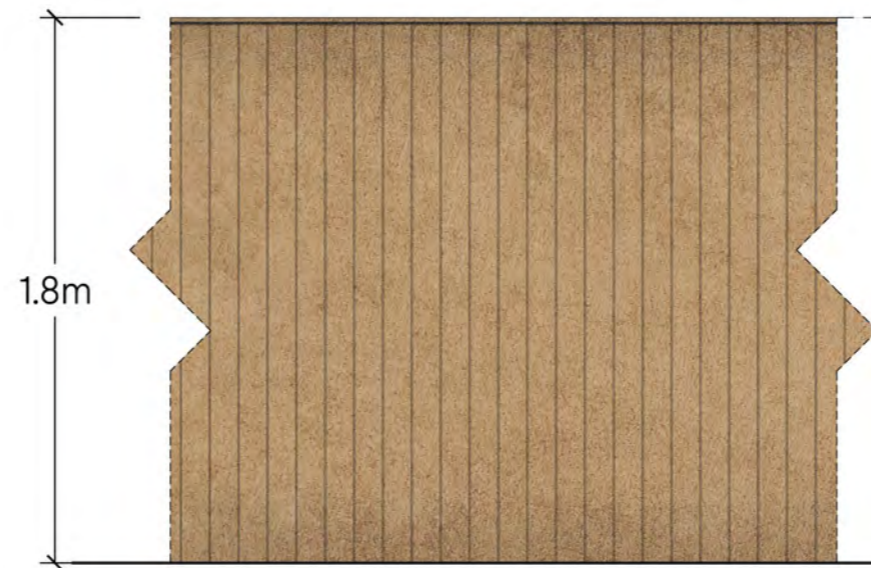
Solid posts should be created to demark the entrance to a private drive at each instance integrating in the placement of drive names. This seeks to encourage community within the cluster and privacy from the wider area as creates distinct neighbourhood threshold change from adopted highways to private. Brick in varying colours could be used to heighten the local character, and visually permeable fences should be used either side to enhance connection and discourage exclusivity.



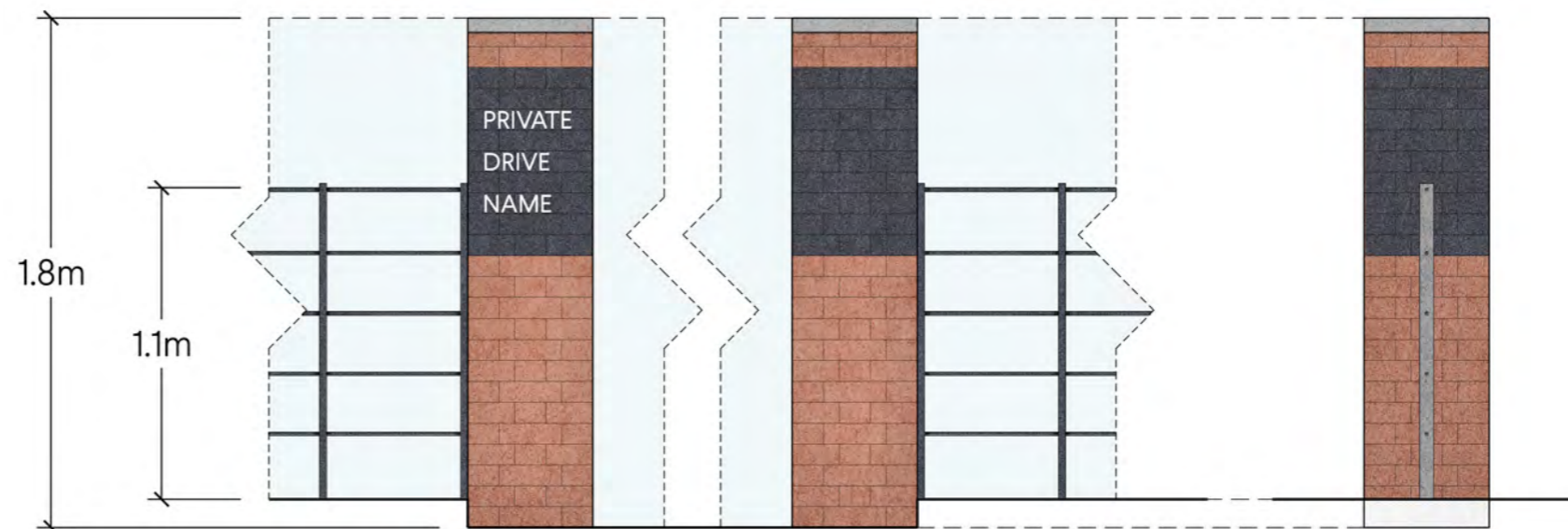
ELEVATION
BRICK WALL - FULL HEIGHT



ELEVATION
BRICK WALL - WITH TRELLIS



ELEVATION
CLOSE BOARD FENCE



ELEVATION
PRIVATE DRIVE GATEPOSTS

SECTION



House in San Marino Funes Hills, Mariel Suárez © Ramiro Sosa



©BlueStone Supply LLC



John Pardey Architects, Eveline Lowe School © James Morris



© Paul Newman Landscapes



Coastal 2Ways © Zephyr and Stone

6.0 URBAN STRATEGY

6.10 Boundary Treatments continued.

6.10.3 Feature Walls (Indicative)

The use of 'feature walls' throughout the development seek to activate spaces where any garden faces onto a green corridor, road or path cutting through the development zones, and will be required to be integrated throughout the design of the development.

The adjacent precedents show a limited number of potential design and material solutions that could be implemented throughout the site to enhance the urban environment. These designs may integrate opportunities for passive observation/overlooking to enhance the safety of residents throughout the site. Final designs will be a matter for the planning application stage and will be a matter for discussion between the planning authority and developer.

The materials will vary to match the adjacent proposed dwellings to enhance character areas.



6.0 URBAN STRATEGY

6.10 Boundary Treatments continued.

6.10.4 Dwelling - Front Garden Boundaries

Boundaries to the front of dwellings should seek to frame the boundaries of the property with low fence/shrubs strategies to encourage community inclusion and connect, creating visible arrival sequences. A variety of approaches could be implemented here depending on hierarchy of paths and roads adjacent, in order to protect gardens close to main thoroughfares and open out landscape lead strategies where green edge conditions occur.

Low Brick Wall with estate rail

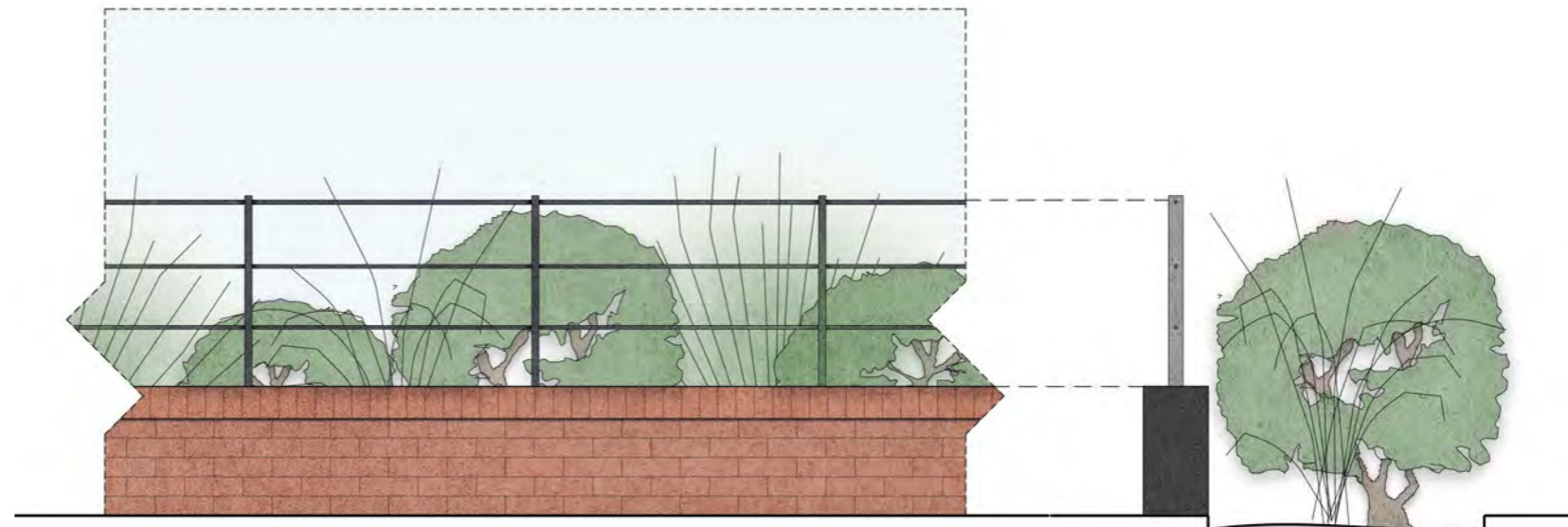
Used where there is a short frontage to the path/road edge and side parking to the property, to create a distinct and modern division from public to private land.

Estate rails and planting

Used where there is a long front garden with front parking to create a lighter touch on the pavement zone, to allow more visibility to green space within the site.

Low hedgerows and shrubs

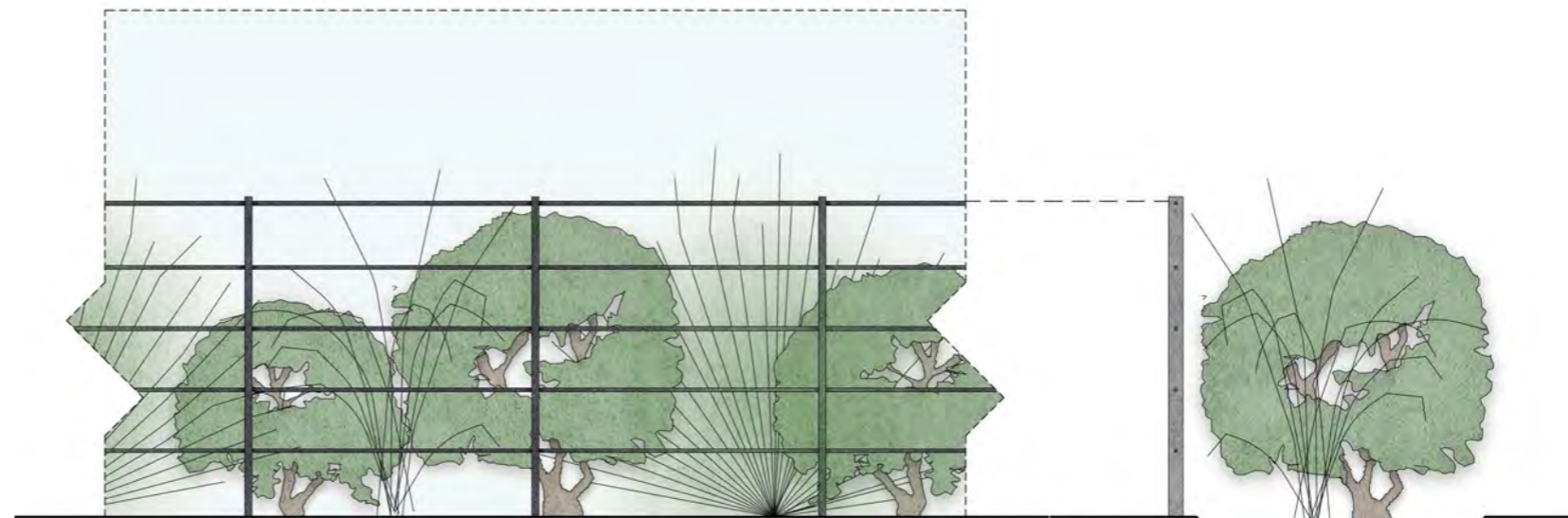
Used in Private drives and shared surface environments to create a more permeable and open solution whilst still distinguishing the private space. More likely to be well maintained in private settings and creates more community cohesion.



ELEVATION

LOW BRICK WALL WITH ESTATE RAIL

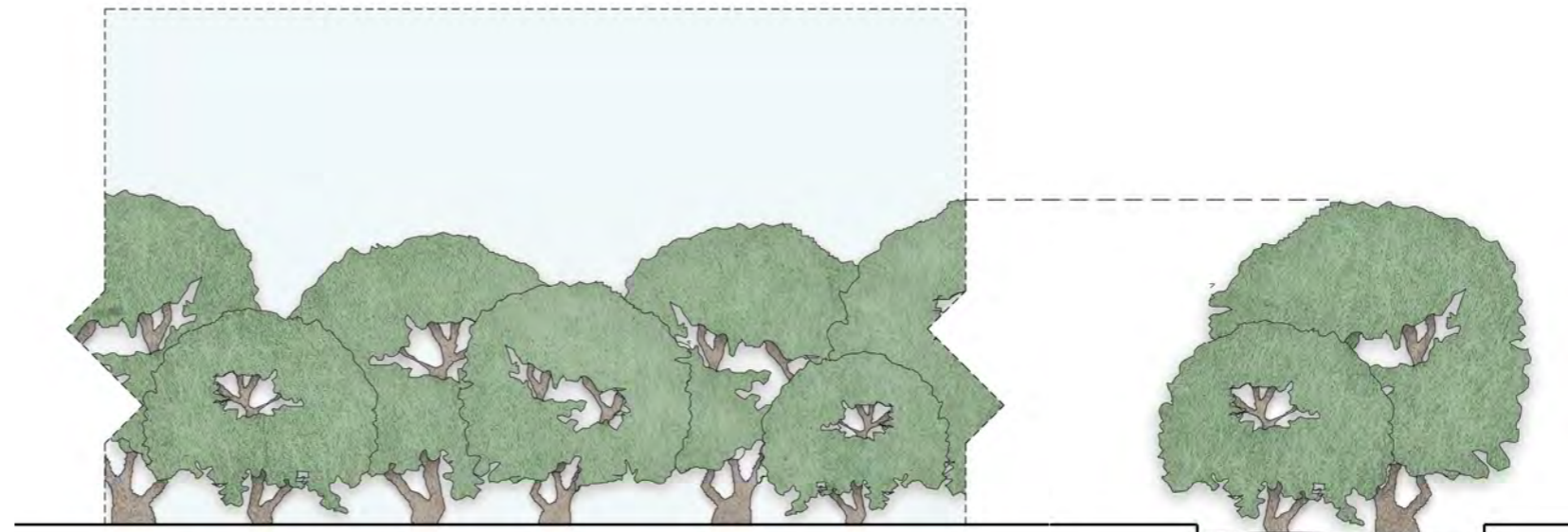
SECTION



ELEVATION

ESTATE RAILS AND PLANTING

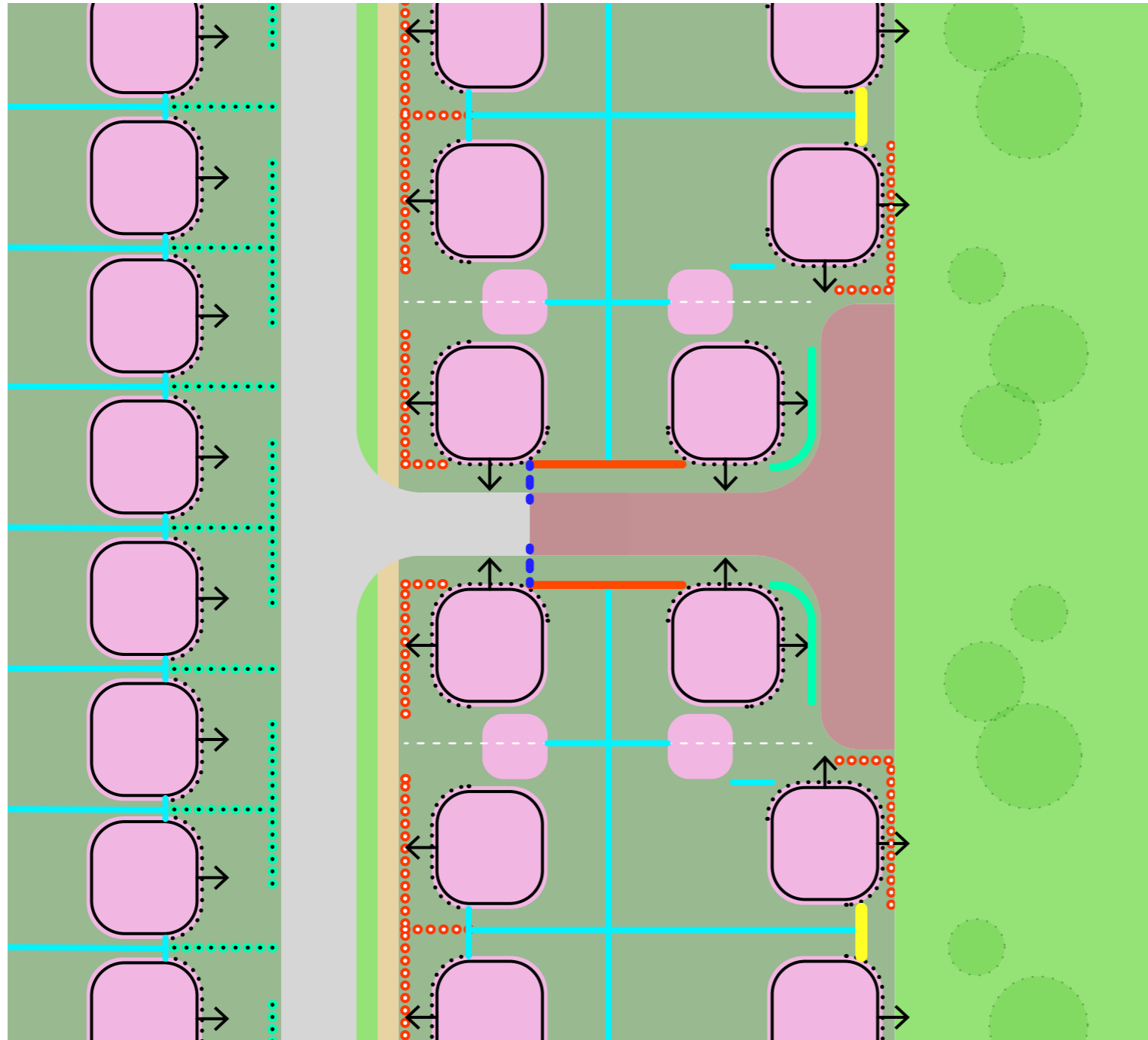
SECTION



ELEVATION

LOW HEDGEROWS AND SHRUBS

SECTION










6.0 URBAN STRATEGY

6.11 Boundary Treatment Indicative Study

The adjacent diagram is an indicative example of the application of the discussed Boundary Treatment principles. This begins to highlight some of the adjacencies created through the designated principles, creating a high-quality public realm with integrated surveillance.

KEY

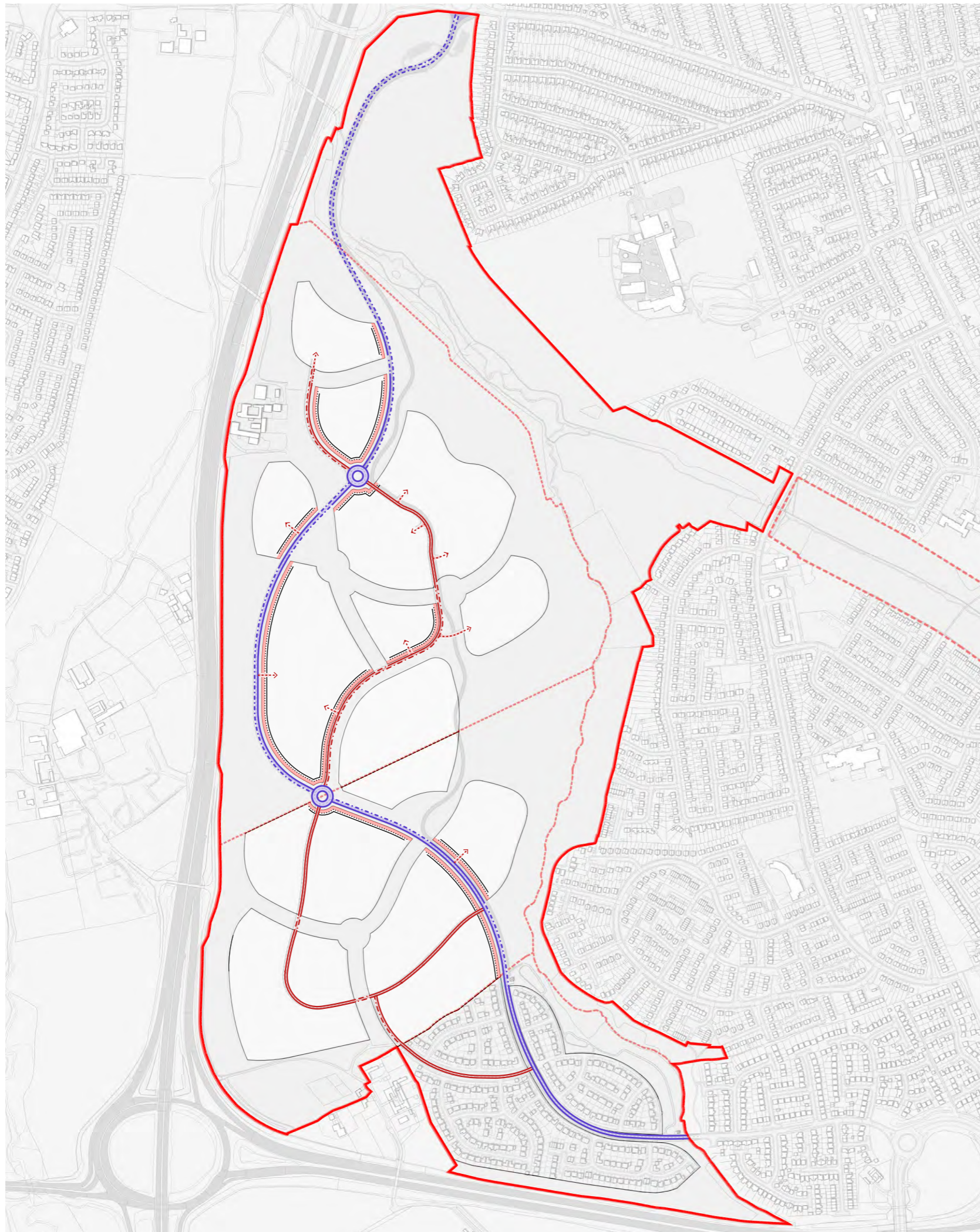
-  BRICK WALL
-  TIMBER FENCE
-  PRIVATE DRIVE GATEPOST
-  FEATURE WALL
-  LOW BRICK WALL WITH RAILING
-  RAILING AND PLANTING
-  LOW PLANTING



7.0 URBAN STRATEGY STUDIES

7.1 Roads





7.0 URBAN STRATEGY STUDIES

7.1 Roads (Circulation and Access)

A series of studies have been undertaken to give indicative examples for application of the varying road hierarchies in relation to defining the development. The extent of possible applications haven't been exhausted but begin to set a standard of guidance that is acceptable for developments. Further options can be developed in conjunction with this guidance and reviewed by Middlesbrough Council.

These studies look at each of the Road hierarchies and how they begin to interact with dwellings.

- 7.1.1 Primary Road Condition 1 - Building/road/green edge.
- 7.1.2 Primary Road Condition 2 - Building/road/building.
- 7.1.3 Secondary Road Condition 1 - Building/road/building.
- 7.1.4 Secondary Road Condition 2 - Building/road/green edge.
- 7.1.5 Tertiary Road - Branch Roads.
- 7.1.6 Home Zone - Shared surface & Gateway.

KEY

<ul style="list-style-type: none"> ——— Primary Road - Facing Development Area Where a Primary Road is directly adjacent to a Built edge to one side. - - - - - Primary Road - Facing Green Space Where a Primary Road is directly adjacent to a Green edge to one side. Primary Road - Potential Expansion Position An extension to the proposed road may be required to serve the site as a whole, accessing the site from it's Northern boundary. This is subject to area Traffic assessments and consultation with Highways. ——— Secondary Road - Facing Development Area Where a Secondary Road is directly adjacent to a Built edge to one side. - - - - - Secondary Road - Facing Green Space Where a Secondary Road is directly adjacent to a Green edge to one side. - - - - -> Entrance Road to Development Areas These access roads are predominantly from Secondary roads although there are a few instances on the Primary Road where they apply 	<ul style="list-style-type: none"> Building Frontages Dotted Lines show the Building Frontages Direction. Building Frontages facing towards the external condition of each development area creating active developments. ——— Development Edge - to Road Development edge borders a Road in these locations.
--	---

KEY POLICY

Roads

Middlesbrough Design Guide SPD: 4.6 Vehicles should not necessarily always have priority on roads, especially within residential areas, and safe passage should be provided for all users. The aim should be to achieve a harmonious mix of user types. 4.7 One way of harmonising user types is to incorporate shared surfaces. In a street with a shared surface, the kerb is absent and pedestrians and vehicles share the same surface. Shared surfaces can:- a) encourage low vehicle speeds; b) create an environment in which pedestrians can walk without feeling intimidated by traffic; c) make it easier for people to move around; and, d) promote social interaction.



7.0 URBAN STRATEGY STUDIES

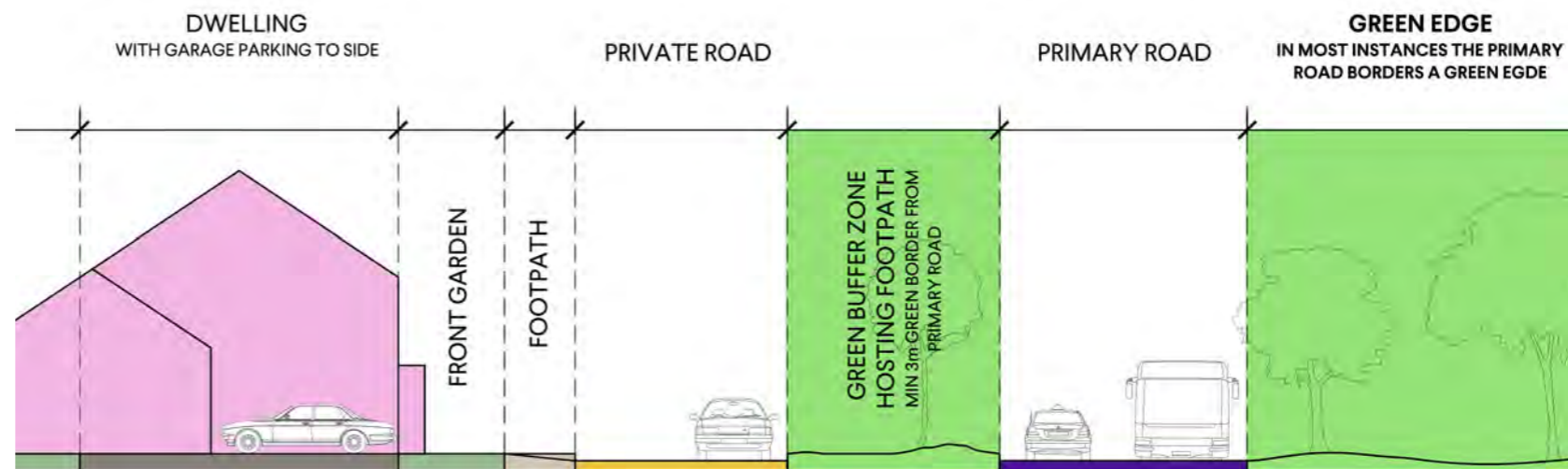
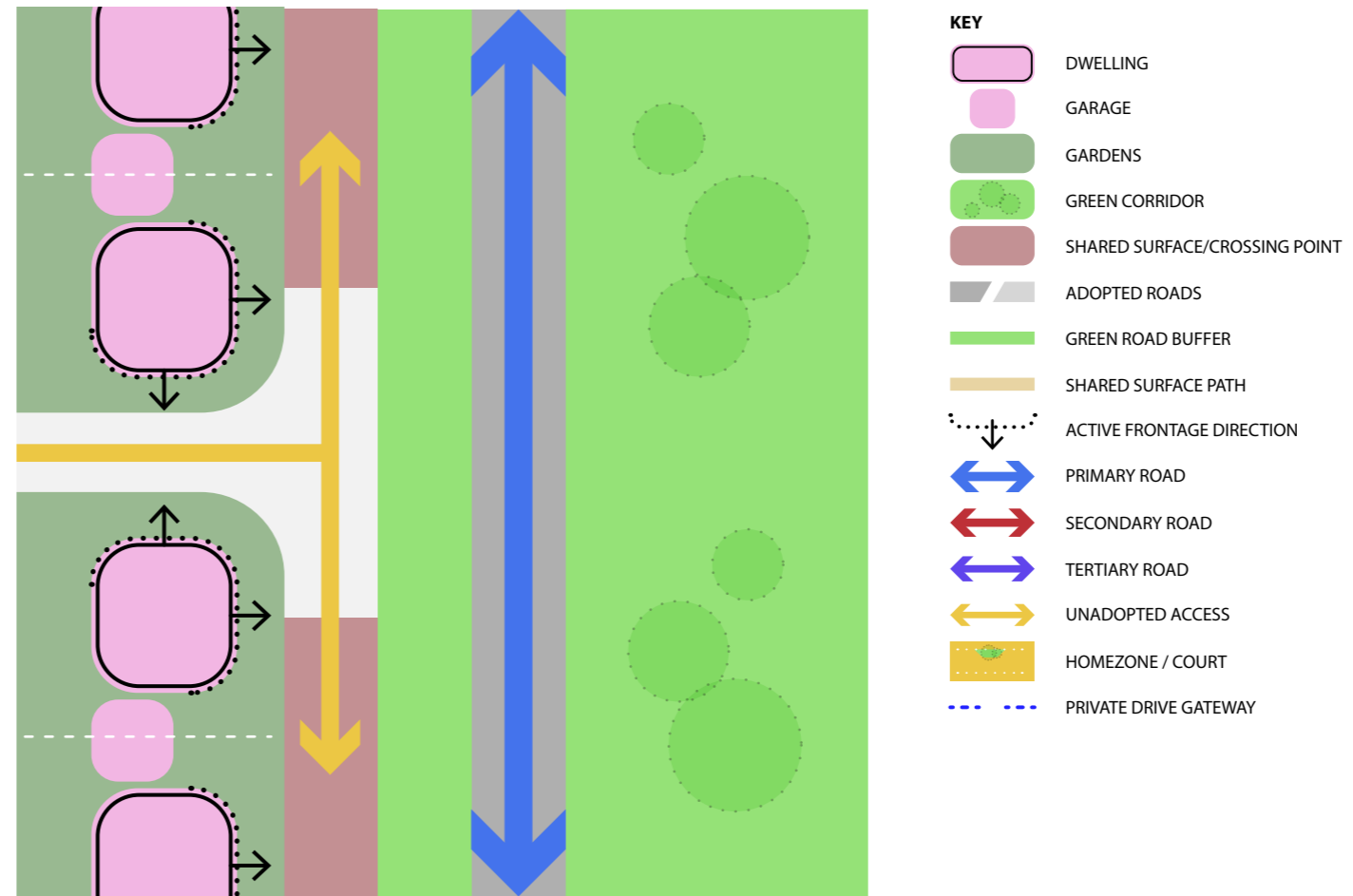
7.1.1 Primary Road Condition 1

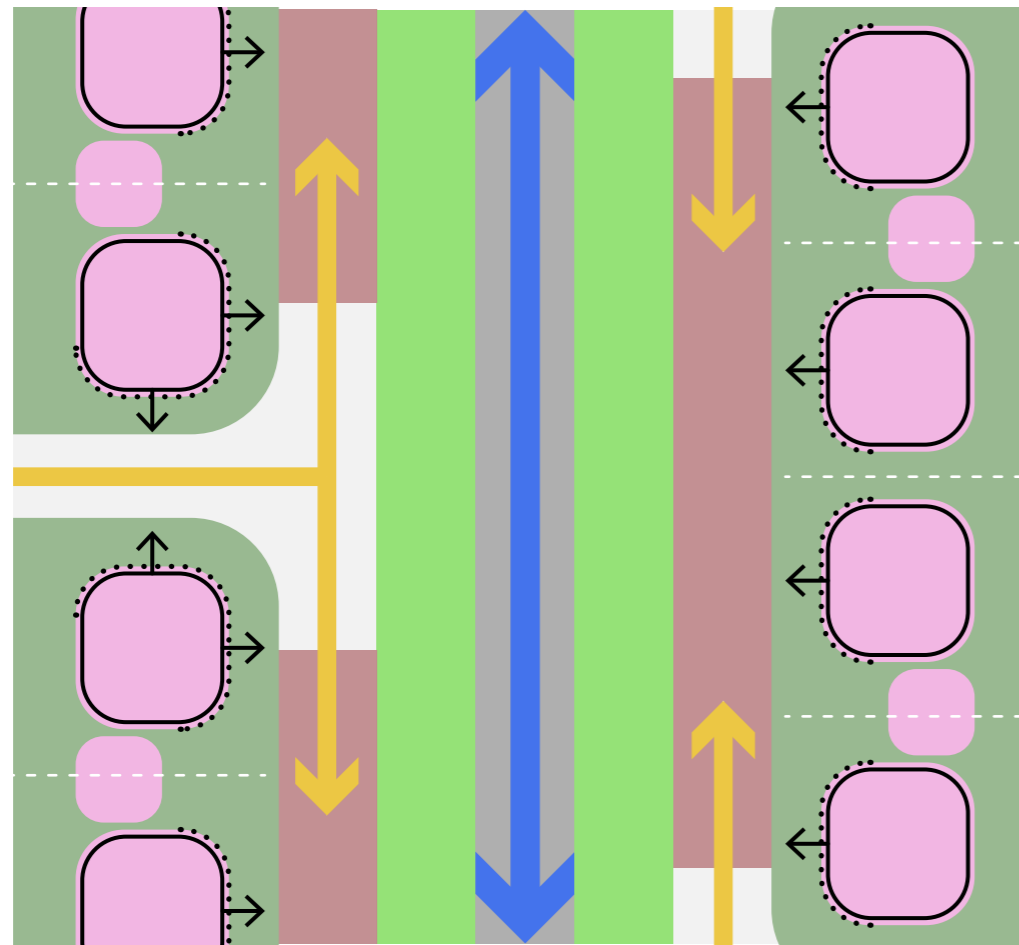
Building / Road / Green Edge

Where building frontages face on to a Primary Road, there should always be a private access road or shared surface access set back from the Primary road behind a min 6m landscaping zone. This landscaping buffer will create separation so no dwellings can be accessed directly from any Primary Road. No front of house parking is permitted in these areas, all parking should be in-curtilage.

These access points to the dwellings will come from the centre of the development areas, always creating an active frontage. The landscaping buffer will host footpaths that connect the wider site together.

All access areas should be overlooked by the adjacent houses at corners to ensure pedestrian safety.





- KEY**
- DWELLING
 - GARAGE
 - GARDENS
 - GREEN CORRIDOR
 - SHARED SURFACE/CROSSING POINT
 - ADOPTED ROADS
 - GREEN ROAD BUFFER
 - SHARED SURFACE PATH
 - ACTIVE FRONTAGE DIRECTION
 - PRIMARY ROAD
 - SECONDARY ROAD
 - TERTIARY ROAD
 - UNADOPTED ACCESS
 - HOMEZONE / COURT
 - PRIVATE DRIVE GATEWAY

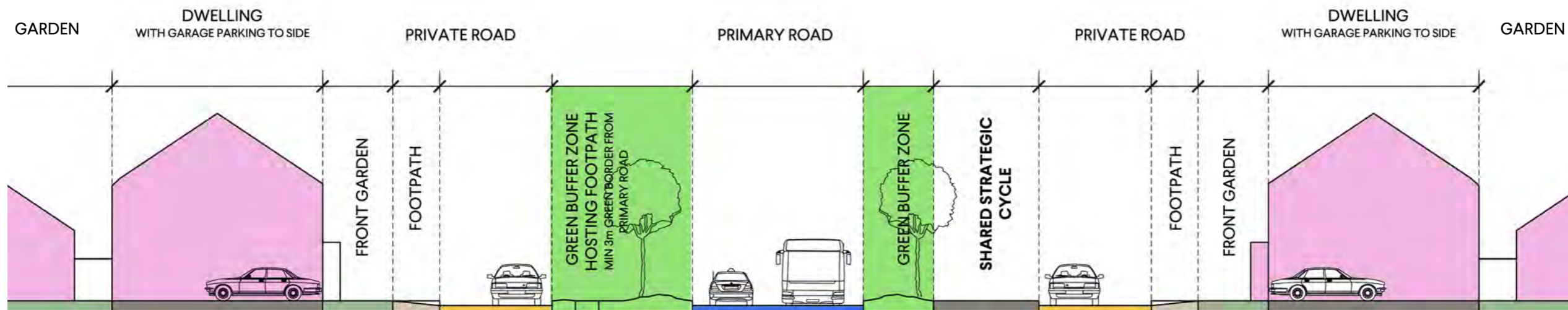
7.0 URBAN STRATEGY STUDIES

7.1.2 Primary Road Condition 2

Building / Road / Building

In limited areas of the site the Primary road will be fronted by a development area from both sides. As with Primary Road Condition 1 active frontages and corner turner dwellings at the access points to these set back roads are essential. The landscaping zones in these areas are vitally important at screening the local areas from the Primary route through the site.

Crossing points between these areas will break the flow of traffic and create connection between the development areas.



7.0 URBAN STRATEGY STUDIES

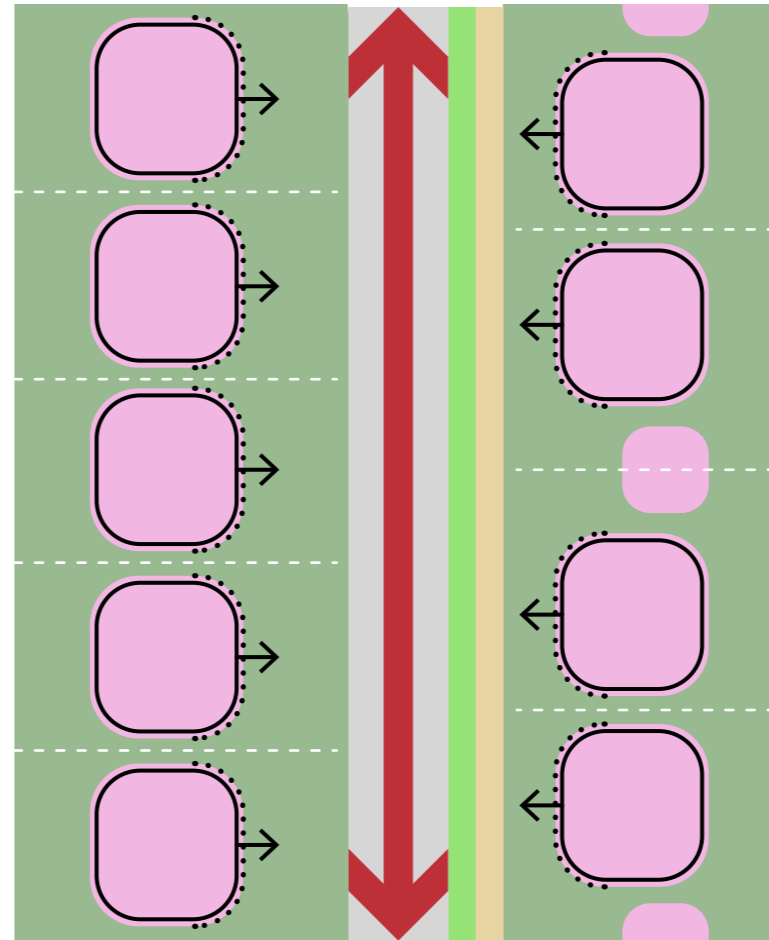
7.1.3 Secondary Road Condition 1

Building / Road / Building

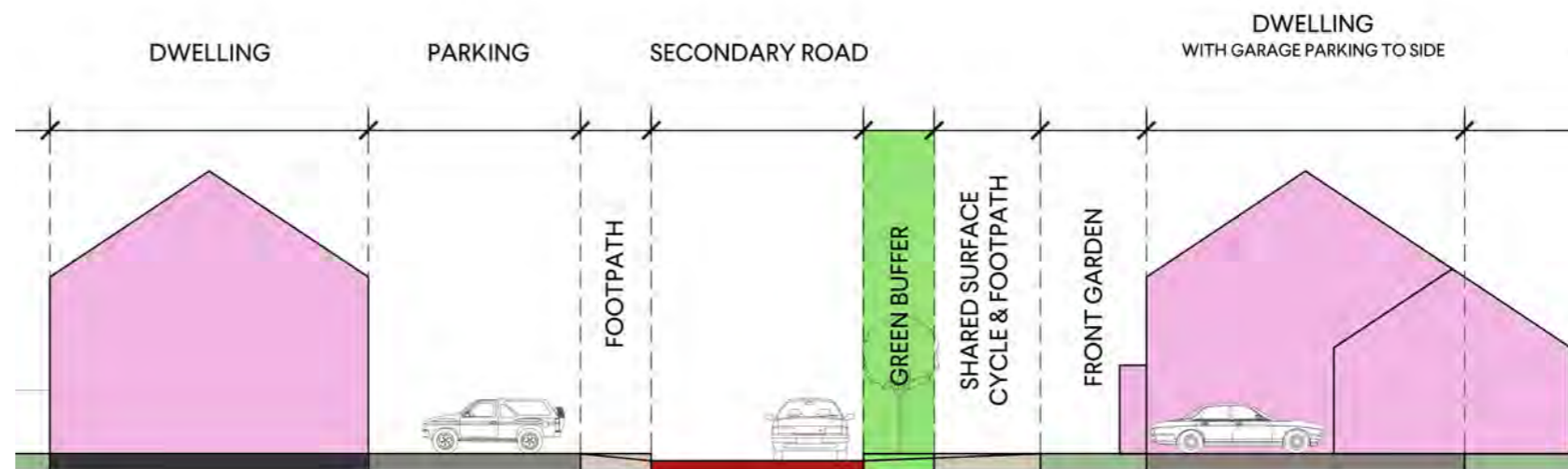
The Secondary roads become the first point of accessing dwellings directly from the road. The build up is incredibly important here to create a welcoming and attractive neighbourhood, slowing the traffic but still providing for through routes and bus access.

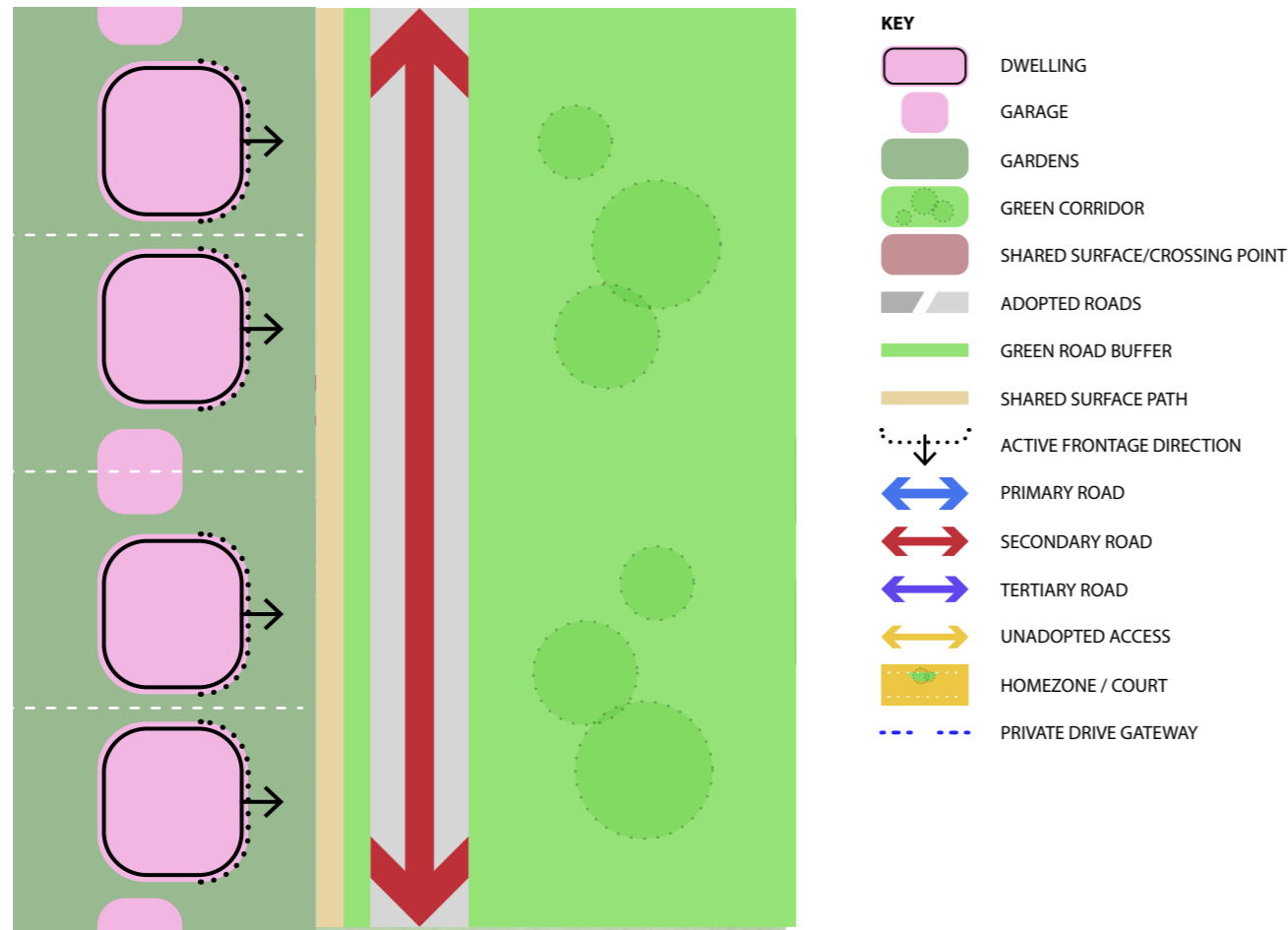
Only one side of these roads are permitted to have parking in front of the dwelling. Where this occurs there is required an 8m offset from the edge of the pavement to the front of the dwelling including a min 1m planting zone in front of the dwelling.

To the other side in-curtilage parking must occur, setting the building face closer to the road adjacent to the multi use path and landscaping strip. The landscaping strip at 2.4m will host the visitor parking bays. Rigid planting in these areas will always lead the public back to the country park and green corridor crossing points.



KEY	
	DWELLING
	GARAGE
	GARDENS
	GREEN CORRIDOR
	SHARED SURFACE/CROSSING POINT
	ADOPTED ROADS
	GREEN ROAD BUFFER
	SHARED SURFACE PATH
	ACTIVE FRONTAGE DIRECTION
	PRIMARY ROAD
	SECONDARY ROAD
	TERTIARY ROAD
	UNADOPTED ACCESS
	HOMEZONE / COURT
	PRIVATE DRIVE GATEWAY





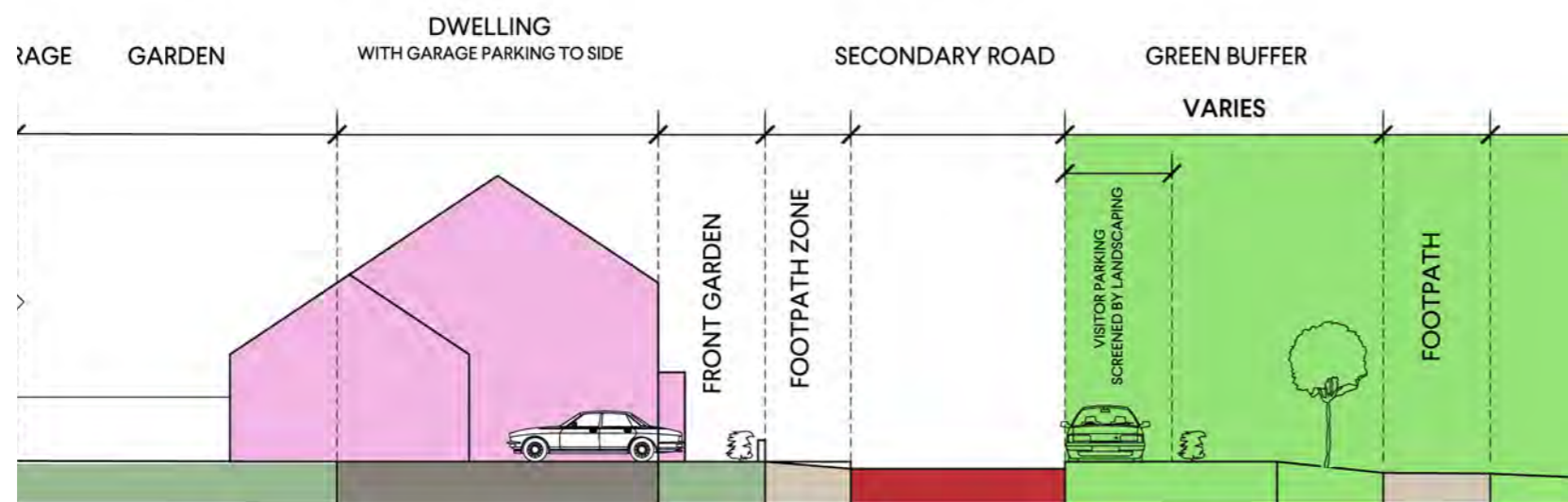
7.0 URBAN STRATEGY STUDIES

7.1.4 Secondary Road Condition 2

Building / Road / Green Edge

Where a Secondary Road has a built edge on one side and a green edge to the other, the landscape zone, multi use footpath and in-curtilage parking should be retained to create a dwelling presence in a landscape setting that isn't flooded by vehicles.

The Green spaces should always be overlooked by active frontages.



7.0 URBAN STRATEGY STUDIES

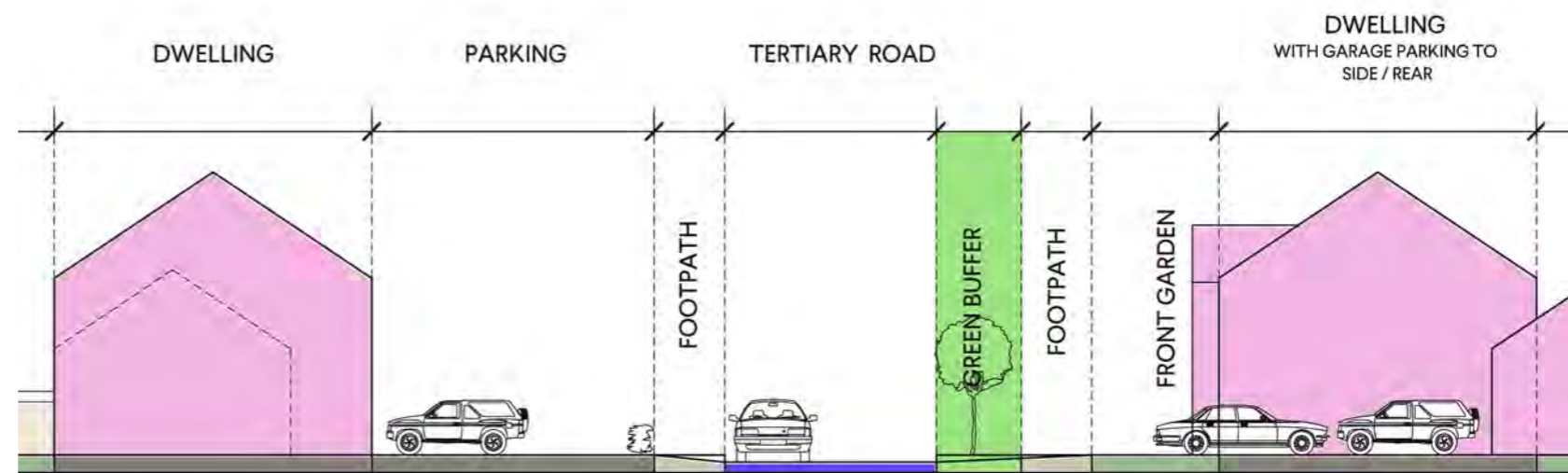
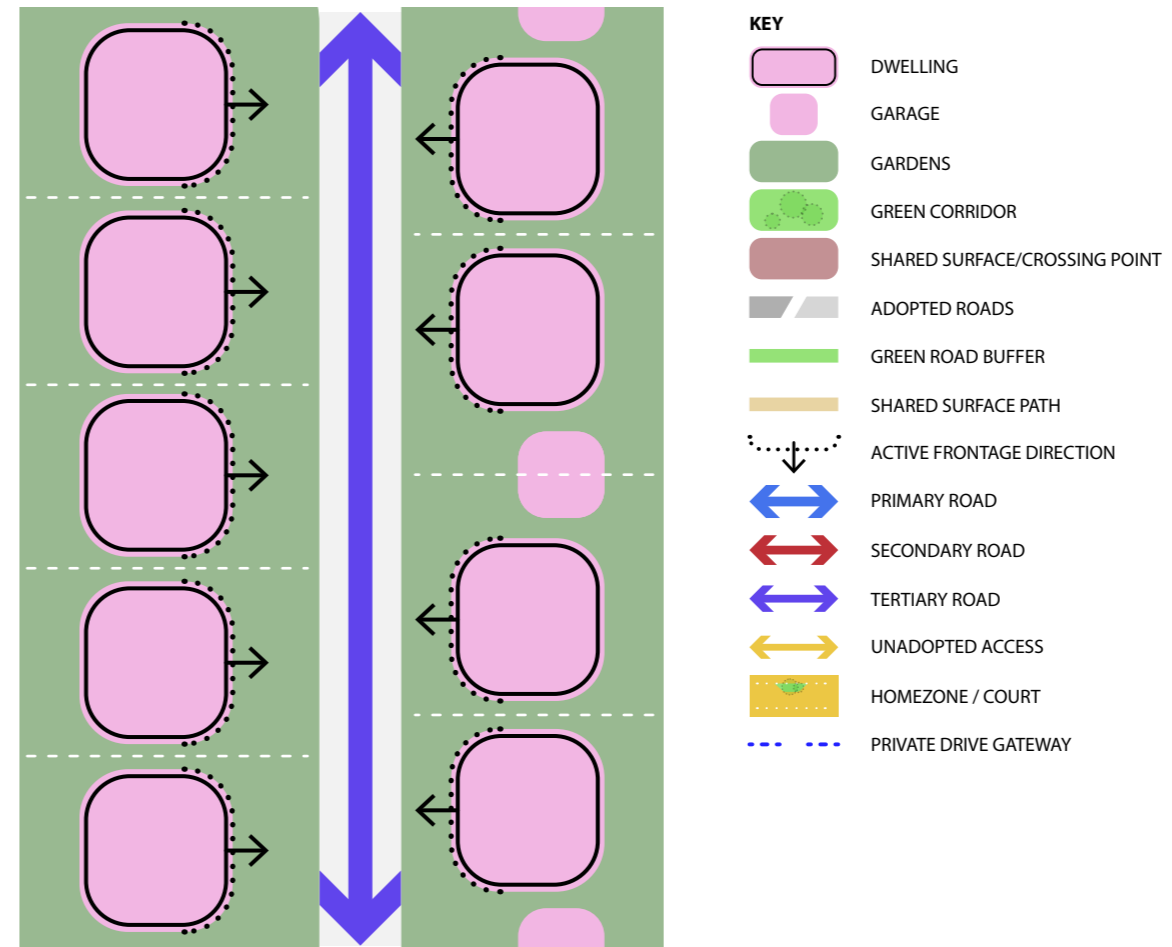
7.1.5 Tertiary Road Condition

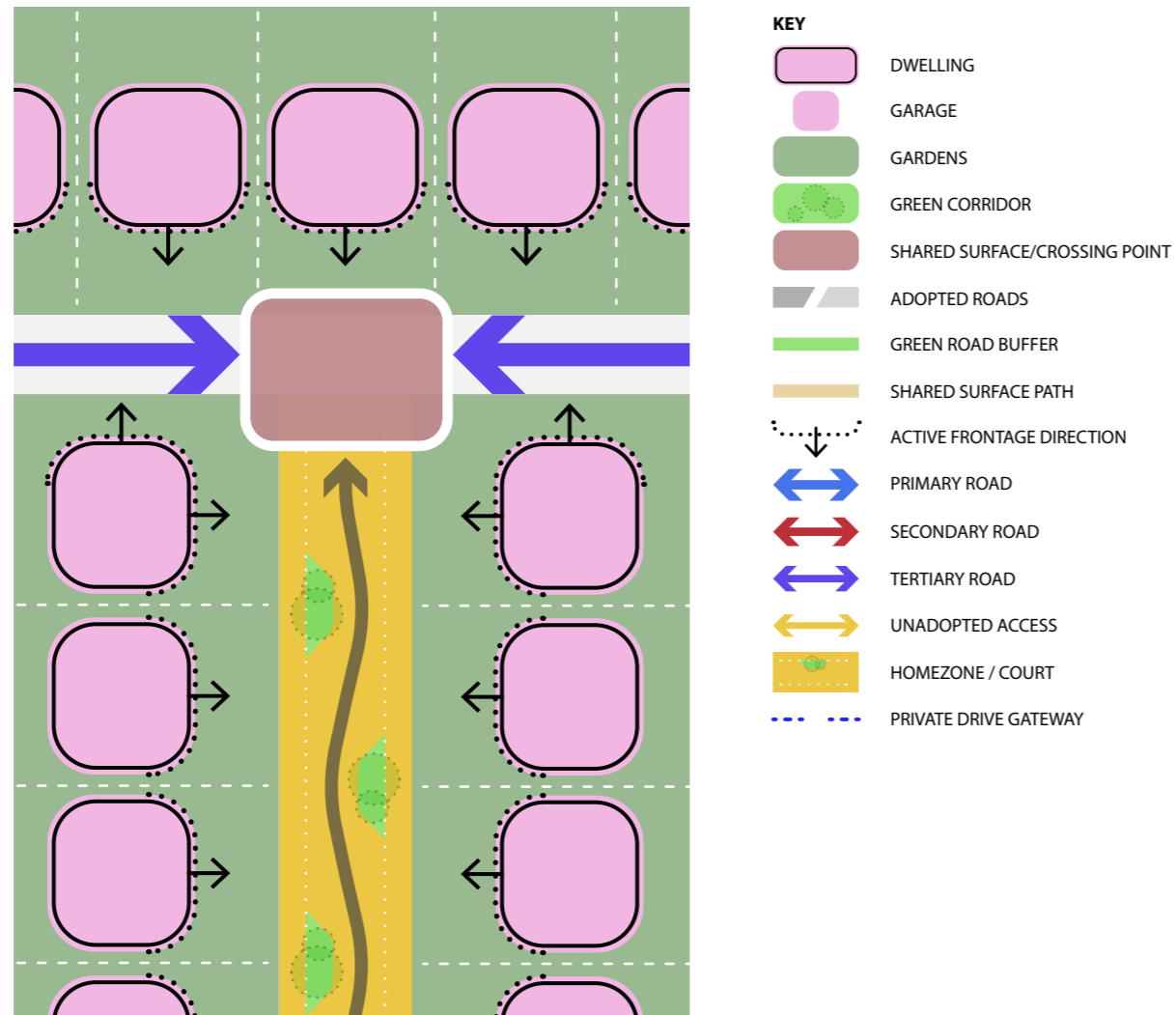
Building / Road / Building

The Tertiary Road condition much like a Secondary Road should always seek to have one side with in-curtilage parking and the other with front parking. The 8m offsets should still be applied to front parking if possible.

No multi use path is required in these areas and a slower traffic limit is enforced with a smaller carriage width, however pavements will still be required on both sides of the road. The landscape zone to host visitor parking is still utilised to create a naturalised street scene.

Higher densities of houses are expected along these streets.





- KEY**
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 - GREEN ROAD BUFFER
 - SHARED SURFACE PATH
 - ACTIVE FRONTAGE DIRECTION
 - PRIMARY ROAD
 - SECONDARY ROAD
 - TERTIARY ROAD
 - UNADOPTED ACCESS
 - HOMEZONE / COURT
 - PRIVATE DRIVE GATEWAY

7.0 URBAN STRATEGY STUDIES

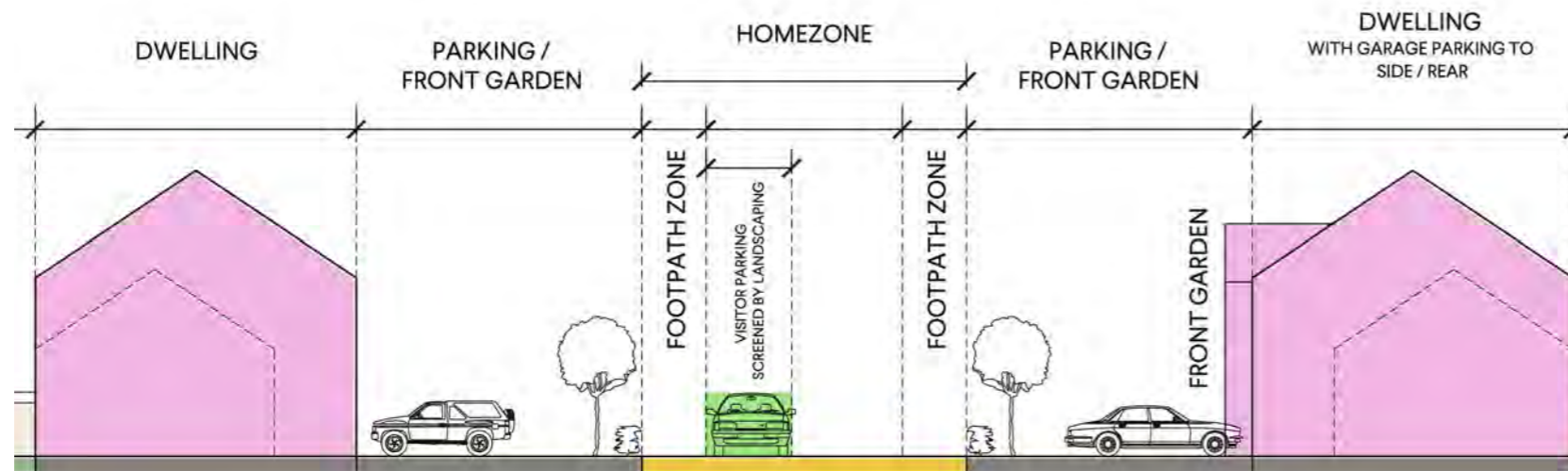
7.1.6 Home Zone Condition

Shared Surface

Home Zones will create a balanced selection of house types overlooking a shared surface area without dominance of vehicular access. Active frontages are required overlooking the space, but variety of house design is encouraged.

Overlooking and corner turner house types are required at the raised access entry points to the home zones to create a gateway feature to demark the change of activity.

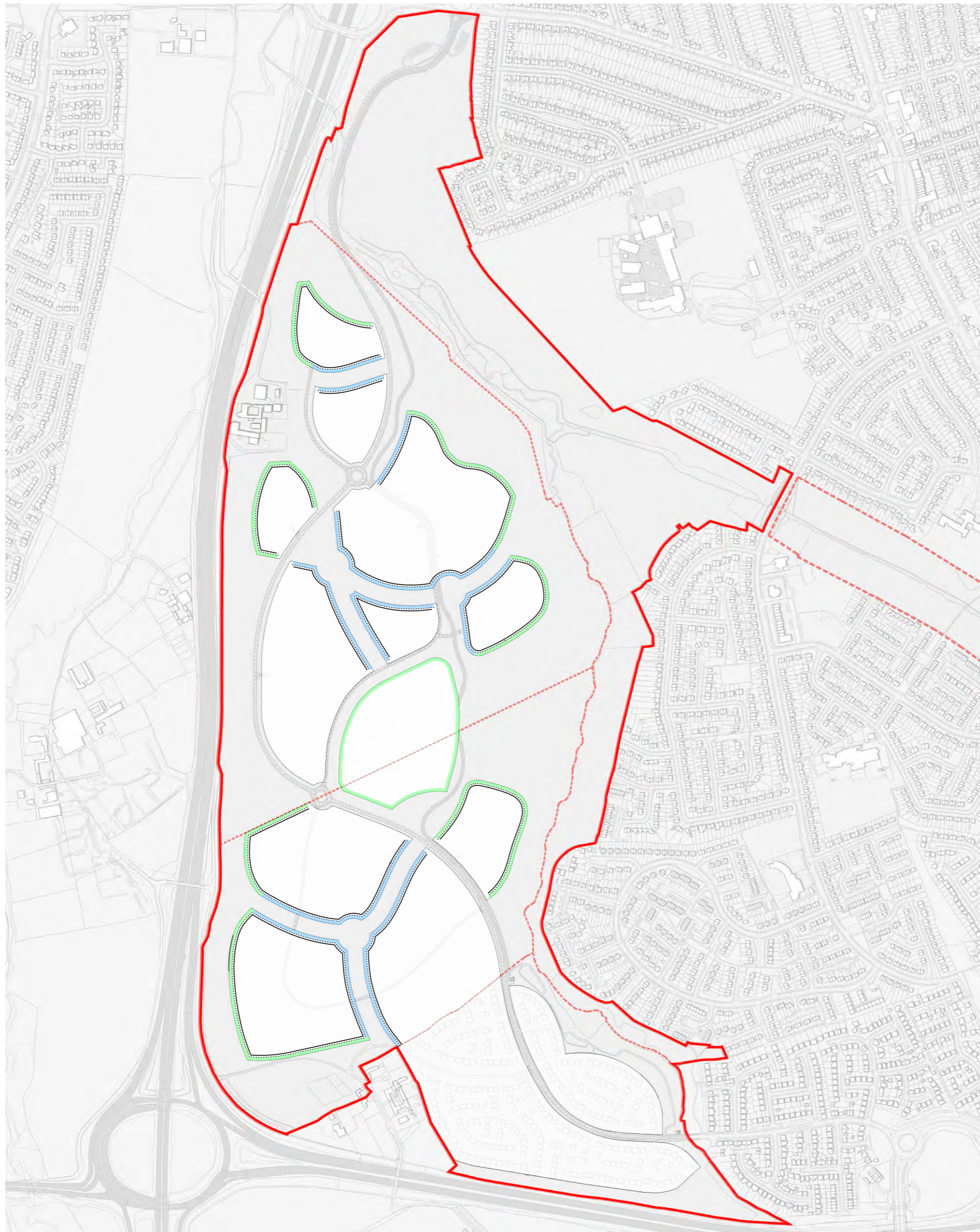
Car parking can be in front or the side of properties and alternative parking methods can be utilised here. Visitor parking will be contained within a landscaping buffer within the body of the roads to slow and filter traffic, prioritising the pedestrian.



7.0 URBAN STRATEGY STUDIES

7.2 Edge Conditions





7.0 URBAN STRATEGY STUDIES

7.2 Edge Conditions

Landscape Corridor and Green Park Edge Principles

Well-designed edge conditions are paramount to the success of the masterplan. Dwellings should be positioned to interact with parkland and provide an enriched amenity for the residents as well as to passively police the masterplan for security and encourage ownership of the public realm.




Park edges should be appropriately permeable in parallel with the landscape strategy and avoid vehicular domination. Areas of the north and north east edge may be treated in a variety of ways in order to limit the presence of vehicles on the park setting. Park edges are to be addressed with residential frontages and animated gables / secondary frontages. Stretches of rear fence conditions for any more than two consecutive dwellings is not acceptable.

There are a few conditions which are considered appropriate to provide the necessary vehicular servicing and accommodation whilst promoting the green edge conditions:

- 7.2.1 Private Drive Frontage
- 7.2.2 Turning Head Serving Private Drives
- 7.2.3 Pedestrian Frontage
- 7.2.4 Pedestrian Corner Condition
- 7.2.6 Green Corridor Condition

The urban strategy studies in this section contain the key design principles that will be expected to be accommodated in the final design solution. The plans and cross sections are included for illustrative purposes and provide one potential solution. It is recognised that these may not be the actual solution settled upon.

KEY

-  Building Frontages
Dotted Lines show the Building Frontages Direction. Building Frontages facing towards the external condition of each development area creating active developments.
-  Development Edge - to Green/SuD's Corridor
Development edge borders a Green/SuD's corridor in these locations.
-  Development Edge - Adjacent to Green Space
Development edge borders a Green space to the edge of the development areas in these locations.



7.0 URBAN STRATEGY STUDIES

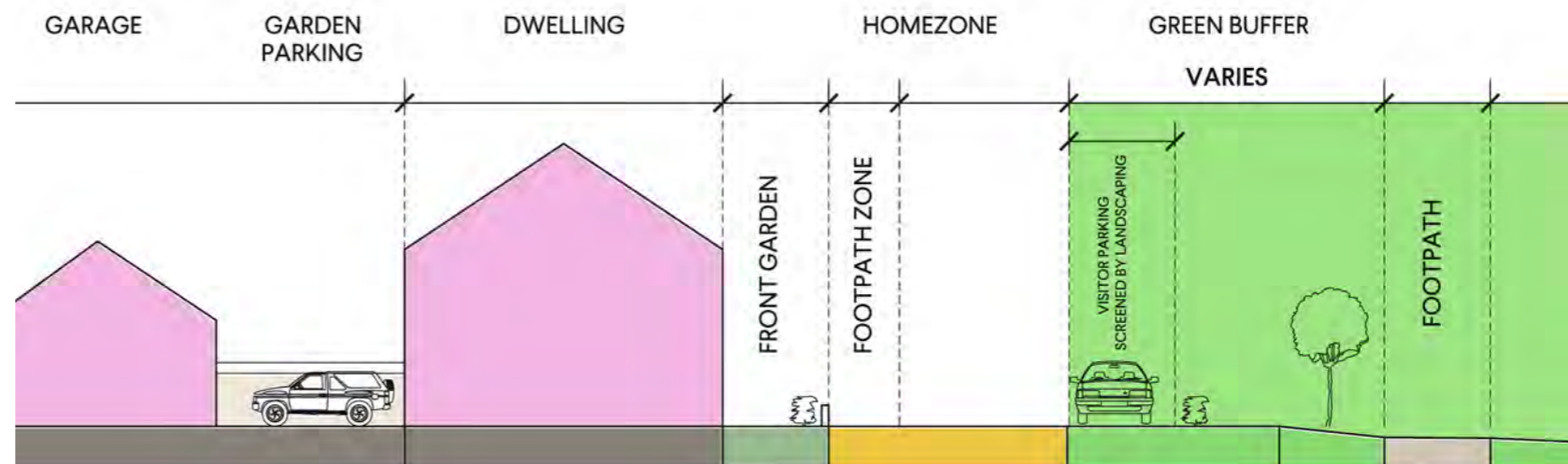
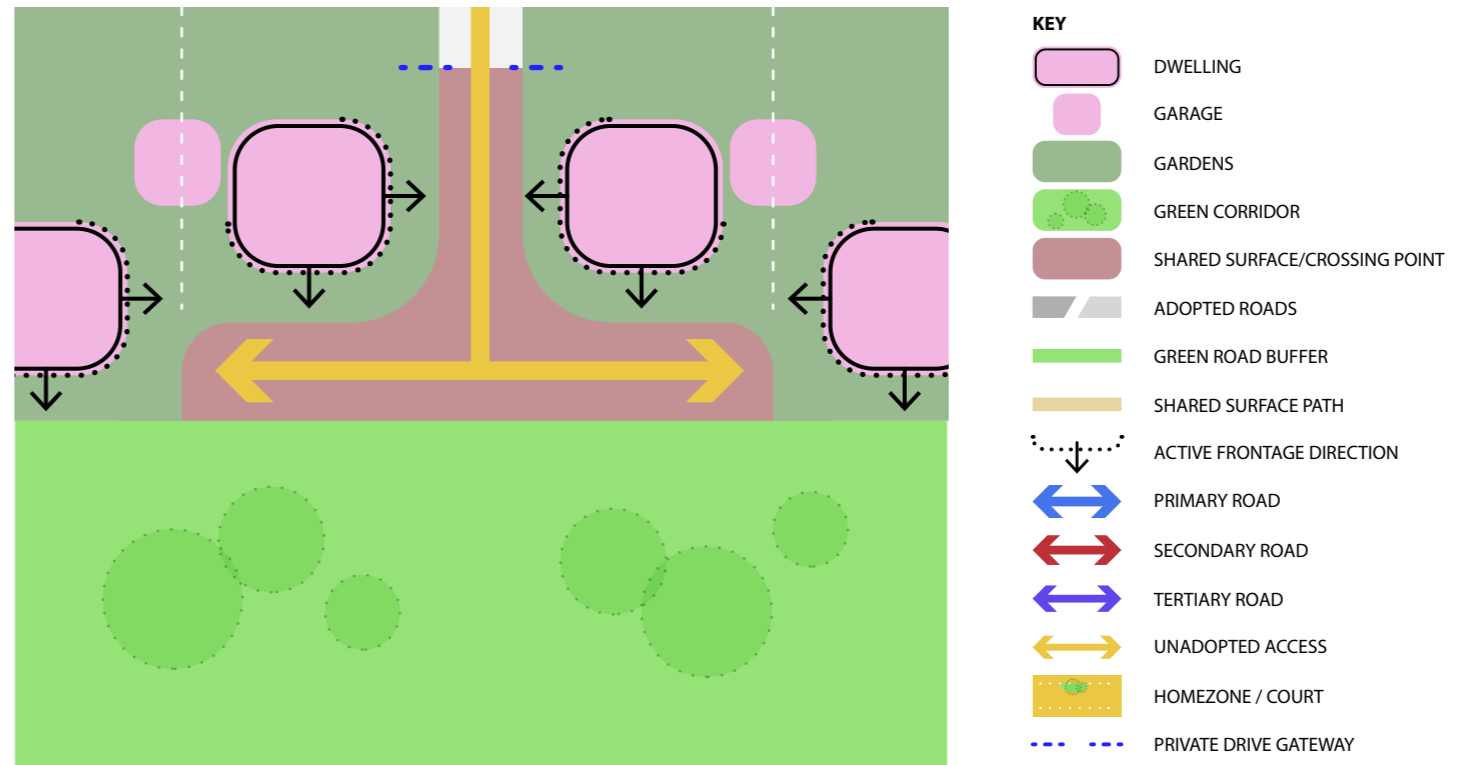
7.2.1 Landscape Edge Condition 1

Private Drive Frontage

Where the development zones front onto Green Open Space, utilising private drives could be an option. Accessed from a secondary or tertiary road, these private zones could give access to a limited number of dwellings, to be discussed with the planning authority, in order to soften the impact of vehicle access on the park setting.

Parking is to be provided to the side of dwellings behind the building line. Areas of visitor parking to be provided but screened by landscaping.

Physical barriers to the landscaped edge are to be excluded with the exception of structured planting to conceal visitor parking.



7.0 URBAN STRATEGY STUDIES

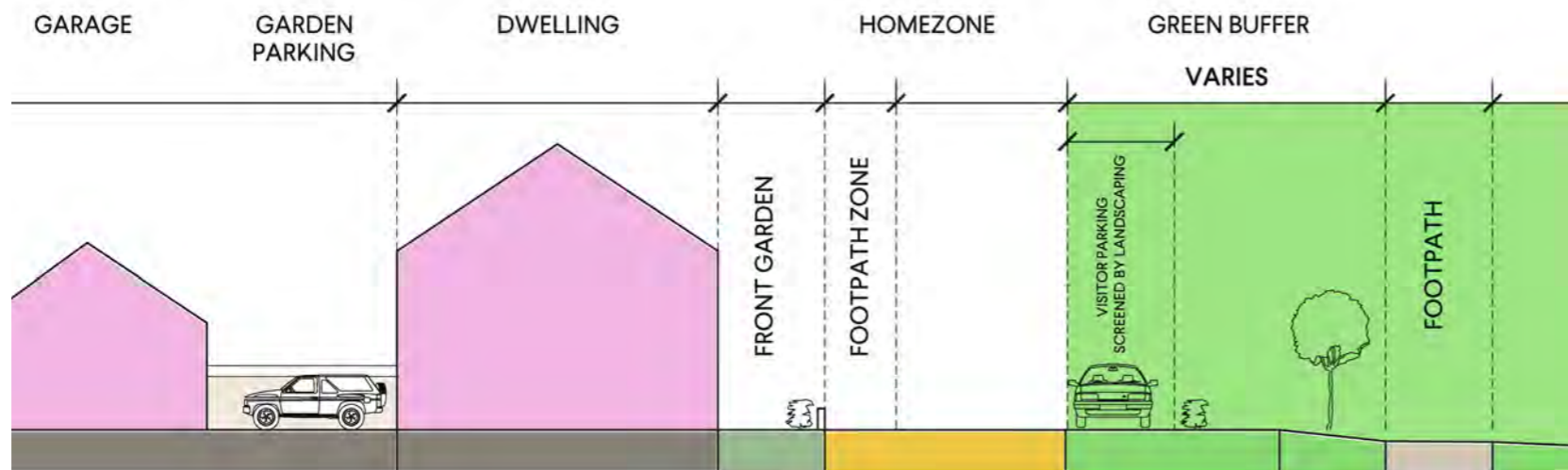
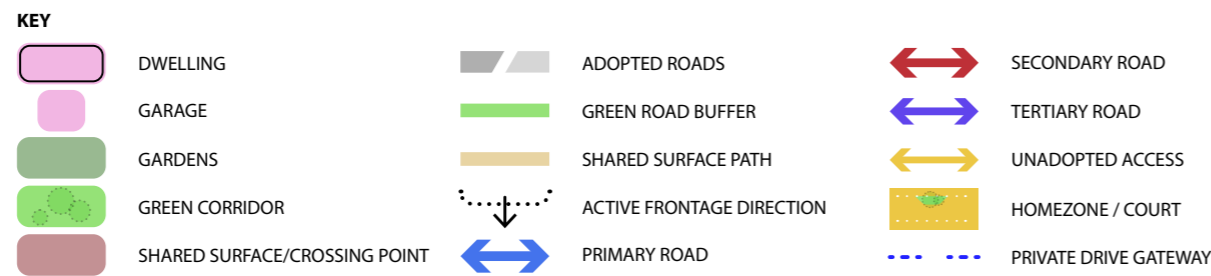
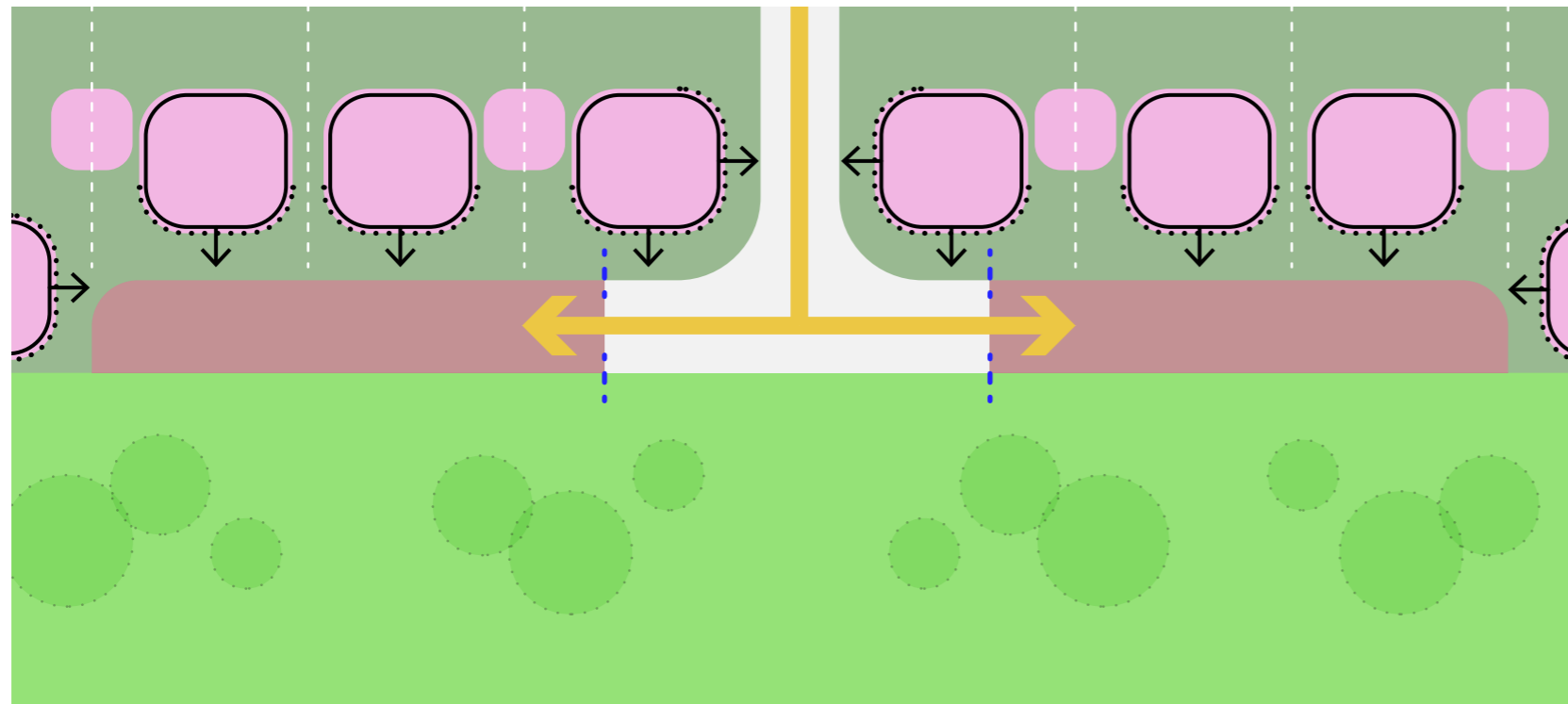
7.2.2 Landscape Edge Condition 2

Turning Head Serving Private Drives

An alternative option is to access a pair of private drives from a secondary road or tertiary road giving access to up to double the amount of dwellings between the 2 private drives, in order to soften the impact of vehicle access on the park setting.

Parking to be provided to the side of dwellings behind the building line. Areas of visitor parking to be provided but screened by landscaping.

Physical barriers to the landscaped edge are to be excluded with the exception of structured planting to conceal visitor parking.



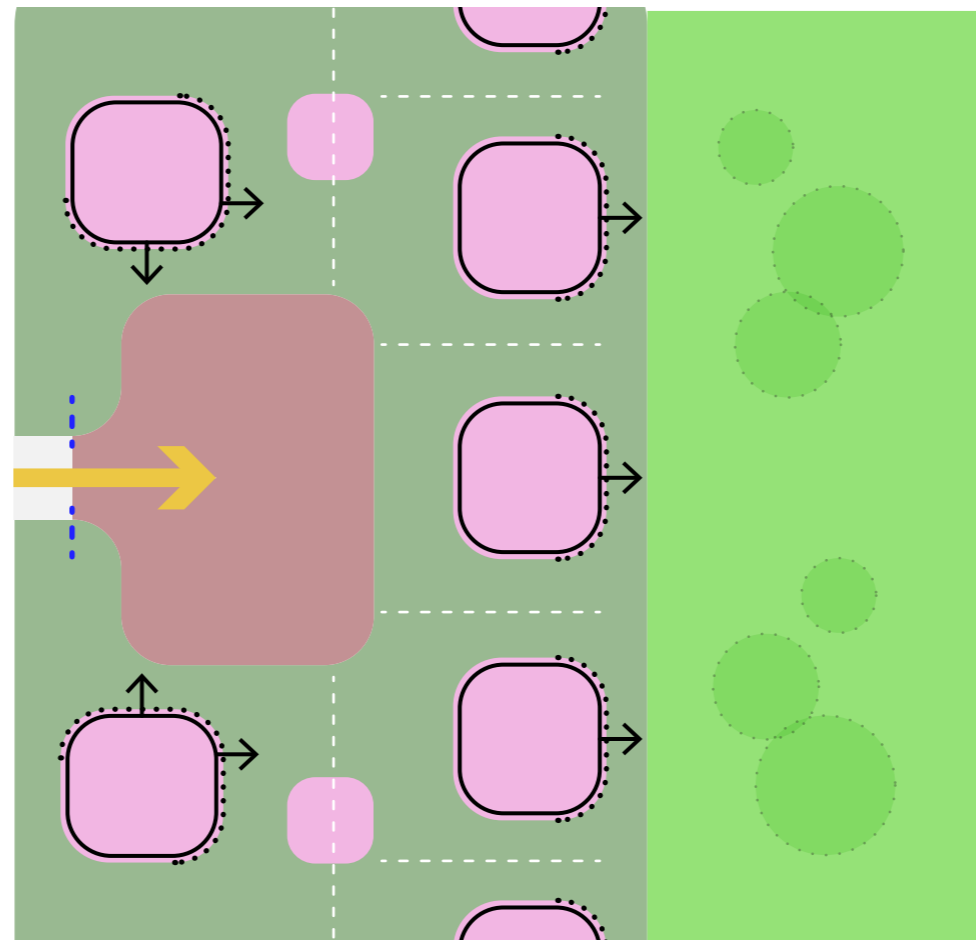
7.0 URBAN STRATEGY STUDIES

7.2.3 Landscape Edge Condition 3

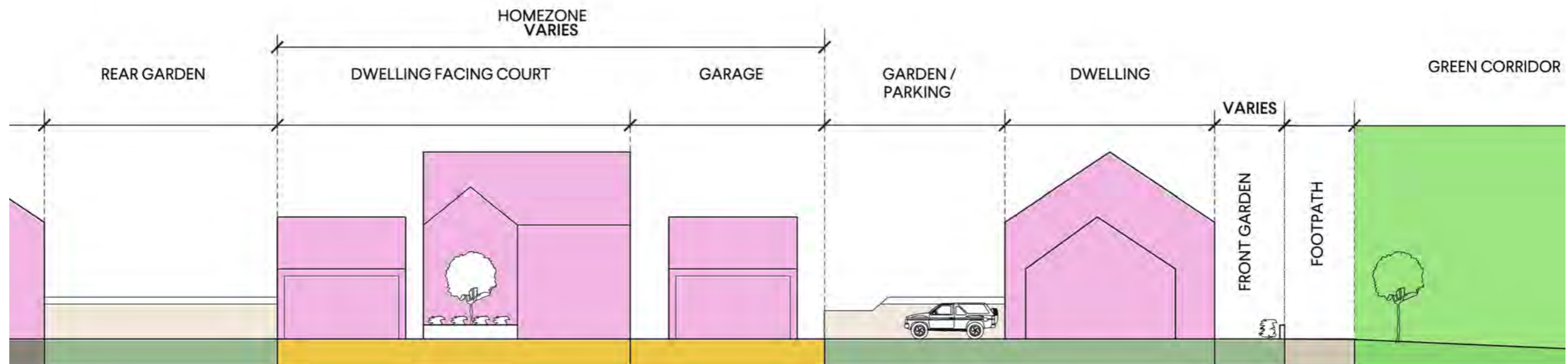
Pedestrian Frontage

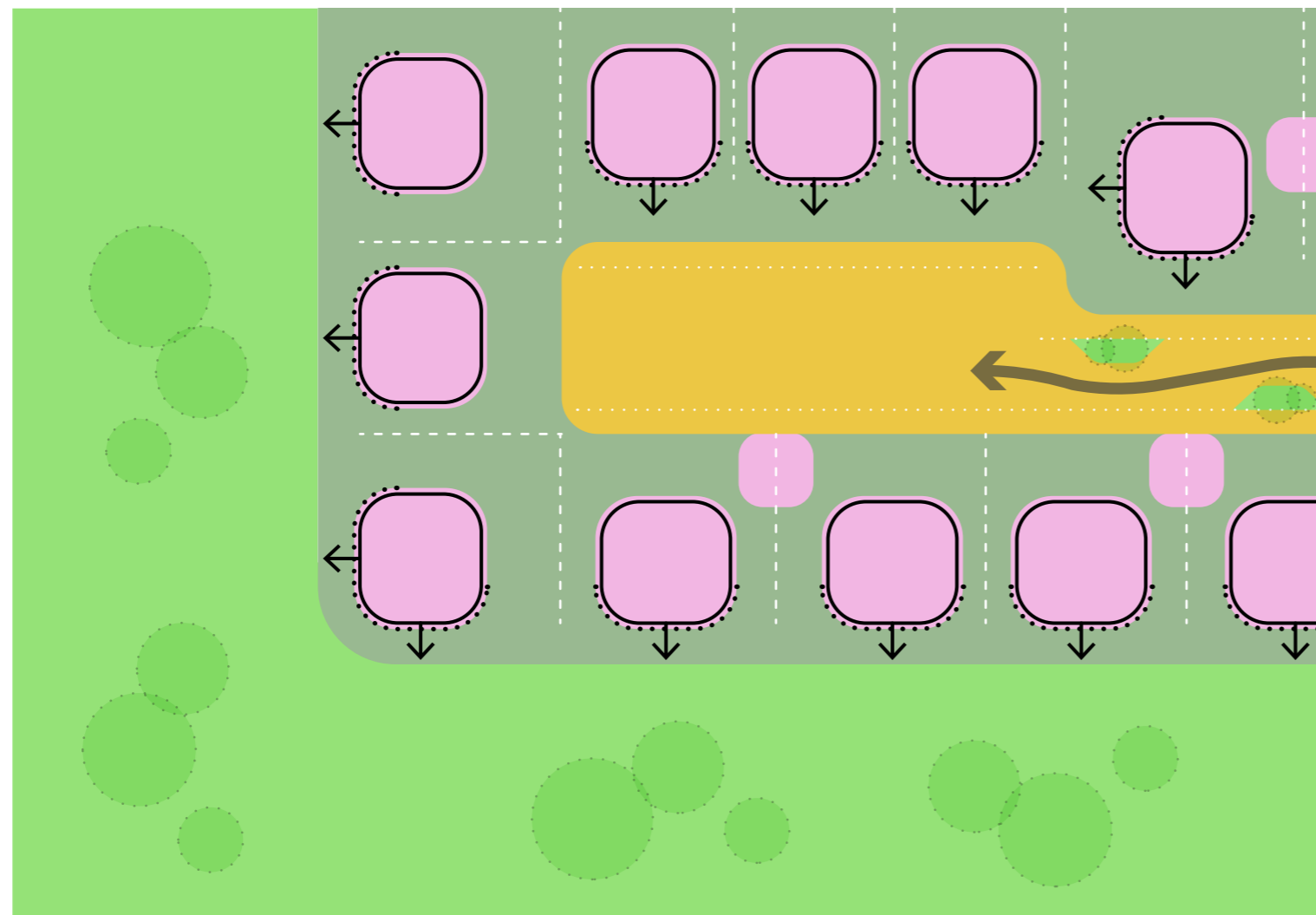
In order to provide an uninterrupted frontage of dwellings addressing the park, containing vehicles behind dwellings is also acceptable. This can be appropriate in areas of higher density.

Rear parking areas should serve no more than five dwellings in a similar way to a private drive. Providing a secondary frontage and surveillance onto these areas is necessary. Areas of reduced height fencing and permeations should be provided to facilitate. Private Drives should have notional change from public highway to semi-private zone with raised texture rumble strips, a change in surface material and gate posts.



- KEY**
- DWELLING
 - GARAGE
 - GARDENS
 - GREEN CORRIDOR
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 - GREEN ROAD BUFFER
 - SHARED SURFACE PATH
 - ACTIVE FRONTAGE DIRECTION
 - PRIMARY ROAD
 - SECONDARY ROAD
 - TERTIARY ROAD
 - UNADOPTED ACCESS
 - HOMEZONE / COURT
 - PRIVATE DRIVE GATEWAY





KEY

	DWELLING		ADOPTED ROADS		SECONDARY ROAD
	GARAGE		GREEN ROAD BUFFER		TERTIARY ROAD
	GARDENS		SHARED SURFACE PATH		UNADOPTED ACCESS
	GREEN CORRIDOR		ACTIVE FRONTAGE DIRECTION		HOMEZONE / COURT
	SHARED SURFACE/CROSSING POINT		PRIMARY ROAD		PRIVATE DRIVE GATEWAY

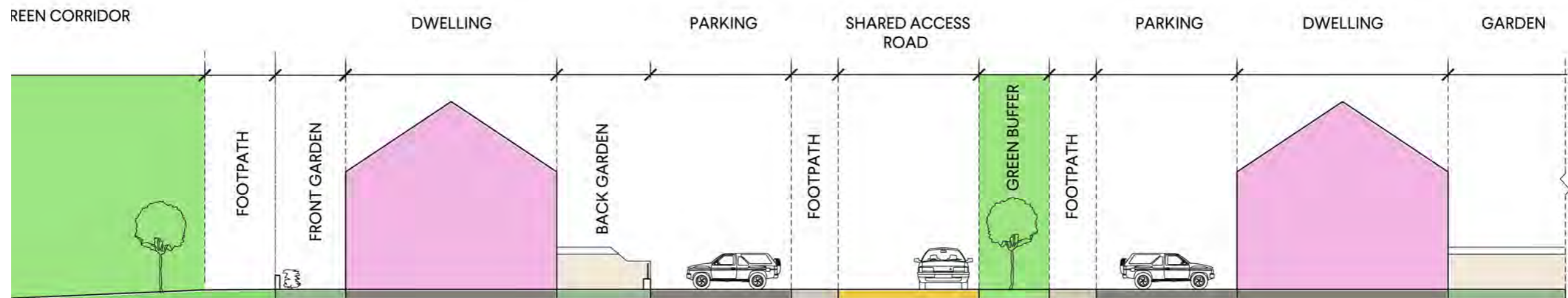
7.0 URBAN STRATEGY STUDIES

7.2.4 Landscape Edge Condition 4

Pedestrian Corner Condition

Where development areas form a corner fronting green open space uninterrupted frontage of dwellings should be implemented and containing vehicles behind dwellings is also acceptable.

In these instances homezone principles should be applied, serving up to 15 dwellings. Providing a secondary frontage and surveillance onto these areas is absolutely necessary. Areas of reduced height fencing and permeations should be provided to facilitate. A shared surface should be used in these areas and parking in front of dwellings is permitted requiring that landscaping is integrated to the scheme. Visitor parking can be contained within a landscaping buffer within the body of the roads to slow and filter traffic, prioritising the pedestrian.



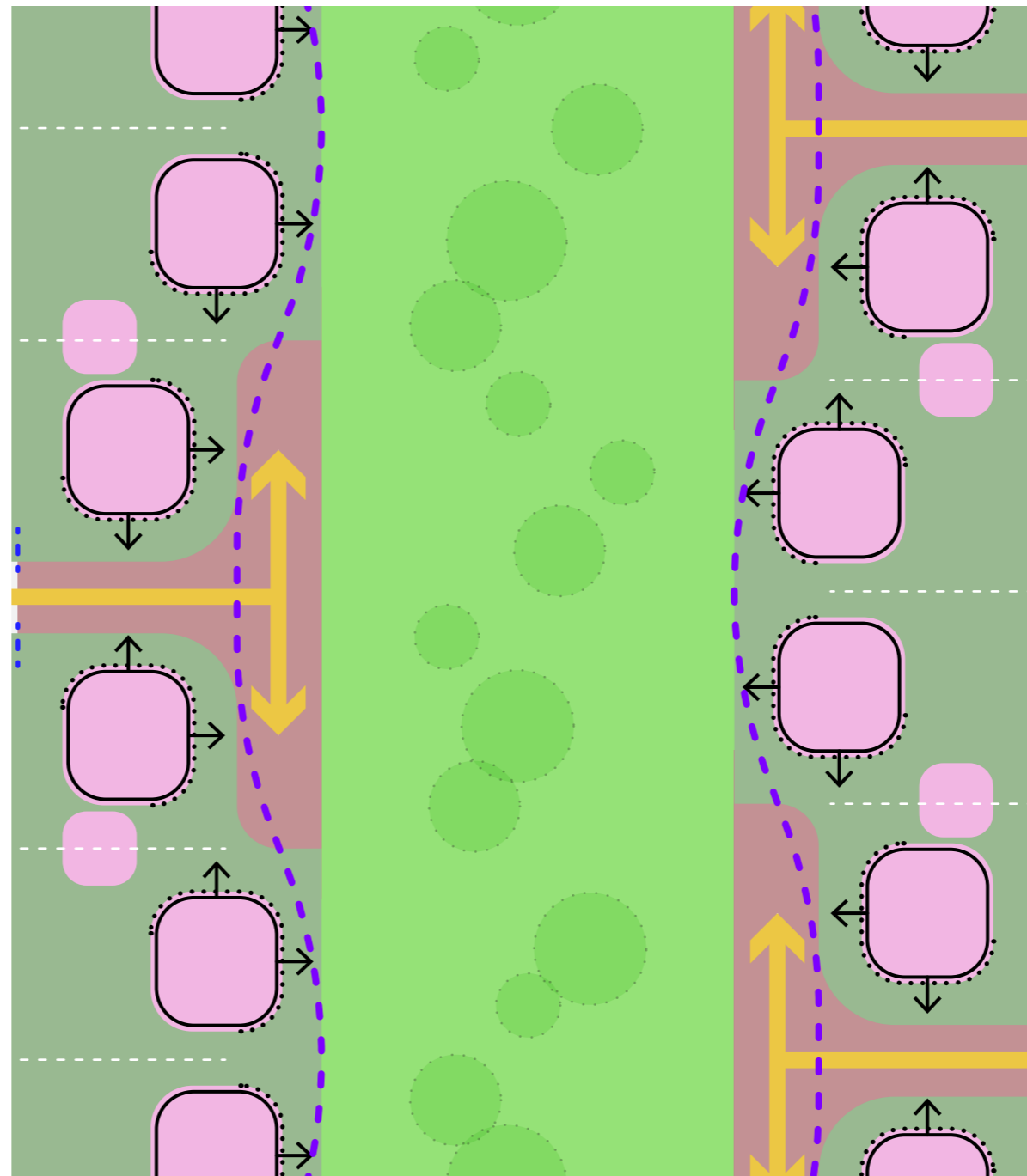
7.0 URBAN STRATEGY STUDIES

7.2.6 Landscape Edge Condition 6

Green Corridor Condition

Where two development zones front onto Green corridors there should be active frontage along the length. This could implement a number of the conditions highlighted.

It is important that in these areas a variety of housetypes and layouts are utilised to create a varied build line. This will create a greater pedestrian priority rather than building dominant lines in these areas, allowing for permeable routes into the development areas. Additional planting should be integrated into these private drives and frontages so that the green open space feels like it isn't limited to outside of the development lines.



KEY	
	DWELLING
	GARAGE
	GARDENS
	GREEN CORRIDOR
	SHARED SURFACE/CROSSING POINT
	ADOPTED ROADS
	GREEN ROAD BUFFER
	SHARED SURFACE PATH
	ACTIVE FRONTAGE DIRECTION
	PRIMARY ROAD
	SECONDARY ROAD
	TERTIARY ROAD
	UNADOPTED ACCESS
	HOMEZONE / COURT
	PRIVATE DRIVE GATEWAY

DWELLING WITH GARAGE PARKING TO SIDE

PRIVATE ROAD

GREEN CORRIDOR & SUDS ZONE (INCLUDING PARKS AND PEDESTRIAN ROUTES TO WIDER SITE)

DWELLING



7.0 URBAN STRATEGY STUDIES

7.3 Feature Locations



7.0 URBAN STRATEGY STUDIES

7.3 Feature Locations

In the process of masterplan development, a series of nodal points have arisen at the point of convergence of landscape and infrastructure network which are considered feature locations.

Aligning with guidance of Building For Life 12 and Manual For Streets, these points are key to establish individual areas of character and place. The nodes assist wayfinding and points of reference.

7.3.1 Green Corridor Crossing

Notional gateways define the passage between two spaces within the site. Gateways are to exist between residential clusters fragmented by landscape and pedestrian routes. Gateways are key to traffic calming and instilling a landscape and pedestrian hierarchy over vehicular infrastructure.

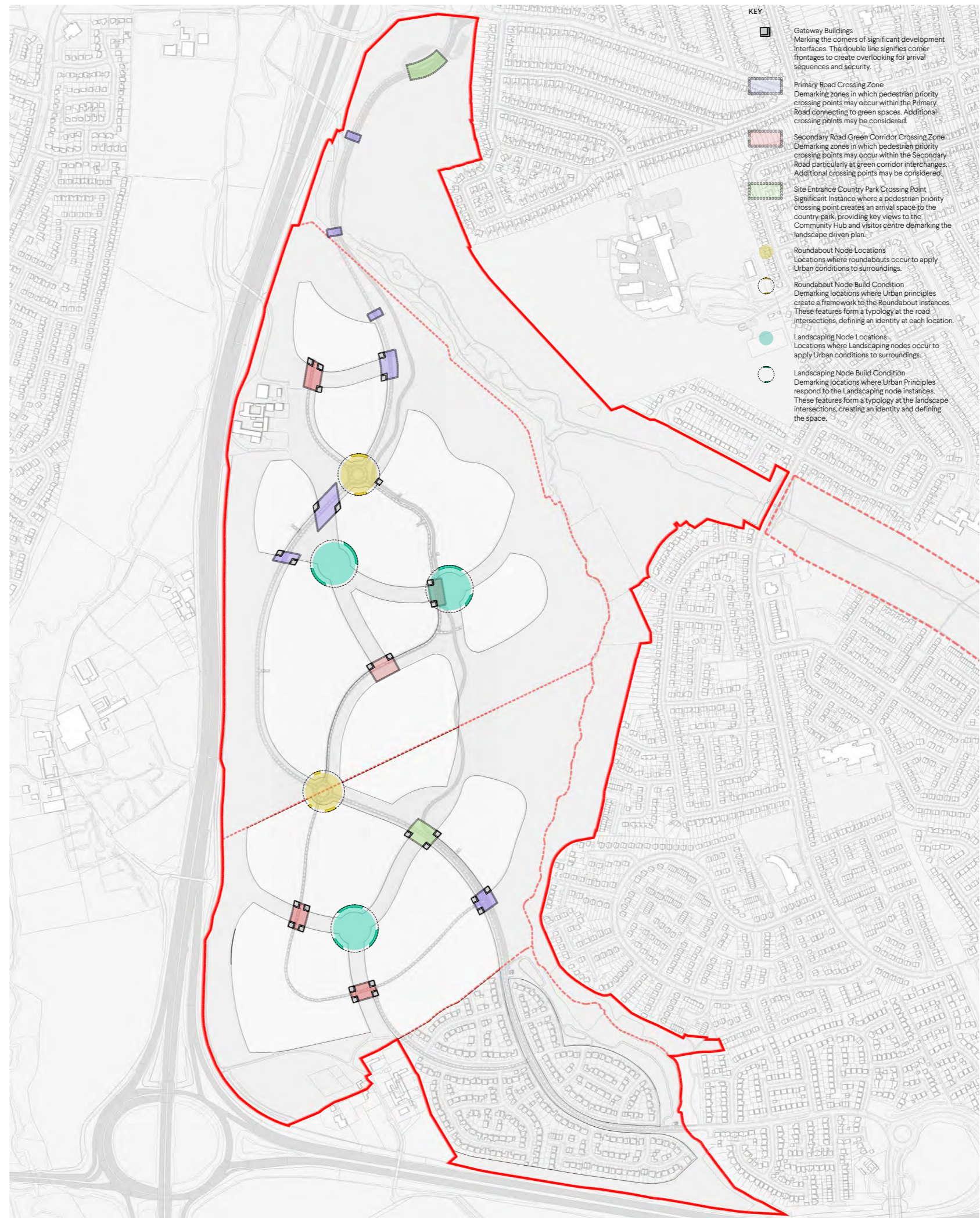
Gateways are to be defined at the passage of vehicular links through landscape space. Treatment of road surface and dwelling location and orientation contribute to the character of the gateway.

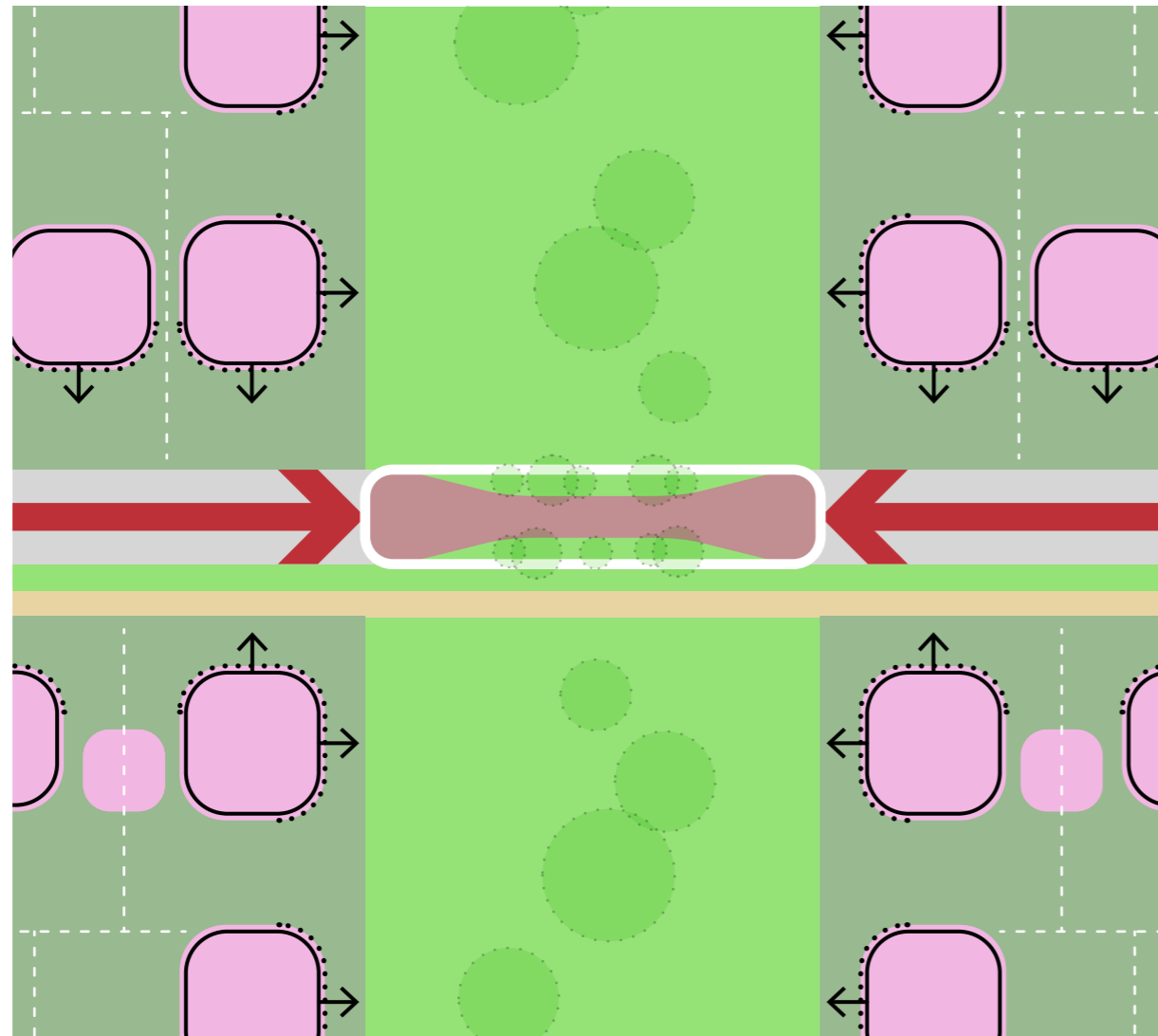
7.3.2 Roundabout Condition and Rear Parking Court

The roundabout locations create a node of vehicular distribution. They are typically more heavily trafficked areas which should be designed with hard building edges to offer a visual and noise screening to the residential amenity beyond.

7.3.3 Landscape Nodes

Where multiple green corridors converge a landscaping node occurs, surrounding by development areas. These nodes create an opportunity to provide 'Locally Equipped Areas for Play' (LEAP) and create public convergence points within the landscaping zone. How these areas are fronted are key to defining space and creating routes for pedestrians from the development zones to these nodal points.





KEY					
	DWELLING		ADOPTED ROADS		SECONDARY ROAD
	GARAGE		GREEN ROAD BUFFER		TERTIARY ROAD
	GARDENS		SHARED SURFACE PATH		UNADOPTED ACCESS
	GREEN CORRIDOR		ACTIVE FRONTAGE DIRECTION		HOMEZONE / COURT
	SHARED SURFACE/CROSSING POINT		PRIMARY ROAD		PRIVATE DRIVE GATEWAY

7.0 URBAN STRATEGY STUDIES

7.3.1 Green Corridor Crossing

Where a secondary road passes between development zones and crosses a green corridor a crossing point should be formed in line with Section 6.5.2 Secondary Road Crossing Points.

At these gateway features corner turner houses should be utilised to create an active frontage to the road and green corridor at each corner of this crossing point. Where dwellings front on to the road or green corridor active frontages are required and should be implemented in line with the principles discussed.



7.0 URBAN STRATEGY STUDIES

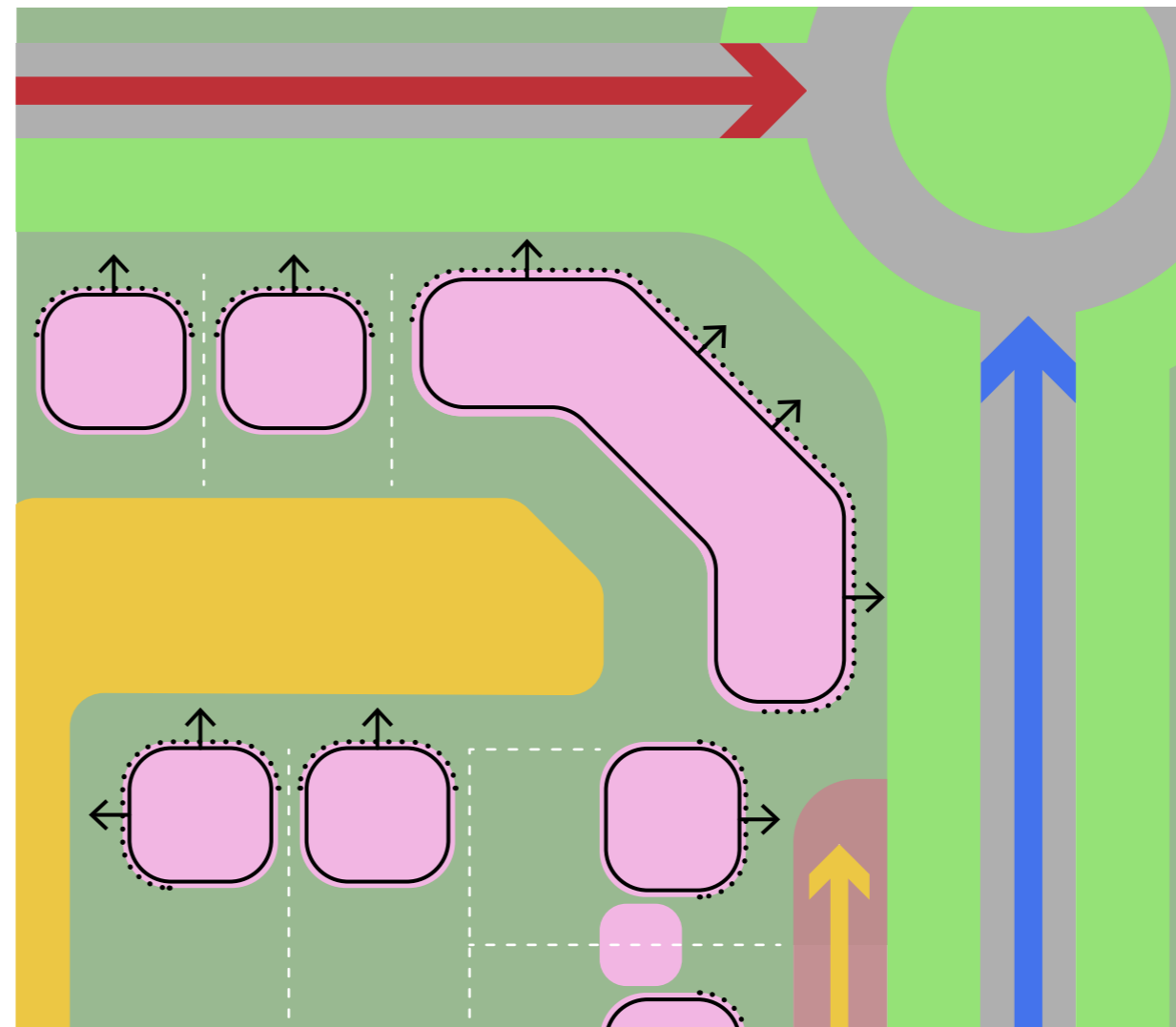
7.3.2 Roundabout Condition and Parking Court

Where roundabouts occur within the development this provides an opportunity to create a residential hard edge facing onto these vehicular nodal points.

One possible arrangement is the implementation of a dense massing block that could be used as terraced dwellings or apartments. This would then create a set back active frontage behind a landscaping strip for overlooking to the roundabout area,

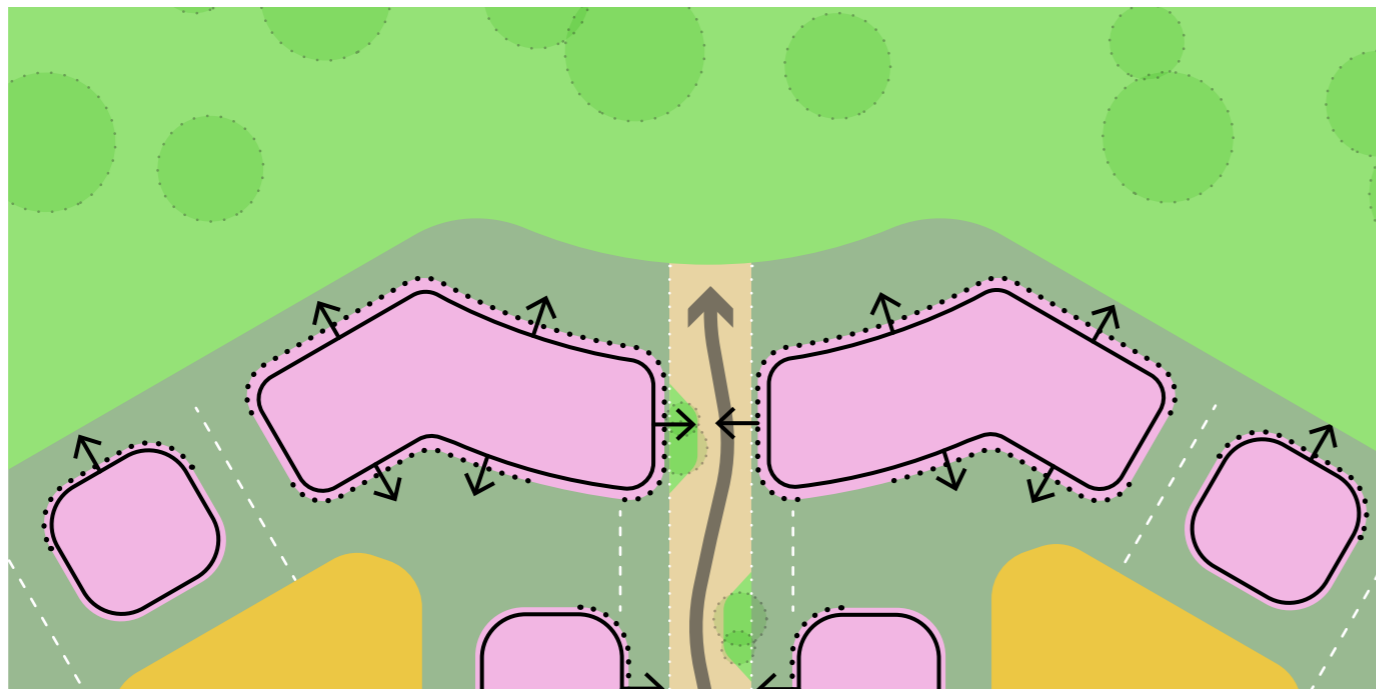
In the situation of a roundabout where frontal vehicular access cannot be achieved, rear parking access is permitted. In such situations, vehicles should be parked on plot and rear gardens addressing access courts should have a low level landscape buffer. Areas of rear parking are to be addressed by dwelling frontages on at least one side for surveillance purposes.

Surfaces of such courts should be treated as homezones, engineered to enable vehicle turning but softened with small pockets to landscaping.

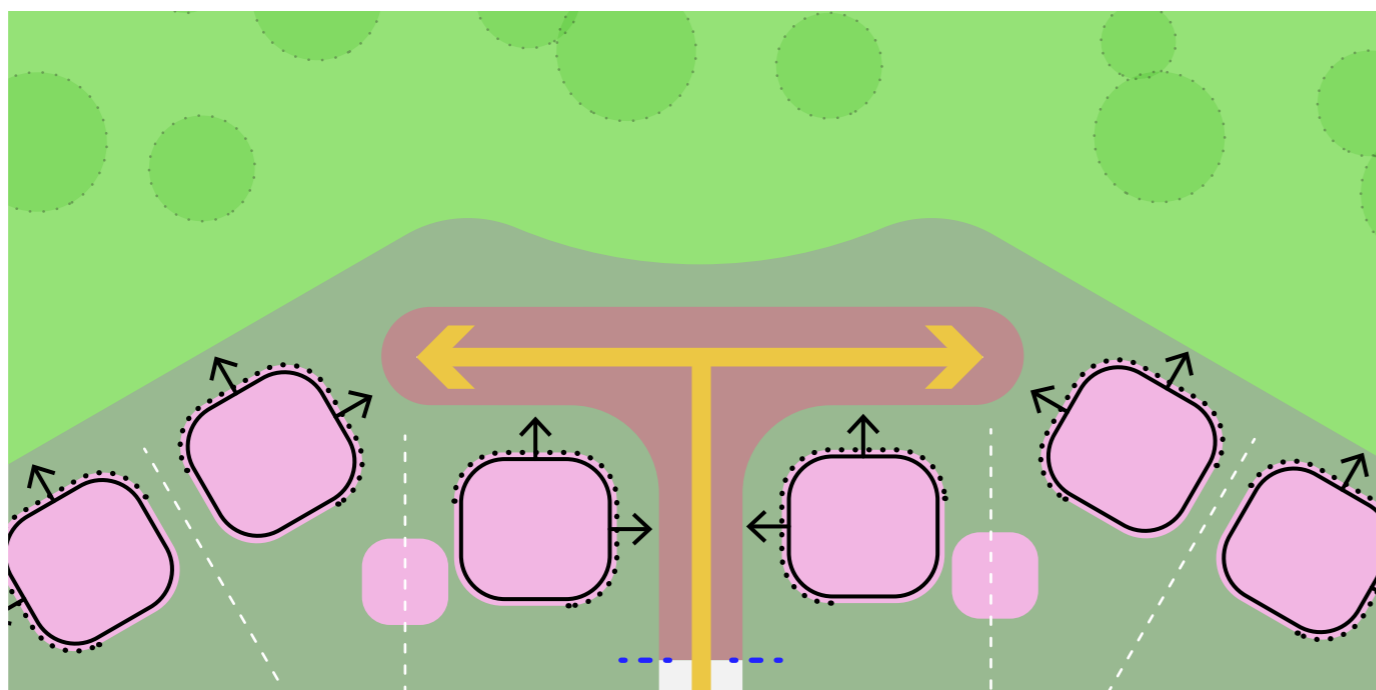


KEY			
	DWELLING		SECONDARY ROAD
	GARAGE		TERTIARY ROAD
	GARDENS		UNADOPTED ACCESS
	GREEN CORRIDOR		HOMEZONE / COURT
	SHARED SURFACE/CROSSING POINT		PRIVATE DRIVE GATEWAY
	ADOPTED ROADS		PRIMARY ROAD
	GREEN ROAD BUFFER		
	SHARED SURFACE PATH		





Condition 1



Condition 2

KEY					
	DWELLING		ADOPTED ROADS		SECONDARY ROAD
	GARAGE		GREEN ROAD BUFFER		TERTIARY ROAD
	GARDENS		SHARED SURFACE PATH		UNADOPTED ACCESS
	GREEN CORRIDOR		ACTIVE FRONTAGE DIRECTION		HOMEZONE / COURT
	SHARED SURFACE/CROSSING POINT		PRIMARY ROAD		PRIVATE DRIVE GATEWAY

7.0 URBAN STRATEGY STUDIES

7.3.3 Landscape Nodes

Where landscape nodes occur it is important that dwellings actively front onto the green open space from all sides. This residential edge defines a sense of place and assists way-finding. Conditions 1 and 2 show examples of how this could be achieved.

Condition 1

One possible option of addressing a landscape node is by creating a residential hard edge that reacts to the shape of the convergence point.

These blocks could be utilised as terraces or apartments and would permit rear parking that could be accessed via a homezone environment. Corner dwellings are always to have dual aspect to provide frontage to the park, providing a safe and secure amenity for residents. Areas of rear parking are to be addressed by dwelling frontages on at least one side for surveillance purposes.

A pedestrian priority environment could then be created, and dwelling pairs at the entrance of these pedestrian routes are to create gateways. As a minimum, one dwelling must have dual aspect to provide surveillance to the pedestrian area. Where spatial separation permits, both gateway houses are to do so. Back fences addressing public spaces should be minimised and limited to private parking areas where possible.

Condition 2

To enable variation, a derivative of 'Section 7.2.1 Landscape Edge Condition 1 - Private Drive' could be applied to allow vehicular access to the front of residential dwellings in a similar to park edge treatment. Active frontage should be utilised along the green edges as well as access roads to the private drives, as these would create public routes of access to the landscape nodes from the development zones.



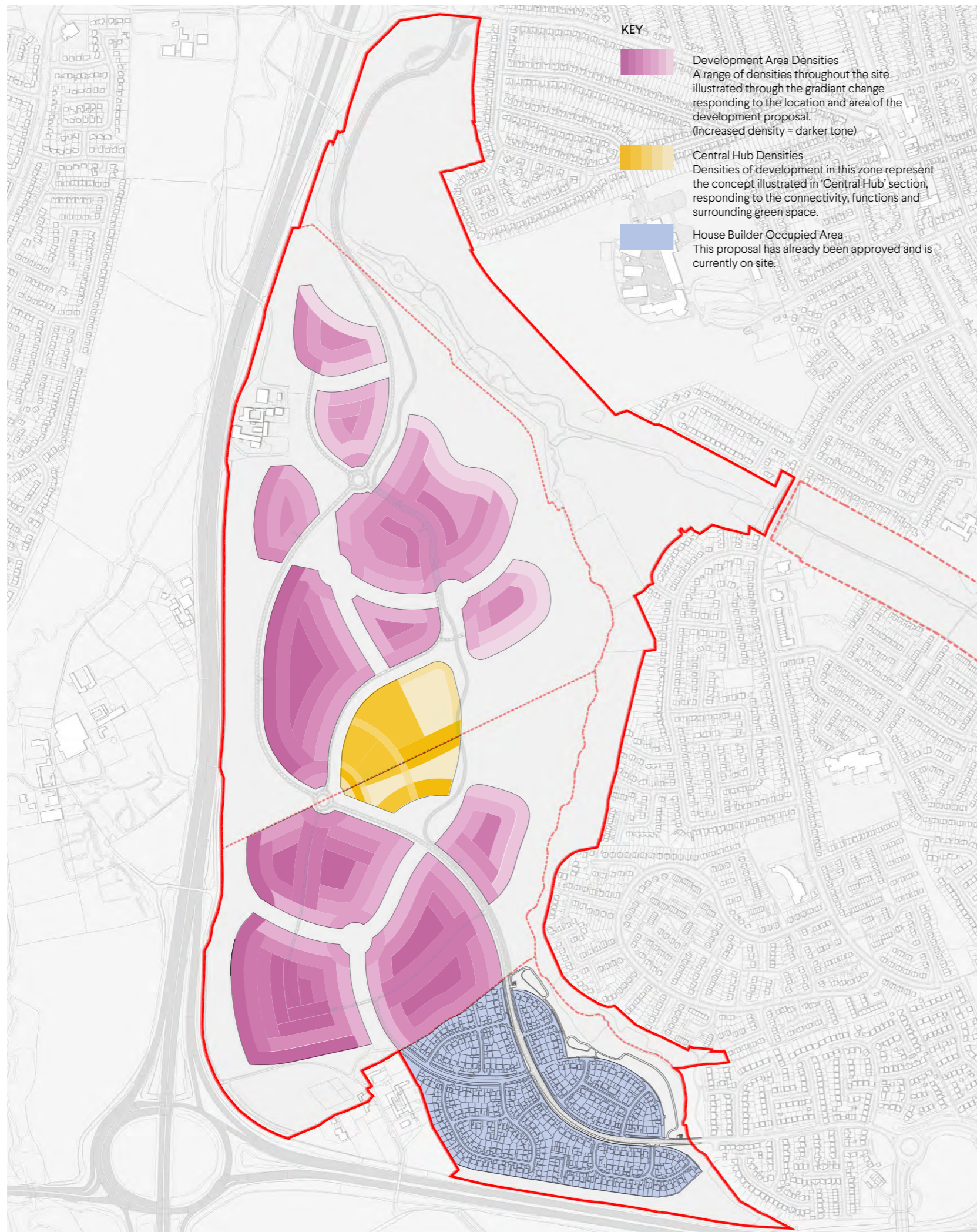
Proctor and Matthews, Cambridge © Tim Crocker



8.0

MASTERPLAN SUMMARY





8.0 Masterplan Summary

8.1 Development Quantum

An indication of suitable density arrangement is shown adjacent.

Peripheral areas addressing the park edge to the east should typically be of low density to permeate the barrier and visually draw the landscaping into the development.

The southern edge adjoining the recently completed earlier phase of development may be considered appropriate for medium to higher density residential arrangement reflecting a continuation in style of the adjacent areas, whilst influenced by the landscape principles established for the latter stages.

Central areas of residential clusters are most appropriate for higher density pockets of development. This approach can also be applied to residences buffering the Primary Road where development can perform a sheltering and screening function.

Specific areas adjacent to roundabouts and transition spaces where larger volumes of traffic are expected, and a hard edge is deemed appropriate are also suitable for higher density. In this location low rise apartments may be considered suitable.


























8.0 Masterplan Summary

8.2 Design and Principles Summary

Guidance provided by this report enables the creation of a sustainable residential development within a landscape setting that provides quality of design and residential amenity that is considered appropriate by Middlesbrough Council.

The adjacent diagram shows an overlay of all the key landscape and urban principles as outlined within this design code. Refer to the individual principle sections throughout the document for detailed diagrams and explanations.

KEY

	Existing Watercourses		Primary Road - Facing Development Area
	Flood Zones		Primary Road - Facing Green Space
	SuDs Attenuation		Secondary Road - Facing Development Area
	SuDs Conveyancing		Secondary Road - Facing Green Space
	Rain Gardens		Entrance Road to Development Areas
	Existing Woodland		Building Frontages (Dotted Lines = Facing out)
	Hedgerows		Development Edge - to Road
	Proposed Woodland		Development Edge - to Green/SuDs Corridor
	Avenue Planting		Development Edge - to Green Space
	Orchards		Gateway Buildings
	Parkland Trees		Primary Road Crossing Zone
	Amenity Grassland / Sports pitch		Secondary Road Green Corridor Crossing Zone
	Retained existing meadow habitats		Site Entrance Country Park Crossing Zone
	Grassland		Roundabout Node Locations
	4.5m Strategic Segregated Pedestrian & Cycle		Roundabout Node Build Condition
	3.5m Mixed Use Path (Indicative)		Landscaping Node Locations
	3.5m Compacted Gravel Paths (Indicative)		Landscaping Node Build Condition
	Potential Site Entrance Points		Development Area Densities
	Network of informal/formal paths (connections between existing habitats and open space network)		Central Hub Densities
	Formal Sports Provision		House Builder Occupied Area
	Multi-Use Games Area / Courts		Bus Stop - with 400m radius
	Landforms		Transport Hub Stop
	Bike Tracks - Single / Pump		Bus Route
	Walking/Running Routes - 0.5 / 1.5 / 2.5 / 3 miles		Transport Hub Loop
	Trim Trails		
	Wildlife Trails		
	Wildlife Hides		
	Docking / Dipping Ponds		
	Allotments		
	Community Growing		
	Seating and Bins		
	Community Centre		



8.0 Masterplan Summary

8.3 Stainsby S106 and Infrastructure Requirements

Development of the scale of that proposed at Stainsby brings with it significant infrastructure requirements. These range from open space, roads, and transport through to new schools, and medical facilities. Not all of these will be required at the start of the development, and the scale and nature of some of the infrastructure requirements will depend upon how the development progresses and other economic and environmental factors.

It is anticipated that developers will be consulted in relation to infrastructure delivery planning, however the masterplan aims to provide some indications of required S106 requirements.

To ensure that the infrastructure that is required to create the quality living environment that is sought at Stainsby, it is essential that these infrastructure requirements are planned for from the start. This not only means identifying what is required, but also identifying key trigger points, how much the infrastructure will cost, how it will be paid for and who will deliver it and how. Understanding these requirements from the outset will also allow developers to plan more effectively both in terms of the design of their schemes but also their financial modelling and phasing plans.

The infrastructure falls into the following broad categories:

- Education
- Country Park
- Strategic Highways/Transport
- Other open space

8.3.1 Education

Whilst school rolls fluctuate according to birth rates and popularity of schools, the scale of the development proposed within Stainsby is of a size that the school population will not be able to be accommodated within existing schools within Middlesbrough. There is also a sustainability issue that children of primary school age should be able to access a primary school within walking distance. To this end provision is being made for a new primary school to be delivered at the heart of the development within the proposed new local centre.

This should be provided in the form of a 2 form entry school (420 pupils) and associated nursery facilities but the site upon which the building is located should be capable of accommodating a larger school to allow for any potential future expansion.

Timing: The new primary school should be provided before occupation of 50% of the dwellings.

8.3.2 Strategic Highways/Transport

The Local Plan identified the need for strategic highways improvements to enable the delivery of the housing allocations contained within it. This included the Stainton Way Westward Extension (SWWE), and the Longlands to Ladgate Link Road (LLLR). The realisation of these two schemes will provide the necessary mitigation to enable the housing to be delivered in full. All residential developments are expected to make a contribution towards the delivery of these schemes. The provision of the SWWE is also key to opening up the Stainsby site for development as it also acts as the main distributor road serving the housing.

Timing: Link road to be provided in entirety by 2027

8.3.3 Country Park

Central to the creation of Stainsby is that of the proposed country park and integrated green links within the development. Further detail will be provided through the preparation of a country park delivery plan. As each phase of development comes forward it will be required to contribute towards the delivery of the country park either directly through the associated landscape strategy submitted as part of any planning application, or through a S106 contribution.

Timing: ongoing as part of phasing of development.

8.3.4 Other open space

Whilst the Country park provides a key strategic asset there will be a need for other elements of open space. The majority of this will be provided through the normal development control processes when assessing an application, but there will be a requirement for some additional open space and facilities to be provided these are:

Sports pitches:

2 adult grass football pitches/1 cricket square

Timing:

The pitches should be provided prior to occupation of 75% of the dwellings

8.3.5 Visitor centre/community hub

As part of the Local Centre and to act as a gateway to the Country Park and sports facilities a visitor centre will be required. This centre will serve the role as a community hub/facility and provide changing facilities for the pitches.

Timing:

Delivery of the centre will be linked to delivery of the Country Park, and should be provided no later than the provision of the sports pitches.



9.0 PLANNING REQUIREMENTS



9.0 Planning Requirements

9.1 Planning Validation Requirements

This design code seeks to establish the principles and strategies that will form the basis to any future planning application for the site at Stainsby.

The final design solution may vary from some of the indicative representations within this document but the key principles established set the guidance and highlight the key details to be included and developed upon within any submission. These will form the basis for further discussions and assessments between the planning authority and developer at a later date.

9.0 Planning Requirements

9.2 Housing Application Validation Requirements

The following list outlines the validation requirements to be met for a housing application at the Stainsby site, as established by Middlesbrough Council Planning Authority:

- o *Forms, Certificates, Site Location Plan & Fee*
- o *Detailed Plans & Elevations*
- o *Coloured Streetscenes*
– particularly for Committee
- o *Parking Plan*
- o *Boundary Treatment Plan*
- o *Statement of Community Involvement*
– letter drop / Community event
- o *Landscape details including management and maintenance*
- o *Design & Access Statement*
- o *Planning Statement*
- o *Transport Assessment / Statement*
- o *Draft Heads of Terms*
- o *Affordable Housing Statement*
– can be included within Planning Statement
- o *Sustainability Appraisal*
– can be included within Planning Statement
- o *Renewables Statement*
– details of 10% Renewables or fabric first approach.
- o *Secured by Design Statement*
– can be included within Planning Statement
- o *Communication infrastructure connectivity requirements' statement*
– can be included within the Planning Statement
- o *Flood Risk Assessment & Drainage Strategy*
- o *Ecological Assessments*
- o *Phase 1 Contamination*
– may be dealt with as a condition if no information is submitted
- o *Tree Survey & Arboricultural Implications Assessment*
- o *Noise Assessment*
– Road traffic/commercial premises as appropriate.
- o *Air Quality Assessment*
– the applicant should provide an air quality assessment or justification within the Planning Statement as to why it is not necessary, using the guidance laid out in the Institute of Air Quality Management landuse planning and development control planning for air quality document updated in 2017.
- o *Phasing Plan*
- o *Archaeological Desktop Assessment*
- o *Waste Audit*
– Waste Management Scheme (disposal of waste materials)
- o *Habitat Regulations Assessment*
– depending on location
- o *Masterplan*
– site/scale dependent
- o *Footpath and Cycleway connectivity plan*





GRADON ARCHITECTURE

Our Philosophy

We design and deliver excellent architecture based upon life experiences with care and efficiency. We believe that the quality of our surroundings has a direct influence on people and the quality of their lives; whether that is in the workplace, at home or in the public realm and it is these life experiences which influence our own ability to produce innovative, sustainable and high quality design.

As a Current Client we never take your patronage for granted and will continue to listen to you, to understand you, to deliver, to exceed expectation and to develop and nurture our relationship with you in order to build upon the trust you bestow upon us.

As a Prospective Client we want you to be a Current Client!

Our Architecture is about People.

Sectors

Commercial

Public & Arts

Residential

Education

Industrial

Retail & Leisure

Interiors

Specialist Care

Ecclesiastical

Services

Architecture

Masterplanning

Interior Design

Planning

Project Management

Contract Administration

Principal Designer

Visualisation

BIM

Recent Awards

CENE Project of the year 2017 - Ogden Physics Building

RIBA NE Award 2017 - Ogden Physics Building

Insider NE Architectural Practice of the Year 2017

LABC Best Inclusive Building 2016 - Bradbury View

CENE Value Winner 2016 - Bradbury View

NI Salon of the Year 2016 - RoCo

CENE SME of the Year 2014

CENE Value Winner 2013 - NEAS



Appendix 3 – Initial Impact Assessment of the Stainsby Country Park and Masterplan

Impact Assessment Level 1: Initial screening assessment

Subject of assessment:	Adoption of the Stainsby Country Park and Masterplan			
Coverage:	Crosscutting			
This is a decision relating to:	<input type="checkbox"/> Strategy	<input type="checkbox"/> Policy	<input type="checkbox"/> Service	<input type="checkbox"/> Function
	<input checked="" type="checkbox"/> Process/procedure	<input type="checkbox"/> Programme	<input checked="" type="checkbox"/> Project	<input type="checkbox"/> Review
	<input type="checkbox"/> Organisational change	<input type="checkbox"/> Other (please state)		
It is a:	New approach:	<input type="checkbox"/>	Revision of an existing approach:	<input checked="" type="checkbox"/>
It is driven by:	Legislation:	<input type="checkbox"/>	Local or corporate requirements:	<input checked="" type="checkbox"/>
Description:	<p>Key aims, objectives and activities</p> <p>To assess the impact of proceeding with the adoption of the Stainsby Country Park and Masterplan.</p> <p>Statutory drivers</p> <p>The Stainsby scheme situated in West Middlesbrough was allocated for housing in the Local Plan adopted in 2014. The Council must prepare Local Plans and associated planning documents, under the Planning and Compulsory Purchase Act 2004, and the National Planning Policy Framework (NPPF).</p> <p>Differences from any previous approach</p> <p>The scheme is part of the Housing Growth processes following the sites allocation within the 2014 Local Plan. As part of the normal process of preparing a site for development, a public space notice was displayed on the site of Stainsby North, highlighting the intention to dispose of the public open space. The publication of this notice provoked 904 response from the local community, and further afield. In response to the objections to the Public Open Space and Land Appropriation Notices, a report was presented to Executive on September 4th 2018. The report outlined the Councils vision for the site and requested that, in consultation with the public, a draft masterplan was to be produced.</p>			

	Key stakeholders and intended beneficiaries (internal and external as appropriate) The key stakeholders are: the Council; Local Community; and, Private Landowners.			
	Intended outcomes. To seek the approvals required to proceed to the next stage of the Housing Growth Process following development of a Draft Masterplan for Stainsby.			
Live date:	The Executive will consider the Stainsby Draft Masterplan report on 7 th June 2022.			
Lifespan:	Until the site is developed or the site allocation in the Housing Local Plan is removed/changed.			
Date of next review:	Not applicable			
Screening questions	Response			Evidence
	No	Yes	Uncertain	
Human Rights Could the decision impact negatively on individual Human Rights as enshrined in UK legislation?	☒	☐	☐	<p>Stainsby site is already allocated for residential use in the Council’s adopted Housing Local Plan, which underwent several rounds of public consultation – in full accordance with the Middlesbrough SCI – and a stringent Public Examination by an Independent Planning Inspector, who declared the Plan sound. As such, the principle of residential development in this location has already been established as being suitable.</p> <p>The draft masterplan looked to create a satisfactory development for residential purposes in consultation with the public. Consultation on a detailed masterplan has been undertaken in accordance with the Council’s adopted Statement of Community Involvement (SCI), except where the Coronavirus Regulations have prevented specific activities.</p> <p>In light of the above, it is not considered that the report will have an adverse impact on individual human rights.</p>

<p>Equality</p> <p>Could the decision result in adverse differential impacts on groups or individuals with characteristics protected in UK equality law? Could the decision impact differently on other commonly disadvantaged groups?*</p>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<p>Stainsby site is already allocated for residential use in the Council’s adopted Housing Local Plan, which underwent several rounds of public consultation – in full accordance with the Middlesbrough SCI – and a stringent Public Examination by an Independent Planning Inspector, who declared the Plan sound. As such, the principle of residential development in this location has already been established as being suitable.</p> <p>The draft masterplan looked to create a satisfactory development for residential purposes in consultation with the public. Consultation on a detailed masterplan has been undertaken in accordance with the Council’s adopted Statement of Community Involvement (SCI), except where the Coronavirus Regulations have prevented specific activities.</p> <p>In light of the above, it is not considered that the report will have an adverse impact on different groups or individuals in terms of equality.</p>
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<p>Community cohesion</p> <p>Could the decision impact negatively on relationships between different groups, communities of interest or neighbourhoods within the town?*</p>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<p>Stainsby site is already allocated for residential use in the Council’s adopted Housing Local Plan, which underwent several rounds of public consultation – in full accordance with the Middlesbrough SCI – and a stringent Public Examination by an Independent Planning Inspector, who declared the Plan sound. As such, the principle of residential development in this location has already been established as being suitable.</p> <p>The draft masterplan looked to create a satisfactory development for residential purposes in consultation with the public. Consultation on a detailed masterplan has been undertaken in accordance with the Council’s adopted Statement of Community Involvement (SCI), except where the Coronavirus Regulations have prevented specific activities.</p> <p>In light of the above, it is not considered that the report will impact negatively on relationships between different groups, communities of interest or neighbourhoods within the town.</p>
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Next steps:

- ➡ If the answer to all of the above screening questions is No then the process is completed.
- ➡ If the answer of any of the questions is Yes or Uncertain, then a Level 2 Full Impact Assessment must be completed.

Assessment completed by:	Alex Conti	Head of Service:	Paul Clarke
Date:	18/05/2022	Date:	18/05/2022